



VICTORIA ROAD DRUMMOYNE URBAN DESIGN REVIEW

Consolidated Report

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by Studio GL

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1-1 Overview

The purpose of the Victoria Road Urban Design Review is to develop initiatives that seek to revitalise retail along Victoria Road, improve public domain outcomes and enhance the existing character of surrounding lower density heritage conservation areas.

Key to the review was ensuring that the desired future character of the Victoria Road commercial precinct was identified through consultation with the community and relevant stakeholders.

In 2018, the City of Canada Bay commissioned Studio GL, supported by AEC (property economics), Sym Studio (landscape) and Henson Consulting (traffic/ transport), to lead an urban design review of the Victoria Road commercial precinct.

The objectives of the study were:

- To work with the community and relevant stakeholders to determine the desired future character of the Victoria Road commercial precinct.
- To ensure future development protects and enhances the existing character of surrounding low density residential heritage conservation areas.
- To improve public domain outcomes, including access and opportunities for landscaping and provision of canopy trees along Victoria Road, its cross streets and parallel streets within the immediate proximity.
- To revitalise and reinvigorate the retail functions along Victoria Road and to properties that benefit from a secondary street frontage, where appropriate.

The Victoria Road Urban Design Review identifies the key challenges facing the commercial precinct and proposes ways to activate and enhance this precinct. Victoria Road Drummoyne is one of the busiest roads in Australia, as one of the few places where it is possible to cross the Parramatta River, this six lane transport corridor provides a crucial link from the north and north-west to the south and south-east towards the CBD, Sydney's Eastern City.



INTRODUCTION

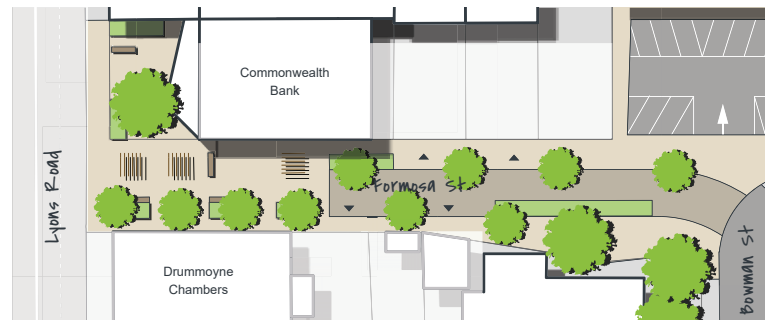
The story of Drummoyne, however, is much older than Victoria Road. Before the Gladesville and Iron Cove Bridges reshaped this quiet peninsula the area was a desirable residential area and this has left a legacy of Victorian, Federation and Inter-War housing surrounded by the Parramatta River and its bays and inlets.

Initially, Victoria Road and its effective links to the CBD attracted businesses that lined this busy thoroughfare servicing local residents and the wider community, but as the corridor has become busier the ability to cross the road has been reduced and kerbside parking has been removed to accommodate additional lanes of traffic. Over time these changes have impacted on local businesses, especially those without onsite parking or alternative street access.

The eastern and western sides of Victoria Road are distinctly different. Typically the land on the eastern side is lower and continues to fall down to the river. Land on the western side of Victoria Road, and either side of Lyons Road is higher, although Victoria Road also undulates across Drummoyne with higher points between Day and Church Street and Lyons Road. Block sizes on the western side are deeper, with some sites extending to Formosa Street and others extending to Council owned carparking. Sites on the eastern side are shallow, back onto low scale housing in a conservation area and very few have access off a secondary road.

Victoria Road Drummoyne is the local centre for the surrounding community, providing convenience retail and local services. It is unique and is a special place for the local community, who value their local centre for its heritage charm and character and the convenience and access it provides to the city and the surrounding areas.

The area is also changing. In recent years a few large developments have been built, especially on the western side of Victoria Road, which has deeper blocks and additional access off Formosa Street. Although not particularly tall, at five to six storeys high they contrast in scale to the low rise conservation areas and smaller heritage buildings.



1-2 The study area

The study area is located within the suburb of Drummoyne and runs along either side of Victoria Road for approximately 1km from north-west to south-east with an area of 11.4 hectares. The study area includes properties either side of Victoria Road and along a small section of Lyons Road.

Victoria Road dominates the character of this section of Drummoyne and is a very busy state-owned road that links this area and areas to the North to the Sydney CBD. The study area is divided due to the heavy traffic flow, the limited number of safe pedestrian crossings and long delays at signalised intersections. The quality of the street level environment is also noisy and polluted creating an uncomfortable place for pedestrians, businesses and residents. The area is well served by local buses.

The boundary of the study area includes the intersection of Victoria Road and Lyons Road, another state-owned road, and a section of Lyons Road up to Marlborough Street. This section of Lyons Road is also busy but traffic flows have been reduced by the redirection of northbound traffic away from this intersection down Marlborough and Bayswater Streets and onto Victoria Road at Westbourne Street.

The study area lies within the City of Canada Bay Local Government Area (LGA) and is subject to the Canada Bay Local Environmental Plan (CBLEP) 2013 and the Canada Bay Development Control Plan (DCP) 2017. As per CBLEP 2013 the study area is currently zoned B4 Mixed Use, and consists of retail shops, commercial uses and medium-to-high density residential buildings along the high-traffic Victoria Road in Drummoyne, surrounded by low-density residential buildings on adjacent streets.

The centre has few 'public' spaces and the few areas providing public benefit include a small triangular area on the north western corner of Lyons Road and Victoria Road, a small area of privately owned land outside the former Commonwealth Bank building on Lyons Road and a privately owned courtyard at the rear of the Sutton Building shops.



EXECUTIVE SUMMARY



Figure 1 Aerial map of the study area

1-3 Methodology

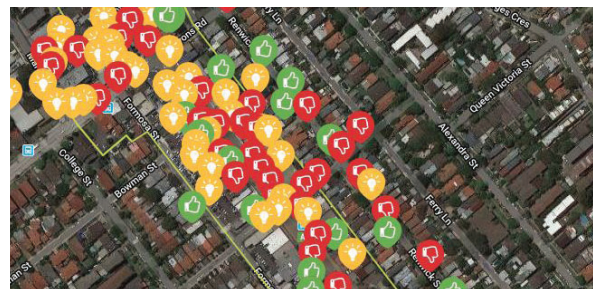
Community Consultation

Community and stakeholder engagement was the first objective of the Victoria Road Urban Design Review and this process has shaped the development of the desired future character, design principles, drivers and recommendations.

Consultation and stakeholder engagement processes involved a series of events in two stages, designed to engage the local community. The community were initially consulted early in the process to capture local knowledge and “user insights” about how the area currently functioned, what was valued and how they wished to see the place develop in the future. A second round of engagement occurred to test the ideas and recommendations while they were still in the process of being developed.

Consultation activities included workshops with a community reference group, chaired by the Mayor, drop-in sessions outside the local supermarket, community workshops, intercept surveys at a local Community Centre to allow for input from senior residents, a workshop with the Student Representative Council (SRC) at the local primary school and an online consultation portal.

The community response was strong. The Victoria Road precinct is valued by locals for the heritage charm of the local area, the character of heritage buildings and the convenient access to the city and surrounding areas. Traffic congestion, the challenges of access, especially the difficulty of crossing Victoria Road, and parking were identified as the biggest challenges. The scale of recent development and building heights raised concerns over the impact of new development on adjoining neighbours, especially as it related to loss of sunlight and reduced privacy and amenity.



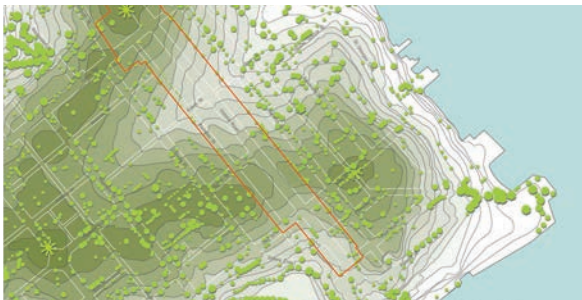
The desire for more landscape and trees, public spaces, street furniture, public art, and colour and vitality have been addressed in *Chapter 6 Activation Initiatives*. The pressure of recent development, due to the challenge that larger development can appear to be out of scale with nearby low scale development in conservation areas, led to recommendations to amend Council's planning controls, identified in *Chapter 7 Planning Framework*. Suggestions to upgrade the existing village character of Drummoyne and encourage commercial and retail uses and night-time activity can also be found in these two chapters.

Many community concerns related to traffic, the difficulty of pedestrian and bicycle access around the centre and the difficulties of parking. Although not directly related to the objectives of the project, this was such a significant issue that the project team felt it was important to consider traffic and, where possible, identify opportunities and potential solutions. These have been recorded in *Chapter 8 Transport and Traffic*. A detailed parking study is currently underway and it is recommended that this section of the report is provided to the consultants preparing this study.

Place, Context and Analysis

Detailed review and analysis studies were undertaken to support the consultation process. This included:

- A detailed review of previous work undertaken, Council's planning policies and studies including Drummoyne Village Development Control Plan and State Government strategies and policies including the Eastern City District Plan and GANSW's Better Placed design policy.
- Spatial analysis of the study area and its wider context including detailed mapping of urban structure, landform, heritage and land use, creation of a 3D model, an activity analysis (day and night), urban characteristics supported by site visits and photographic analysis.
- A review of the existing planning controls and an exploration of alternative development scenarios through capacity testing and feasibility analysis.



Recommendations

Underpinning the recommendations of this study are the design principles and the desired future character outlined in *Chapter 5 Future Character*, which identifies the role of the study area as a focus for economic activity and community interaction, future placemaking initiatives and strategies in relation to landscaping, public art and place activation.

The proposed implementation actions have been separated into two categories. Actions that occur on public land, or can be led by Council where Council is a major landowner, are identified in *Chapter 6 Activation Initiatives*. Actions that impact on development on private land and would require amendments to the Canada Bay Local Environmental Plan 2013 and/or the Canada Bay Development Control Plan can be found in *Chapter 7 Planning Framework*. A summary of the key initiatives can be found in Figure 2.

Together the design principles, drivers and actions create an integrated strategy and activation plan for the Victoria Road commercial precinct. While it goes beyond the scope of this project to solve the significant conflict created by Victoria Road being a critical transport corridor for Sydney and the area's function as a local centre (the social, retail and community heart of the local area), this study identifies the underlying challenges and offers recommendations that will help strengthen the attraction of the place, expand the quality and size of public open space and reveal and nurture the precinct's unique character.

1-4 Key initiatives

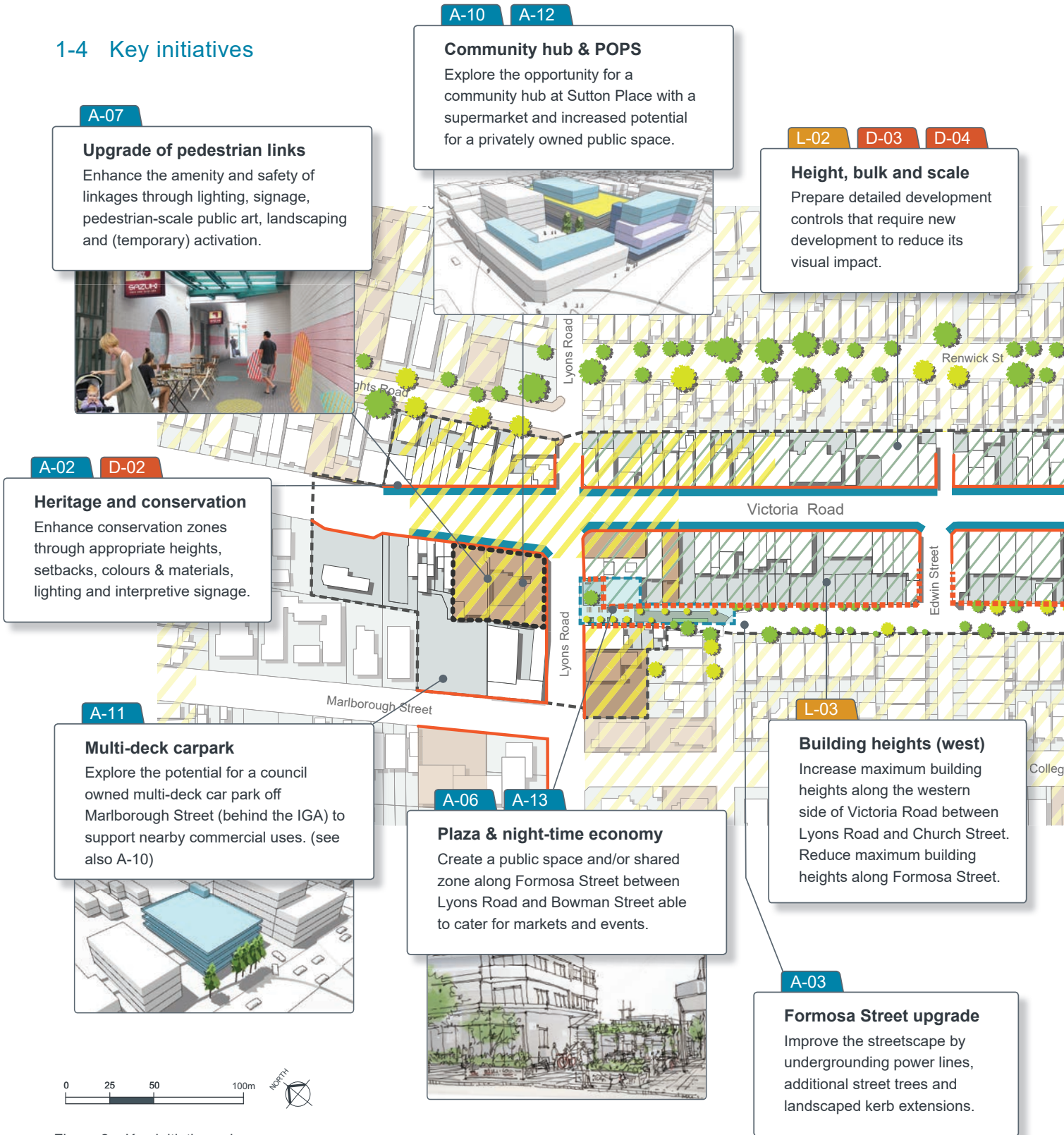


Figure 2 Key initiatives plan

A-04 A-09 D-05

Streetscape upgrades

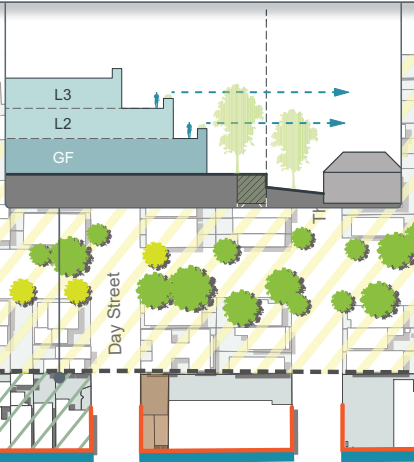
Improve the streetscape quality of Victoria Road including better footpath paving, smart light poles and greening/landscaping.



D-01

Rear and side setbacks

Develop new setback controls to minimise the impact of new built form on outlook, sun access and privacy of adjoining properties.



A-01

New street trees

Develop a street tree masterplan and plant additional trees, particularly along Renwick, Formosa and Wright Streets.



Victoria Road

Day Street

Park Avenue



Formosa Street

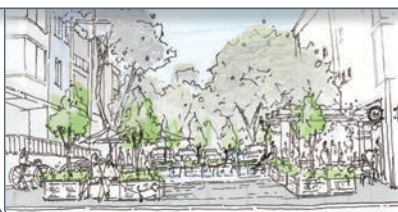
Church Street

Church Crescent

A-05 A-08

Public space & public art

Create new pedestrian/ gathering space on Church Street and add trees and greenery, seating, a children's play area and public art.



D-06

Planning Proposal Site

Site of current planning proposal for Mixed Use development.

L-01

Active frontages

Extend the requirement for active ground floor uses along Formosa, Day, Church and Edwin Streets.

D-07

Sustainable Design

Consideration to be given to sustainable design initiatives including passive solar, energy conservation & efficiency and natural ventilation.

D-06 D-08 D-09 D-10

Revitalising retail

Expand current development controls to include more detailed requirements for quality signage, awning design, lighting, facades and shopfronts.

- Victoria Road streetscape upgrade
- Potential new public space
- Potential community hub site
- Existing active frontage
- Proposed active street frontage
- Amended building height controls (LEP)
- Heritage conservation zone
- Lot with heritage item
- Existing street tree
- Potential new street tree
- Study area boundary



CHAPTER 2 PLANNING CONTEXT REVIEW

2-1 Strategic context

Eastern City District Plan

The Eastern City District Plan released in 2018 by the Greater Sydney Commission provides a 20-year plan to manage social, economic and environmental growth in the Eastern City suburbs in order to achieve the 40-year vision for Greater Sydney.

Within this Plan, Drummoyne is identified as a Local Centre and Victoria Road is identified as a City Serving Transport Corridor. As per the Plan, Local Centres should focus on increasing walkable access to the centre, renewing public places within the centre, respecting the District's heritage, improving access to public transport, co-locating facilities and social infrastructure, and protecting and expanding retail and/or commercial floor space.

The Plan also identifies residential land around local centres for medium density housing such as villas and townhouses to provide greater housing variety and links for walking and cycling to help promote a healthy lifestyle.

The document states that *"additional residential development within a five-minute walk of a centre focused on local transport, or within a 10-minute walk of a centre with city-shaping or city-serving public transport, will help to create walkable local centres. However, housing should not compromise a centre's primary role to provide goods and services, and the opportunity for the centre's employment function to grow and change over time"*.

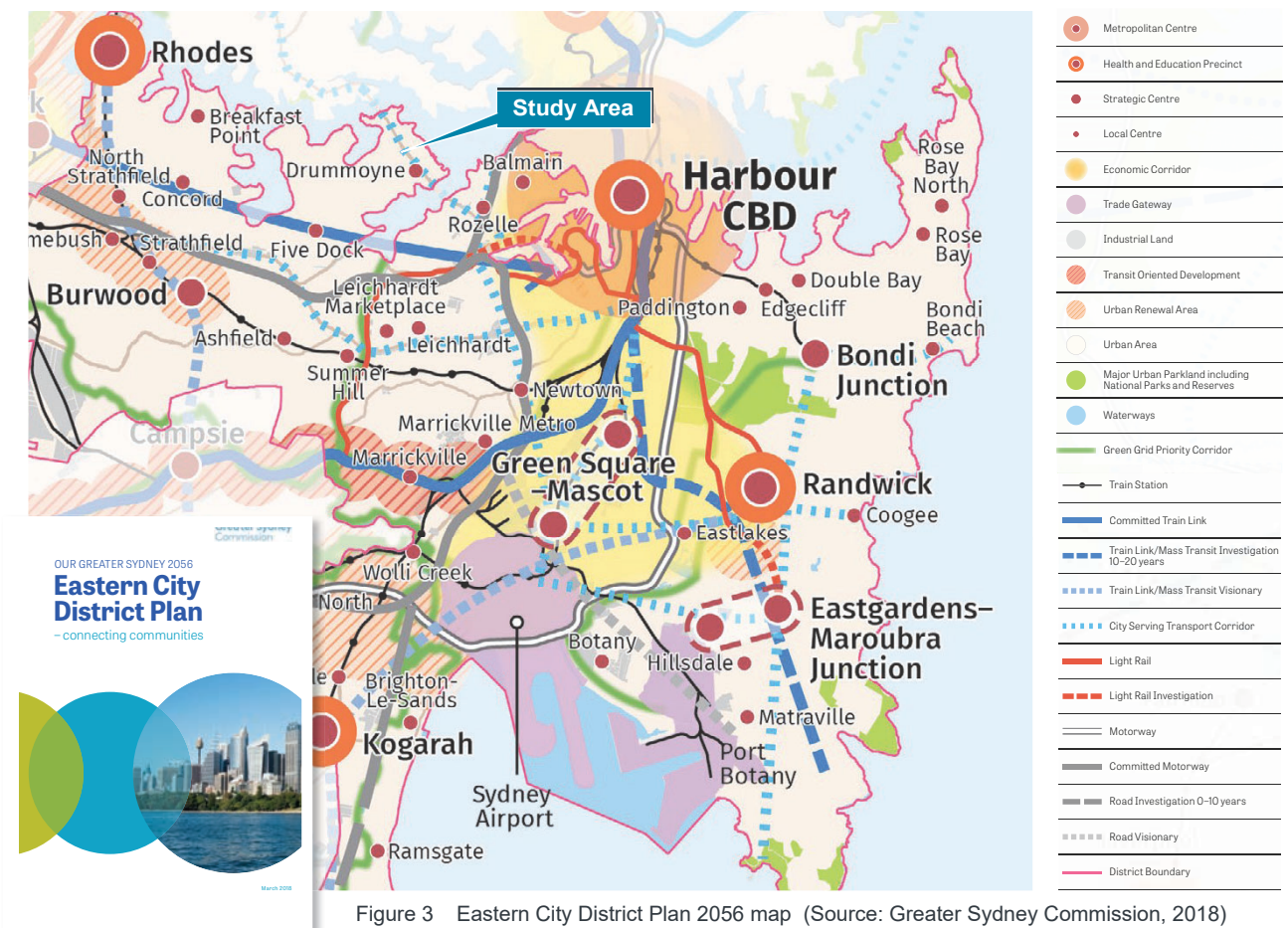


Figure 3 Eastern City District Plan 2056 map (Source: Greater Sydney Commission, 2018)

Better Placed - An integrated design policy for the built environment of NSW

Author: Government Architect NSW
Date: May 2017



Purpose of the document

Better Placed emphasises the importance of good design in improving the quality of infrastructure, architecture and public spaces in New South Wales. The document identifies key priorities in six challenge areas including health, climate resilience, growing population, changing lifestyles and demographics, infrastructure and urban renewal.

Relevance to this study

The document identifies seven key objectives for achieving a better design of the built environment:

- Better fit: Every place has distinctive qualities and characteristics which should be retained even with significant growth, and buildings that resonate with the local community are more desirable.
- Better performance: New developments should incorporate sustainable systems through efficient use of building materials, energy and water.
- Better for community: Need for diverse uses and housing types, inclusive economic frameworks and places for social engagement and recreation.
- Better for people: Creation of accessible public spaces for people with a focus on safety, well-being and enjoyment.
- Better working: Good design must support long-term usability for formal and informal activities.
- Better value: Urban precincts, spaces and buildings should have long-term returns in terms of financial, social and environmental value.
- Better look and feel: Spaces should be welcoming, engaging and attractive to the community.

SEPP 65 and the Apartment Design Guide

Author: NSW Department of Planning and Environment
Date: July 2015



Purpose of the document

The State Environmental Planning Policy No.65 - Design Quality of Residential Apartment Development (SEPP 65) and the Apartment Design Guide (ADG) promote better apartment design across NSW. The policy establishes a consistent approach to the way apartment designs are assessed by councils and the guide explains how to apply SEPP 65's design principles.

Relevance to this study

SEPP 65 outlines nine Design Quality Principles to improve the attractiveness of residential flat buildings, shop top housing and mixed use developments. These are:

- Principle 1: Context and neighbourhood character
- Principle 2: Built form and scale
- Principle 3: Density
- Principle 4: Sustainability
- Principle 5: Landscape
- Principle 6: Amenity
- Principle 7: Safety
- Principle 8: Housing diversity & social interaction
- Principle 9: Aesthetics

The ADG provides detail and design guidance for residential apartment development to meet the SEPP's principles including visual privacy, solar and daylight access, common circulation and spaces, apartment size and layout, ceiling heights, private open space, natural ventilation, and storage. The requirements of the SEPP 65 and the ADG are important considerations for future apartment designs within the study area.

Community Strategic Plan Your Future 2030

Author: City of Canada Bay
Date: June 2018



Purpose of the document

The City of Canada Bay's Community Strategic Plan for the next twelve years highlights five key themes for the LGA based on extensive community engagement. These are inclusive; involved and prosperous; environmentally responsible; easy to get around; engaged and future- focussed; visionary; and smart and accountable.

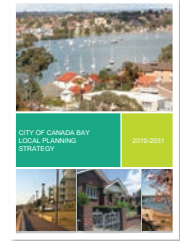
Relevance to this study

According to the Plan, the forecast population growth for Drummoyne until 2036 is 11.4% in contrast to 27.9% for Concord, 43.4% for Five Dock and 98.8% for Rhodes. Victoria Road in Drummoyne is identified as a major urban transformation area. Through the five key themes, the community strategic plan highlights that future development should:

- Improve inclusiveness and accessibility of spaces and increase use of Council run facilities;
- Create diversity of housing stock to meet needs, provide cheaper housing prices, increase percentage of affordable housing and consider heritage values;
- Create more linked footpaths and cycleways, improve public transport connections;
- Increase community participation, improve community satisfaction with planning processes, increase number of businesses participating in economic initiatives; and
- Improve Council operational processes and increase participation in social media.

Canada Bay Local Planning Strategy

Author: City of Canada Bay
Date: June 2010



Purpose of the document

The Canada Bay Local Planning Strategy (CBLPS) charts out objectives and actions for the outcomes from seven themes as outlined in the FuturesPlan20, the Community Strategic Plan released in 2009. The document provides a framework to guide the preparation of a new LEP and DCP through alignment with the visions of Sydney's Metropolitan Strategy. The CBLPS is currently being reviewed to align with the updated community Strategic Plan 'Your Future 2030'.

Relevance to this study

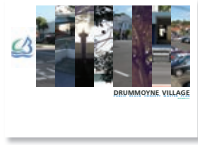
The CBLPS identifies Victoria Road Drummoyne as a 'traditional main street' centre and estimates capacity for 342 additional dwellings within the centre in the short to medium term. As compared to other suburbs within the LGA, Drummoyne is identified as an area having one of the highest percentages of persons who commute by public transport (25.4%). A 3.5km stretch of Victoria Road, between Westbourne Street and The Crescent, is identified for future road upgrades (by RMS). Drummoyne's per capita provision of open space is below average (1.95ha/1,000 people as opposed to an average of 2.72ha/1,000 for the LGA) with an additional open space requirement of 10.68ha by 2031. Key actions for the study area include:

- Increase residential densities in, and in the immediate vicinity of, the Drummoyne centre;
- Establish new and improved civic spaces;
- Protect existing employment generating capacity and provide diverse opportunities for future employment; and
- Investigate opportunities to increase the amount of employment floorspace.

Drummoyne Village Public Realm Concept Master Plan

Author: City of Canada Bay

Date: September 2011



Purpose of the document

This document provides concept designs for the upgrade of several 'community gathering spaces' within the Drummoyne Village area, considered the area in and adjacent to the Lyons Road / Victoria Road intersection. Six spaces are identified for improvement including the Wrights Road corner, currently used as a bus turning area.

Relevance to this study

Proposed interventions are generally small in scale and focus on changes to the following elements:

- Signage;
- Trees and landscaping;
- Traffic management;
- Parking; and
- Integration of public art or community art projects.

Some of the concept designs have already been implemented, such as the installation of planting and street furniture at the corner of Lyons Road and Victoria Road. The success of small changes, and the improvement that can be achieved via 'greening' of the area, is of relevance to the current study.

Drummoyne Village Development Control Plan

Author: City of Canada Bay

Date: September 2007



Purpose of the document

This Development Control Plan applies to the area bound by Victoria Road, Lyons Road and Marlborough Street. The document sets out planning controls designed to create a viable Village Centre.

Relevance to this study

The objectives of this DCP are to improve access, both vehicular and pedestrian, and to improve the built environment outcomes via a focus on response to heritage, integration of open space and the provision of high quality design and amenity. There is also a desire to facilitate diversity of use, including retail, commercial and residential uses.

Sections propose development up to 6 storeys in height, based on modelling that steps back from heritage and adjacent residential items. Parts of this DCP have been actioned with development occurring on the Marlborough Street/ Lyons Road corner, but there has not been significant development of the entire identified area. The Council carpark remains on-grade, and the majority of buildings in the Village Centre area are one or two storeys.

Canada Bay Local Environment Plan

Author: City of Canada Bay
Date: 2013



Purpose of the document

This document identifies the land uses appropriate for the area and sets upper limits for elements such as building heights and permissible density of development. This is a legal document, prepared by Council and approved by the State Government to regulate land use and development.

Relevance to this study

This document is the primary planning control tool that governs development in the area, and covers issues such as permitted or prohibited development, land use, exempt and complying development and principal development standards.

Section 2.2 of this chapter sets out the key LEP controls and identifies key provisions impacting on the study area.

Chapter 7 of this report considers a range of provisions set out in the LEP that relate to the study area, and proposes amendments aimed at creating better development outcomes.

Canada Bay Development Control Plan

Author: City of Canada Bay
Date: 2018



Purpose of the document

This document provides detailed planning and design guidelines in support of the LEP, tailored to local issues. It is a guideline only and is intended to be specific for the area in which it operates.

DCP controls cover issues including:

- General controls relating to equity of access, waste management, tree preservation etc
- Controls around Heritage relating to development of and in the vicinity of heritage items and development in and in the vicinity of Heritage Conservation Areas
- Residential development, including design quality, environmental quality and residential amenity
- Mixed use areas and neighbourhood centres including building envelope controls and site specific design controls and local neighbourhood shops
- Industrial development
- Signage and Advertising
- Child Care Centres

Relevance to this study

A DCP can set out a wide range of controls that provide guidance for an area, with a focus on development on private property.

Section 2.3 later in this chapter provides a summary of the key DCP controls that are impacting on the study area.

Chapter 7 of this report proposes a range of amendments to the existing DCP controls that would result in better development outcomes.

2-2 Key LEP controls

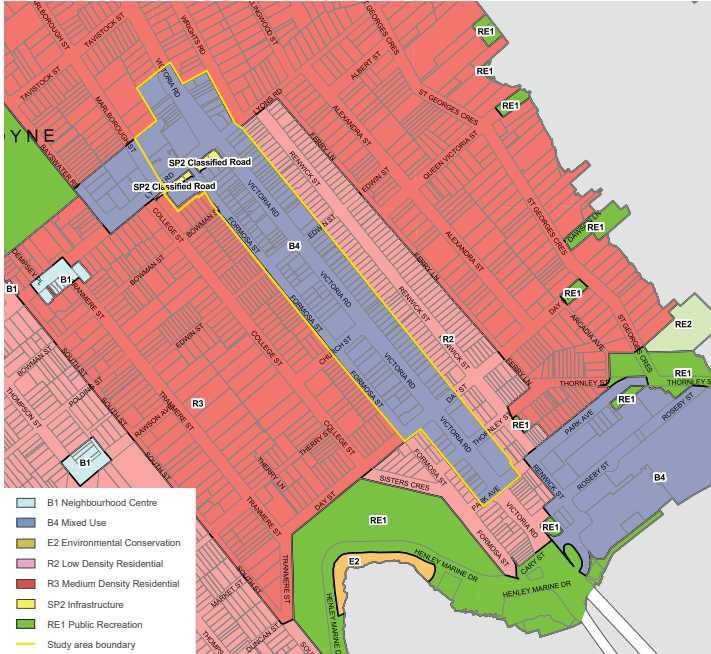


Figure 4 Study area over extract of CBLEP 2013 Land Zoning Map

Land Zoning

The study area is zoned B4 Mixed Use with the exception of two areas near the intersection of Victoria Road and Lyons Road which are identified as SP2 'Classified Road'.

The purpose of the B4 zone is to integrate suitable land uses such as business, office, residential and retail in accessible locations in order to maximise public transport patronage and encourage walking and cycling. At present a variety of uses have active frontages along the ground floor of Victoria Road, and include general retail, homeware and furnishing, healthcare, and laundry/salon services. A number of premises are currently vacant.

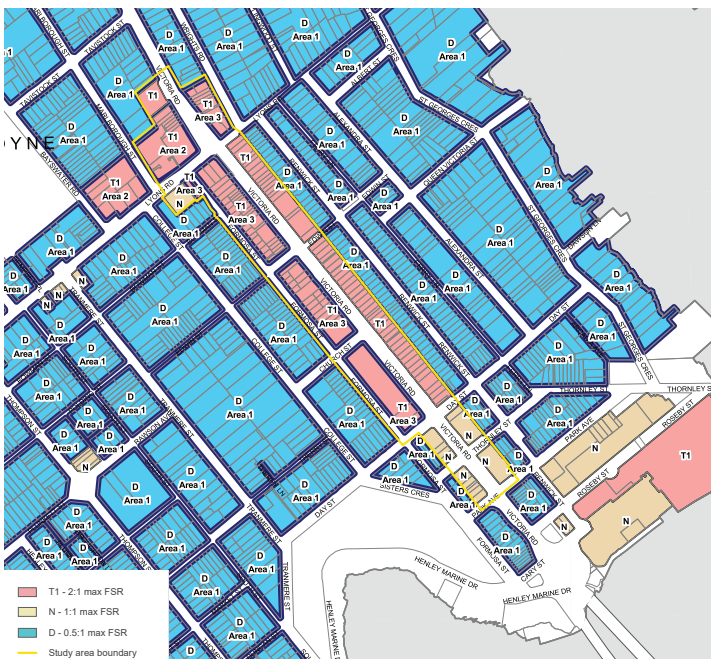


Figure 5 Study area over extract of CBLEP 2013 FSR Map

Floor Space Ratio

Predominantly, the study area has an FSR of 2:1 (T1) with the exception of one block to the south of Lyons Road and four blocks on Victoria Road stretching from Day Street to Park Avenue which have an FSR of 1:1 (N). The FSR along Victoria Road is significantly higher than that of the surrounding low-density residential areas where an FSR ratio of 0.5:1 applies.

Certain provisions in the LEP allow for higher FSRs of up to 3.5:1 for sites marked as 'Area 2' and up to 3:1 for sites marked as 'Area 3' on the basis of their site area. The Drummoyne Village site at the intersection of Lyons Road and Victoria Road is the only site marked as 'Area 2' within the study area.



Figure 6 Study area over extract of CBLEP 2013 Height of Building Map

Height of Buildings

Predominantly, urban blocks to the west of Victoria Road have a maximum building height ranging from 15m (O1) to 20m (Q), while land to the east of Victoria Road generally has a lower maximum building height of 12m (M). Exceptions to this include four blocks on Victoria Road between Day Street and Park Avenue which have a maximum building height of 8.5m (I), and a block at the intersection of Lyons Road and Victoria Road which has a maximum building height of 15m (O1).

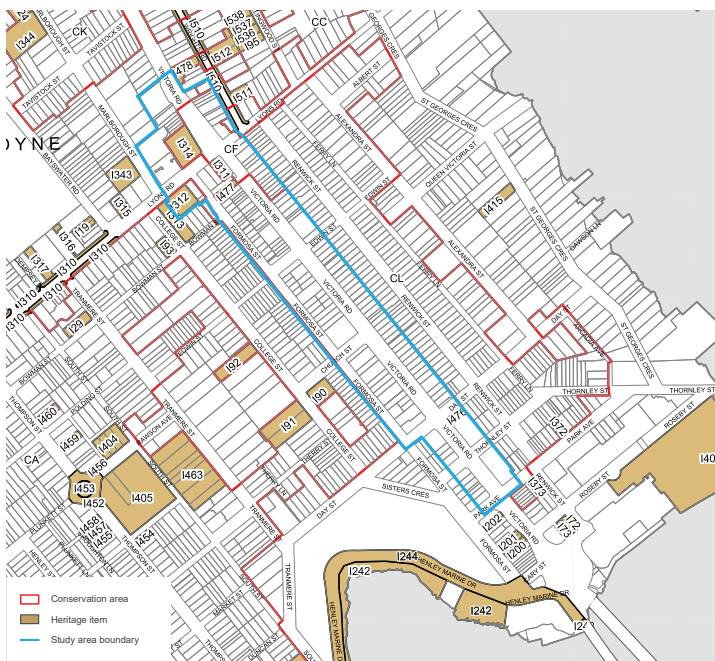


Figure 7 Study area over extract of CBLEP 2013 Heritage Map

Heritage items

Five heritage items are located within the study area, including the Sutton Place shops, the Oxford Hotel, the Drummoyne Fire Station, the former Westpac Bank and a shop and residence at the corner of Day Street and Victoria Road. A number of heritage items are located in the immediate vicinity of the study area, predominantly to the north and east.

Large areas around the study area are identified as heritage conservation areas, and extend into the study area around the intersection of Victoria Road and Lyons Road to the north-west. Note: Additional information about heritage items is provided in section 3-4 of this report.

2-3 DCP controls summary

Canada Bay DCP

The sections of the City of Canada Bay Development Control Plan (DCP) most relevant for the study area are Part F Mixed Use Areas and Part D Heritage. The Drummoyne Village DCP is another important document that outlines controls for the Drummoyne Village site on the corner of Lyons Road and Victoria Road.

Part F - Mixed Use Areas

This part of the DCP outlines the general requirements for all development within the B4 Mixed Use zone (Part F1) and site-specific design controls that are particularly relevant to Victoria Road Drummoyne (Part F2.1).

The future character of Victoria Road is envisioned as a busy, thriving urban area with street edge development consisting of retail, residential and commercial uses. Improvements to the streetscape of Victoria Road are proposed through the use of coordinated advertising policies. The western corner of the Victoria Road and Lyons Road intersection is identified as a gateway to Drummoyne and emphasising the corner site is considered important for the future of the area.

The following existing general controls are identified for Victoria Road Drummoyne:

| Controls | |
|----------|---|
| C1 | Infill development in the Drummoyne Commercial Area should include parapet skylines and use non-reflective materials. |
| C2 | Buildings should be built to the street alignment. |
| C3 | Transitional building heights should be provided between the commercial and residential areas to protect the amenity of surrounding neighbours. |

The controls for this area require buildings to be built to the street alignment on Victoria Road and Formosa Street, for all blocks between Lyons Road and Day Street.

The existing controls specify that the maximum building height along the western side of Victoria Road should be six (6) storeys and should occur in the block between Church Street and Day Street. The upper storey should be set back a minimum of 3m from the five (5) storey component facing Victoria Road. The maximum building height along Formosa Street should be three (3) storeys with a 5m upper storey setback. This block also requires a continuous cantilevered awning to be provided along the Victoria Road frontage.

The existing maximum building height along the western side of Victoria Road, between Church Street and Lyons Road, should be four (4) storeys along Victoria Road reducing to two (2) storeys along Formosa Street. The depth of the two storey development is required to be one-third (1/3) of the lot depth measured from Formosa Street. Continuous cantilevered awnings are not specified for this section of Victoria Road.

The eastern side of Victoria Road is identified to have a maximum building height of three (3) storeys towards Victoria Road tapering to one (1) storey at the rear boundary at an angle of 30 degrees. Transitional Building Heights should be provided between commercial and residential areas.

The eastern side of the Victoria Road/ Lyons Road intersection should have a maximum building height of two (2) storeys on Victoria Road, Lyons Road and Wrights Road, with four (4) storeys within the centre of the site.

The blocks to the south of Day Street along Victoria Road should have a maximum building height of two (2) storeys.

The vehicular parking rates for the permissible uses within the B4 Mixed Use zone can be found in Part C3 of the General Controls.

Part D - Heritage

Part D of the DCP identifies controls for the management of heritage items, properties in the Heritage Conservation Areas (HCAs) and properties in the vicinity of heritage items and HCAs.

Part D2 - Development of heritage items

The DCP states that where a heritage item is a landmark, it is particularly important that new development does not obscure its visual presence in the streetscape. It is also important that new development respects the scale of the existing heritage buildings that contribute to the significance of the place. The roof lines of buildings and elements such as chimneys, parapet walls and verandahs are important elements of the form of a heritage item. Some key controls with respect to development of heritage items are as follows:

| Controls | |
|------------|---|
| D2.1 C1 | Original elements that contribute to the setting of a heritage item such as landscaping, fences, driveways, seawalls etc should not be removed. |
| D2.2 C1 | Alterations and additions to a heritage item should not be larger in scale than the heritage item and should preferably be single storey. |
| D2.2 C2 | Development of a larger scale than the heritage item is allowable only if it can be demonstrated that the new development will not detract from the aesthetic quality and important views of the heritage item. |
| D2.3 C1 | Important elements of the form of a heritage item such as main roof forms, chimneys, parapet walls, verandahs etc should not be demolished or obscured by alterations and additions. |

Part D3 - Development in the vicinity of heritage items

As per the DCP, development in the vicinity of a heritage item will predominantly affect properties that share a boundary with or are opposite a heritage item. It may also be applicable when important views of a landmark building are being impacted by a development.

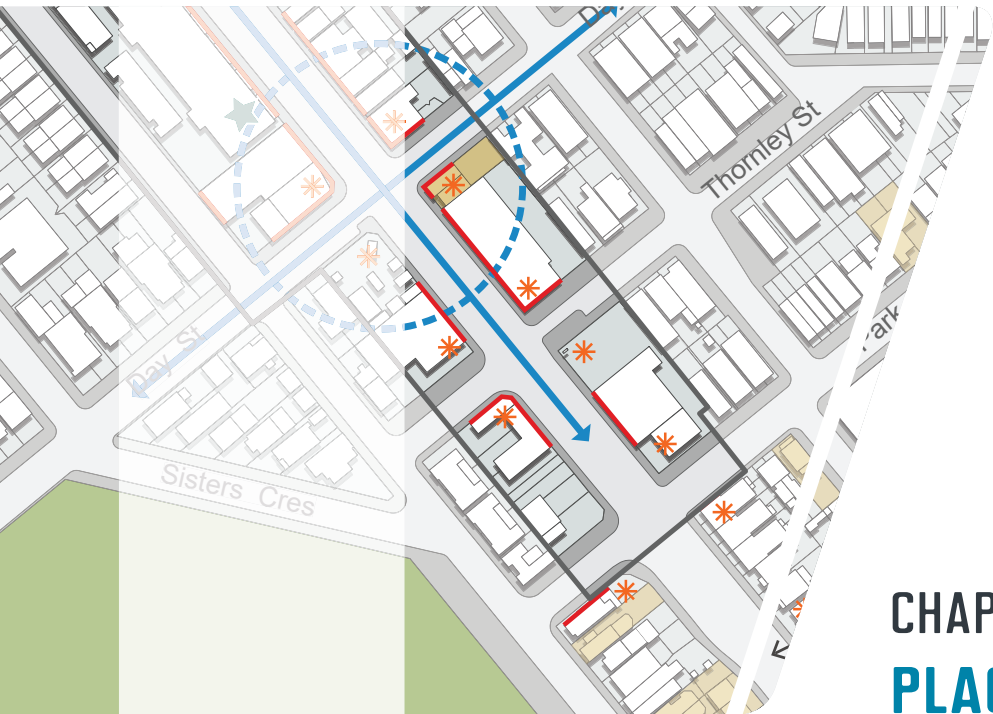
Some key controls for development in the vicinity of heritage items are as follows:

| Controls | |
|------------|--|
| D3.1 C1 | The setbacks of new development in the vicinity of a built heritage item should generally be equal to or greater than that of the heritage item. |
| D3.2 C1 | The scale of new development in the vicinity of a built heritage item should not be greater than that of the heritage item. |
| D3.2 C2 | Development of a larger scale is allowable only if the new development will not be seen when viewing the heritage item from the public realm. |

Part D4 - Development in and in the vicinity of Heritage Conservation Areas

The strong Federation to Inter-war character of some of the retail buildings in the Victoria Road Retail Conservation Area should be retained with their masonry facades, often intact shopfronts, upper floor verandahs, parapet forms and overall consistent and intact streetscape setting. Some key controls for development in and around the conservation area are as follows:

| Controls | |
|------------|--|
| D4.1 C1 | The side and front setbacks of new development in and in the vicinity of a conservation area should be similar to the spacing of contributory buildings in that conservation area. |
| D4.1 C2 | New buildings should conform to the orientation pattern of existing buildings in the area. |
| D4.2 C2 | New development should follow the natural slope of ground. Cuts, excavations or infill of natural ground levels should be limited to 1m. |
| D4.7 C2 | Garages should not be incorporated into the building in a conservation area. |



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View towards the low point at Edwin Street



Recent development response to slope on western side of Victoria Road

3-1 Landform and topography

Topography plays a significant role in shaping the character of places. The study area is laid over two ridges and one valley. The northern ridge is located close to the alignment of Lyons Road and the southern ridge is located between Church Street and Day Street. The low point between these two ridges generally follows the alignment of Edwin Street.

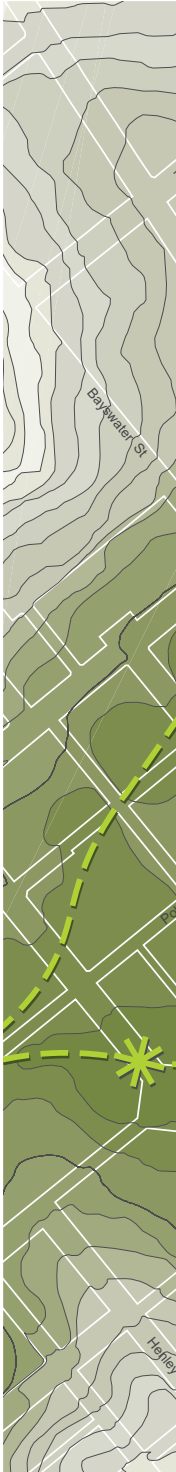
Lyons Road, which generally follows the northern ridge, is predominantly level which makes pedestrian access easier and results in shops that are level with the street. Victoria Road, the major road in the study area, travels over both ridges and rises and falls following the landform. Some sections are steep making pedestrian access difficult and limiting access to commercial and retail development, due to the difficulty of stepping developments to follow the slope.

The southern ridge rises steeply from the harbour to the south and east. The dramatic changes in elevation emphasise the visual impact of taller buildings and at times also contribute to increased overshadowing impacts. Land to the south of the study area, around Park Avenue and approaching the Iron Cove Bridge, is relatively flat, but falls steeply towards Brett Park to the south-west and rises to the north-east.

At local high points there are street level views towards the water and the Iron Cove Bridge and Gladesville Bridge. Taller buildings along high points provide views to St Leonards and North Sydney to the north-east and the Sydney CBD to the south-east.

Summary points

- High points at Lyons Road and Church/ Day Streets
- Significant slope along Victoria Road
- Local high points provide water views



PLACE ANALYSIS



Figure 13 Landform and topography analysis map





Heavy vegetation along Renwick Street adjacent to the study area



Lack of street trees or other landscape elements is obvious along Victoria Road

3-2 Open space and vegetation

There are no areas of public open space located within the study area. Brett Park adjoins the south-western corner of the study area boundary and the regional facilities of Drummoyne Oval are located to the north-west.

The lack of community gathering spaces was identified in the Drummoyne Village Public Realm Concept Master Plan in 2011. Following this plan, the changes made to the study area included new seating and additional planting at the corner of Lyons Road and Victoria Road and temporary improvements to the area outside the Commonwealth Bank on Lyons Road.

There are very few street trees within the study area, particularly along Victoria Road and Lyons Road. This is partially a result of the combination of the wide road and amount of travel lanes, and deep awnings. Recent development has tended to use shallower, lighter-weight awnings with breaks, which has allowed some new trees to be planted next to Victoria Road between Church Street and Day Street.

Formosa Street, to the south-west of the study area has some trees, including recent tree planting, but the opportunity for additional trees is limited by parking, driveways and shallow front gardens. Along Renwick Street to the north-east, outside the study boundary, there is a strong landscape character with medium to large established street trees.

Summary points

- Victoria Road is almost devoid of trees
- Recent development has provided some street tree planting on Victoria Road
- Prominent street trees present in surrounding areas

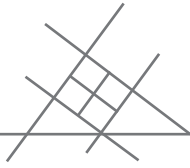


PLACE ANALYSIS



Figure 14 Urban canopy coverage analysis map





Long blocks limit opportunities for east-west movement



View of Edwin Street which narrows to 10m east of Victoria Road

3-3 Urban structure

The street pattern in the area predominantly follows a north-west/ south-east rectilinear grid. While most streets and lanes are connected, blocks in the area are generally very long and narrow. This creates limited opportunities for east-west movement across Victoria Road and the shallow depth of sites limits the opportunity for larger, integrated redevelopment.

There is a wide range of street widths within and surrounding the study area. Victoria Road is approx. 26.7m wide and Lyons Road, west of Victoria Road is approx. 21.4m wide. Lyons Road, east of Victoria Road, narrows down to 15.9m wide, Formosa Street is approx 16.6m narrowing to 11.8m near Lyons Road. Edwin Street is 18.7m between Victoria Road, narrowing to 10m east of Victoria Road and 8.9m west of Formosa Street. Renwick Street to the east of the study area and Day Street are more the common Sydney road reserve street width at 20m.

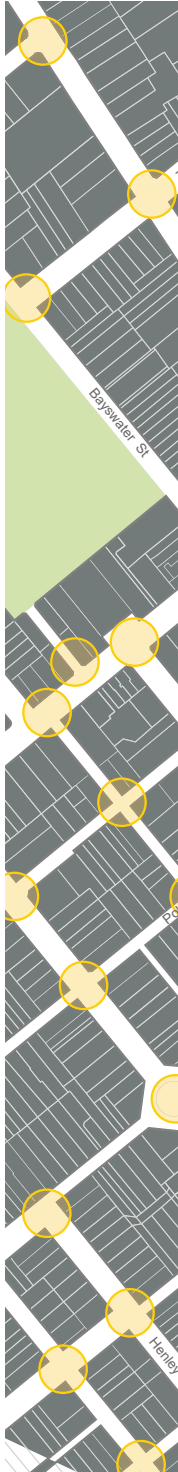
All streets within the study area have footpaths, but they are very narrow on the streets with limited available width and in these instances there is little opportunity for street trees and landscape. There are no public open spaces within the study area, although an area in private land ownership is located outside the IGA to the north of the study area.

Summary points

The wider roads, Lyons Road and Victoria Road, accommodate significant vehicular and bus traffic

Many of the surrounding connecting roads are narrow and of a width more commonly associated with laneways

Many of the blocks are long and narrow creating limited opportunities for east-west access across Victoria Road



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Figure 15 Urban block structure analysis map





- 1 Sutton Place shops
- 2 Hotel
- 3 Fire Station
- 4 Westpac Bank (former)
- 5 Shop and residence
- 6 Church of Jesus Christ
- 7 House
- 8 War Memorial Monument
- 9 Presbyterian Church & School
- 10 St Bede's Anglican Church
- 11-16 Houses

3-4 Heritage

The study area contains five heritage listed items including the Sutton Building shops, the Oxford Hotel, the Drummoyne Fire Station, the former Westpac Bank and a shop and residence at the corner of Day Street and Victoria Road. A number of heritage items are also located in the immediate vicinity of the study area. Large areas around the study area are identified as heritage conservation areas and extend into the study area around the intersection of Victoria Road and Lyons Road. The Drummoyne Post Office, located at the corner of Lyons Road and Victoria Road, is a contributory item and located within both the study area and conservation zone.

Heritage items within the study area are generally in a good condition and seem to be little altered. The group of corner shops known as the Sutton Building, which have more recent development at the rear, continue to retain the original facade and shop front elements.

Due to the presence of significant conservation areas, the built form of the wider area is diverse with taller buildings along Victoria Road quickly transitioning into low to medium density housing. Landmark heritage items in the surrounding streets include the Church of Jesus Christ, the War Memorial Monument, St Bede's Anglican Church, Presbyterian Church & School and a number of Victorian and Federation houses.

Summary points

Five heritage listed items within study area

Conservation area around the intersection of Lyons Road and Victoria Road within the study area

Heritage items generally in a good condition

Diverse built form due to the combination of growth pressures (recent taller development) and heritage items/conservation areas



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Figure 16 Heritage and conservation analysis map

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Heritage items within the study area



Sutton Building Shops, 38-50 Lyons Road

This building is a good example of Inter-War retail development. Although significantly modified internally, the façade is largely intact and provides a distinct corner treatment on this prominent site.



Hotel, 13 Lyons Road

The building is an excellent example of hotel architecture from the Inter-War period. Its location at a prominent intersection gives the building high visual presence and streetscape value.



Drummoyne Fire Station 29 Lyons Road (1910)

This facility is a relatively unaltered example of a typical twentieth century fire station.



Westpac Bank (former), 191 Victoria Road (late 1920's)

This former bank building is a good example of the Classical Revival Style. Currently utilised as a commercial premises.



Shop and residence, 64 Victoria Road

Victorian terrace with significant alterations, currently used as a retail/ commercial premises.

Key Heritage items around the study area



Reorganised Church of Jesus Christ of Latter Day Saints, 234 Victoria Road

A simple, attractive brick building built in a contemporary style in keeping with this denomination. This substantial building is located on the fringes of the study area.



House, 2 Wrights Road

This house is an eclectically styled Federation house notable for its complex roof cupola.



**War Memorial Monument
Marlborough Street**

This classic-style monument was unveiled on ANZAC Day in 1928.



**Drummoyne
Presbyterian Church**

This impressive church is an example of the late Federation Gothic Style.



**St Bede's Anglican Church
14 College Street**

St Bede's Anglican Church is an example of a 1930's church built in an individual Neo-Romanesque style and is a prominent local landmark in Drummoyne.



Due to traffic volume, signalised pedestrian crossings are important



The north-south cycling route detours off Victoria Road onto Renwick Street

3-5 Walking, cycling, public transport

The study area has limited pedestrian and cycling links. Due to the high volumes of traffic on Victoria Road and Lyons Road, the intersections appear to prioritise vehicles over pedestrians, thus making it challenging for pedestrians to cross. All pedestrian crossings of Victoria Road occur at signalised intersections, with the movable traffic barrier generally preventing crossing at any other point. There is one pedestrian-only signalised crossing located mid-block between Lyons Road and Edwin Street.

At present, an on-road cycle route exists along Renwick Street that continues along the Bay Run and Victoria Road in the south and connects to Gladesville in the north. As per the Canada Bay Bike Plan released in 2014, a cycle route is planned from Drummoyne to Five Dock via Edwin Street in the medium to long term.

Victoria Road Drummoyne is well serviced by public transport with local buses connecting to Sydney CBD, Parramatta, Macquarie University, Chatswood and Burwood. Buses run along Victoria Road and Lyons Road with some services utilising the bus turning area at the intersection between Lyons Road and Wrights Road. Metro buses servicing limited stops also operate through this area.

The Drummoyne Ferry Wharf is located north of the study area and is a 10 minute walk (900m) from the nearest bus stop at the intersection of Lyons Road and Victoria Road.

Summary points

Limited connectivity for pedestrians and cyclists within study area

Study area well serviced by local buses and within walking distance of Drummoyne Ferry Wharf



PLACE ANALYSIS



Figure 17 Walking, cycling and public transport analysis map





Attached development along Victoria Rd with continuous ground floor retail uses



Smaller scale semi-detached and attached typologies in the surrounding areas



Figure 18 Existing building heights map

3-6 Built form

The built form along the western side of Victoria Road and at the intersection with Lyons Road is characterised by one (1) to six (6) storey commercial buildings with retail uses on the ground floor. These buildings have zero setbacks to Victoria Road and provide a continuous street wall edge. This is in contrast to the eastern side of Victoria Road which has lower building heights of one (1) to four (4) storeys and a fragmented street wall edge broken by on-grade car parks at multiple locations. The typologies along Victoria Road are predominantly attached and perimeter block type buildings, with continuous ground floor retail uses.

Smaller scale detached, semi-detached and attached typologies surround the study area. Renwick Street and Formosa Street which run parallel to Victoria Road, are characterised by fine-grain detached housing of one (1) to two (2) storeys with typically small lots providing narrow frontages to the street. Due to the absence of a rear lane for the dwellings on the western side of Renwick Street, properties have predominantly on-street car parking and parking in the front setback, and very small rear gardens ranging from 4m to 10m in depth. Properties on the eastern side of Renwick Street and surrounding streets generally have rear gardens ranging from 15m to 20m in depth and rear lane access for parking. Properties on the western side of Formosa Street do not have rear vehicle access so on-street car parking and parking in the front setback is predominant here too. Properties to the eastern side of Formosa Street are either Council carparks or share a site frontage with Victoria Road, and form the transition zone to the higher development fronting the busier street.

Birkenhead Point located to the south of the study area is characterised by large-scale commercial footprints that sharply contrast the fine-grain residential character of Renwick Street.

Summary points

Study area characterised by attached, strip retail and perimeter block buildings with zero setbacks

Large-scale commercial footprints at Birkenhead Point in sharp contrast to surrounding smaller scale detached, semi-detached and attached dwellings



PLACE ANALYSIS



Figure 19 Built form figure ground analysis map





The intersection at Day Street provides prominent corner buildings



View of the six-storey residential building at 1B Wrights Road from Lyons Road



View of the six-storey 'Tempo' building on Victoria Road with a facade image of a Kookaburra

3-7 Urban characteristics

The linear core of the Victoria Road Drummoyne Centre has two key intersections, one with Lyons Road in the north and the other with Day Street in the south. Another smaller intersection occurs with Edwin Street in the centre of the study area. This access point is also regularly used by residents in the east to access Drummoyne Public School on a daily basis. The built form around these intersections serve as prominent street corners.

In addition to the five heritage listed items which contribute towards the local character of the study area and a Heritage Conservation Area around the Victoria Road/ Lyons Road intersection, a few key buildings serve as local landmarks, and are identified in the adjacent diagram. They include a six-storey residential building at 1B Wrights Road behind the Drummoyne Post Office, a six-storey commercial building at 50-52 Lyons Road behind the Sutton Building shops, the six-storey 'Tempo' building at 77-105 Victoria Road with a large 'Kookaburra' artwork on the southern elevation, and the St Bede's Anglican Church building at 14 College Street.

The study area provides a number of localised views that terminate in built form. This provides an opportunity to create visual landmarks at the terminating views. The zero-setback retail strip along the western side of Victoria Road provides a defined urban street edge. A number of commercial premises on the eastern side of Victoria Road and along Lyons Road also provide defined street edges which add to this visual containment. There is a lack of green open space and landscape elements within the study area.

Summary points

Two key intersections and one secondary intersection with prominent street corners create key wayfinding markers within the study area

Five heritage listed items, a number of contributory buildings and key landmarks add to the local character

Terminating views create an opportunity for additional visual markers

There is a general lack of green open space and landscape elements within the study area



PLACE ANALYSIS



Figure 20 Urban design characteristics analysis map



3-8 Activity analysis



The Destro's Pharmacy entry off Victoria Road



Non-active street frontage of the IGA along Victoria Road with parking and the main entry at the rear



Pedestrian connection through to Victoria Road from Formosa Street carpark

A review of the most activated places within and around the centre in 2018 shows that most of the activity occurs towards the western side of Victoria Road. This is influenced by the presence of community facilities and open spaces on the western side of Victoria Road, including Drummoyne Public School, St. Marks Catholic Church, St Bede's Anglican Church, Learn and Laugh Child Care Centre, Drummoyne Oval and the open space adjoining Henley Marine Drive. However, it should be noted that none of these community facilities are located within the study area.

Within the study area, activity is more concentrated to the north, along Victoria Road between Lyons Road and Church Street as seen in the adjoining figure, and is driven by:

- Medical centres including DMC and Da Vinci Centre
- Services including Australia Post and the Commonwealth Bank
- Retail attractors including Harris Farm and IGA
- The Oxford Hotel
- Council car parking along Formosa Street and Marlborough Street

The eastern side of Victoria Road is less active due to the absence of substantial retail, services, community facilities, public parking or desire lines to community facilities.

Summary points

A majority of activities occur on the western side of Victoria Road.

Concentration of activity within the study area occurs at the northern end of Victoria Road.

The eastern side of Victoria Road is less active.



PLACE ANALYSIS



Figure 21 Places of interest diagram





Outdoor seating at The Grind House, along Lyons Road which closes at 5pm



The Harris Farm development also has food / restaurant options that activate this cluster into the evening

3-9 Day analysis

This diagram graphically represents the ‘whole of day’ activity level across the centre. The daytime activities are focused around key locations that the community accesses such as schools, the supermarket, medical facilities, and the likely connections that exist between activities. Within the centre, the desire lines highlight retail opportunities due to passing pedestrians.

The night-time activity drivers are large restaurants and the pub, that may encourage people to visit the centre into the evening. Currently the majority of these evening venues are stand alone, with limited connection to any other locations, apart from adjacent parking. McDonald’s has also been identified, but it operates as a stand alone with very limited pedestrian access or activation of the surrounding area.

The lack of activities available on the eastern side of Victoria Road, in comparison to the western side, is stark. There is limited ‘pull’ to the east side, either during the day or into the night. None of the traditional drivers are present on the east side, apart from the Post Office.

Also apparent is the micro clustering that has occurred around public parking, or venue parking as is the case with Harris Farm, which has an associated restaurant that operates until 10 pm.

Summary points

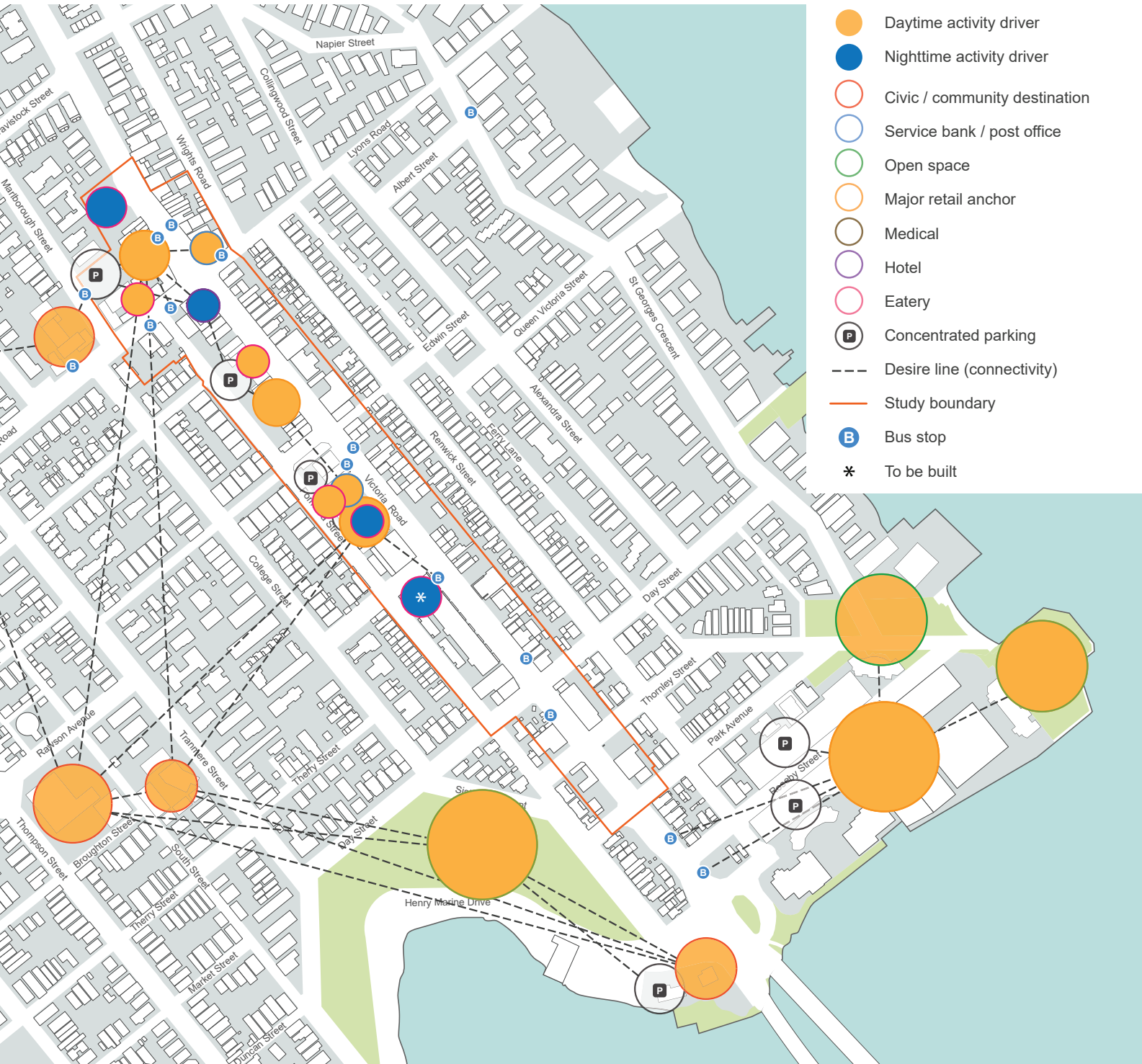
Daytime activity is focused on the western side of Victoria Road and is clustered around available parking

There is very limited night-time activity, located exclusively on the west side of Victoria Road

At all times, the availability of parking is a key driver of activity



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- Daytime activity driver
- Nighttime activity driver
- Civic / community destination
- Service bank / post office
- Open space
- Major retail anchor
- Medical
- Hotel
- Eatery
- P Concentrated parking
- - - Desire line (connectivity)
- Study boundary
- B Bus stop
- * To be built

Figure 22 'Whole of day' activity analysis diagram





Sites with recent multi-storey developments are less likely to re-develop in the near future



Heritage items such as the Presbyterian Church constrain the development opportunities of surrounding sites

3-10 Development constraints

The study area was analysed for potential future development opportunities. The adjoining map identifies the development constraints within the study area. The following types of sites were considered to constrain future development:

- Existing strata developments: due to multiple ownership and difficulty in acquiring the site.
- Recent developments, current Development Applications (DAs) and Planning Proposals: due to improbability of further development on these sites in the near future.
- Lots with heritage items, within Heritage Conservation Areas (HCAs) and at the interface with HCAs: due to limited possibilities of development within these lots.
- Contaminated sites including service stations and auto mechanics: due to increased costs associated with residential development on such sites.

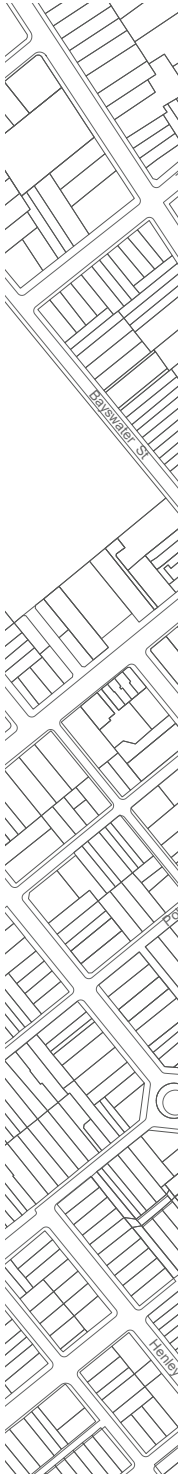
Development on the eastern side of Victoria Road is particularly challenging due to the close proximity of detached houses at the rear of these properties, which fall within the HCA, as well as the absence of a rear lane which makes access to these sites for construction and permanent parking extremely difficult.

A number of strata developments have been built recently on the western side of Victoria Road, and a planning proposal for rezoning 63-69 Victoria Road is currently in progress.

Summary points

Future development within the study area is constraint by strata ownership, recent DAs and planning proposals, heritage items, HCAs and contaminated sites.

The presence of a HCA and the absence of a rear lane make the eastern side of Victoria Road challenging for future development.



PLACE ANALYSIS

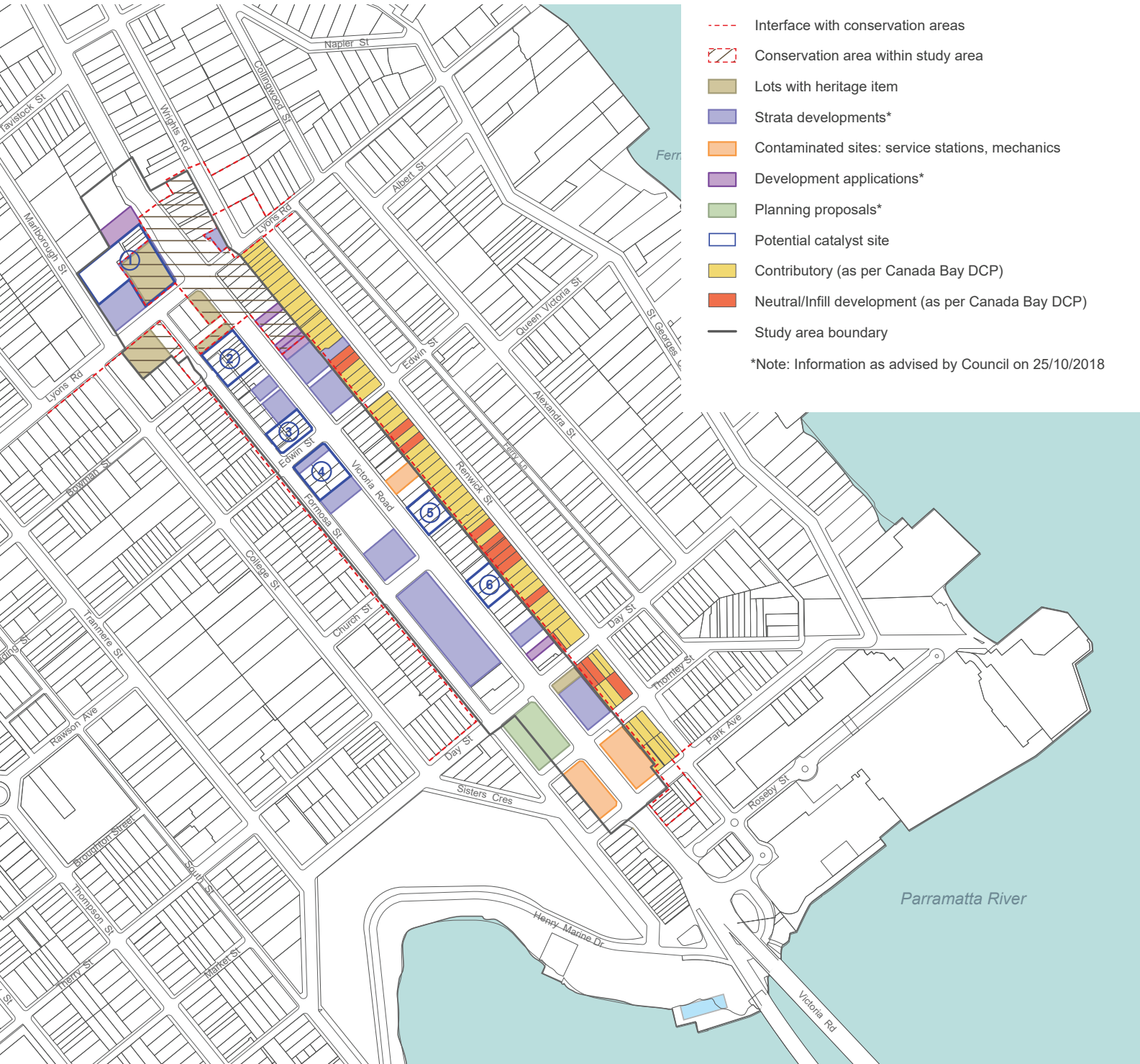


Figure 23 Development constraints diagram with 6 identified opportunity sites



3-11 Traffic constraints

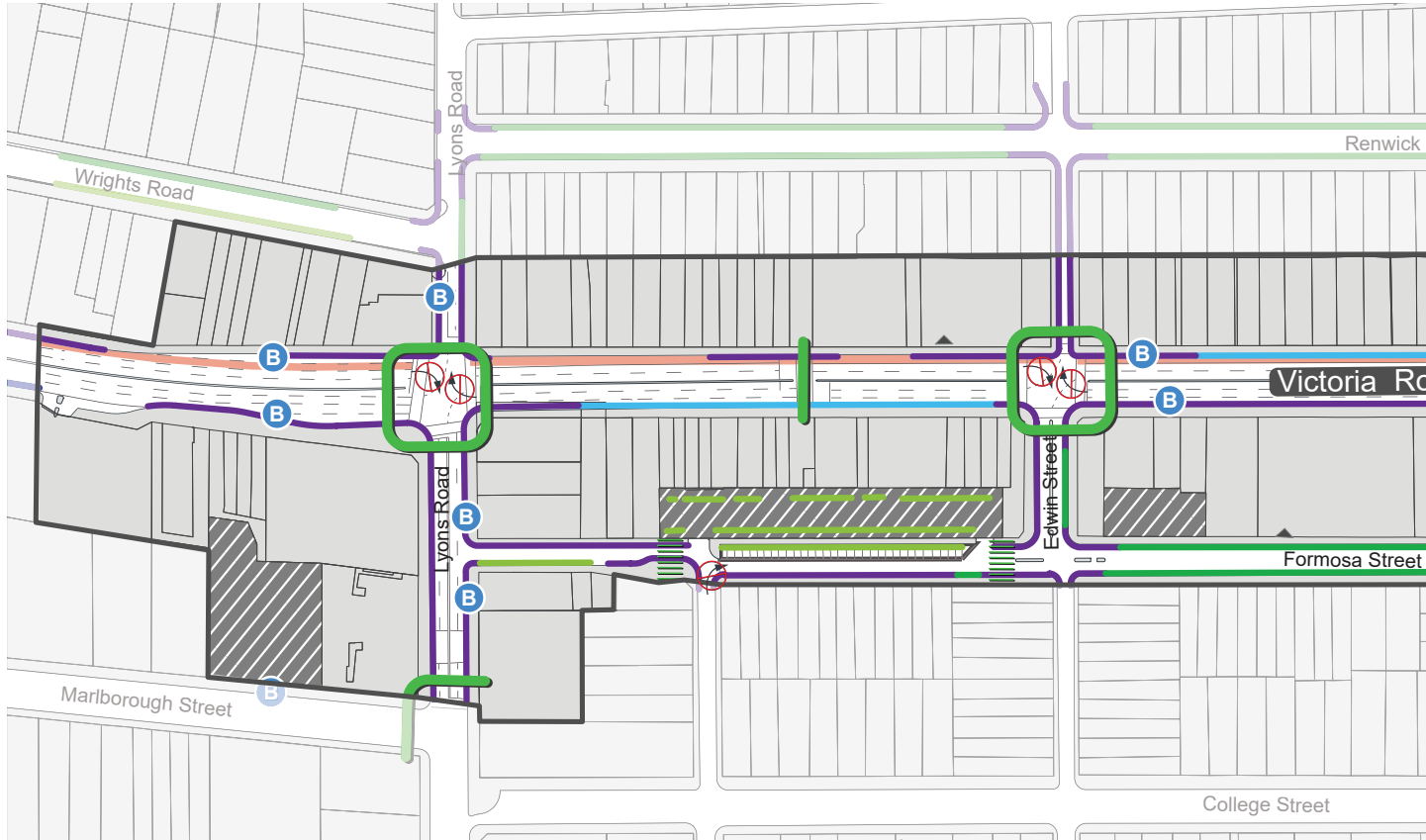


Figure 24 Traffic constraints

Parking

The study area is significantly impacted by the traffic and parking constraints along Victoria Road, Formosa Street and Renwick Street. As can be seen from the diagram, a majority of the study area has restricted timed parking. Renwick Street and Formosa Street have 2-hour parking to discourage commuter parking, whilst seating encourage visitors and shoppers who will access the centre. The community has said that these streets are often used by others to park overnight outside the timed parking hours.

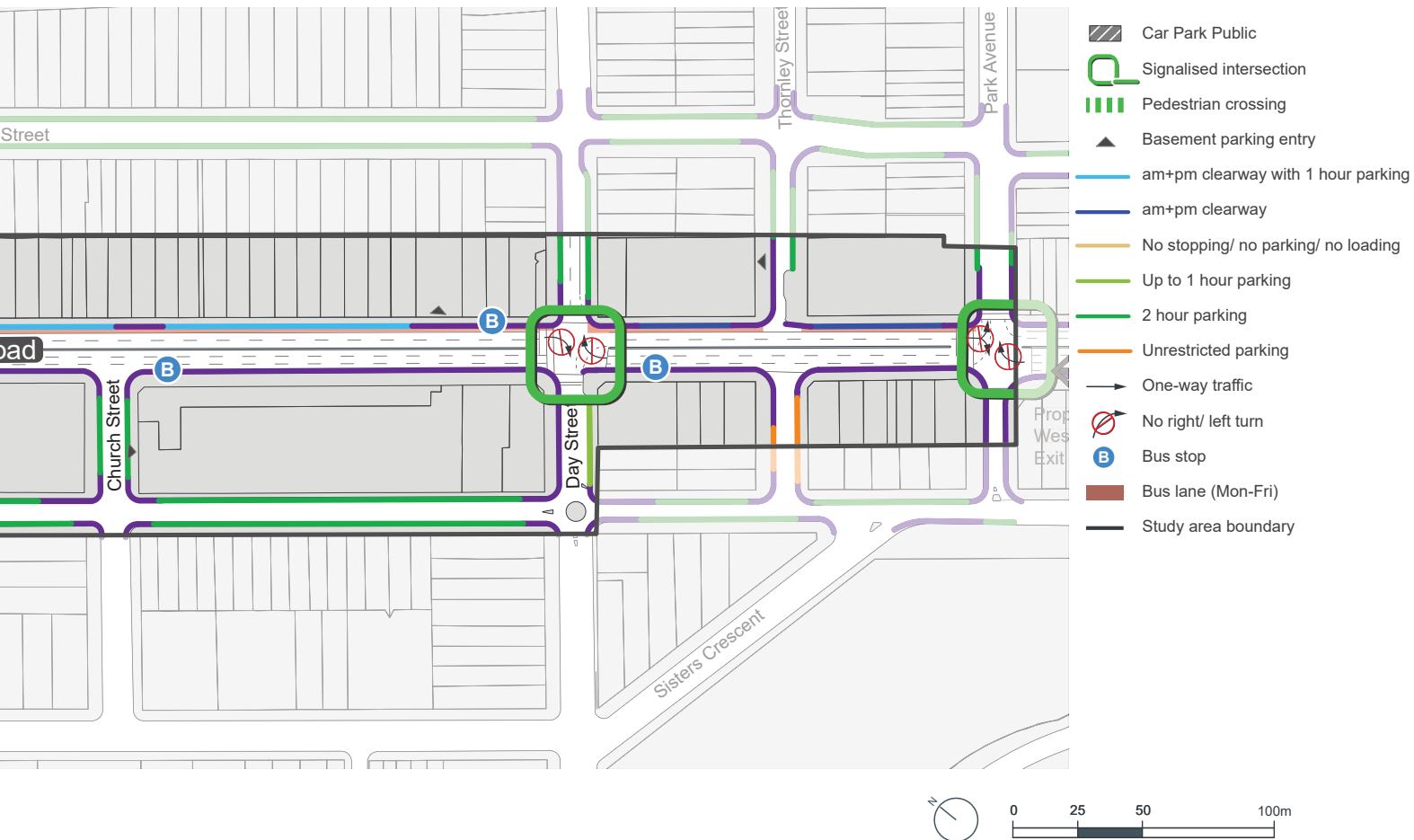
A timed dedicated bus lane is located on the western side of Victoria Road for part of the day Monday-Friday. 'No Stopping' zones and clearway zones apply at various places along Victoria Road. There is timed parking of 1/2 hour and 1 hour duration in some areas, and these parking restrictions are a strong influencer on the pedestrian and shopper experience.

Access

Due to the absence of rear lane access for properties along the eastern side of Victoria Road, some of these sites currently have vehicular access off Victoria Road. There is currently no vehicular access along the western side of Victoria Road since access is available from Formosa Street.

As per the Roads and Maritime Services (RMS) Guide to Traffic Generating Developments (2002), all developments require access from the frontage Road, to car parking and services facilities. However, direct access across the boundary with a major road is to be avoided wherever possible. For multi-modal corridors such as Victoria Road with bus lanes, busy footpaths, a moveable centre median tidal flow barrier and strategic arterial traffic flows, maintaining flow speeds are key for RMS.

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Therefore, as identified by Henson Consulting (traffic consultant), it is likely that RMS will seek to prohibit vehicular access from Victoria Road for new developments if there is any reasonable alternative. Sites which currently have vehicular access to on-site parking off Victoria Road may be required to reduce their access when they undergo development.

RMS is also likely to require construction access off side roads wherever possible, thus causing inconvenience for residents living in the immediate vicinity.

Any proposed changes in relation to traffic and parking will need to be considered in greater detail in order to fully understand the potential impacts and benefits. Currently Council is undertaking a parking study in Drummoyne to provide more understanding of the opportunities and constraints.

Summary points

Parking is highly valued and often difficult to find, for both residents and visitors.

Vehicular access to properties within the study area, off Victoria Road, is difficult and unlikely to become easier as traffic volumes increase.

3-12 Public domain constraints



Figure 25 Public domain constraints

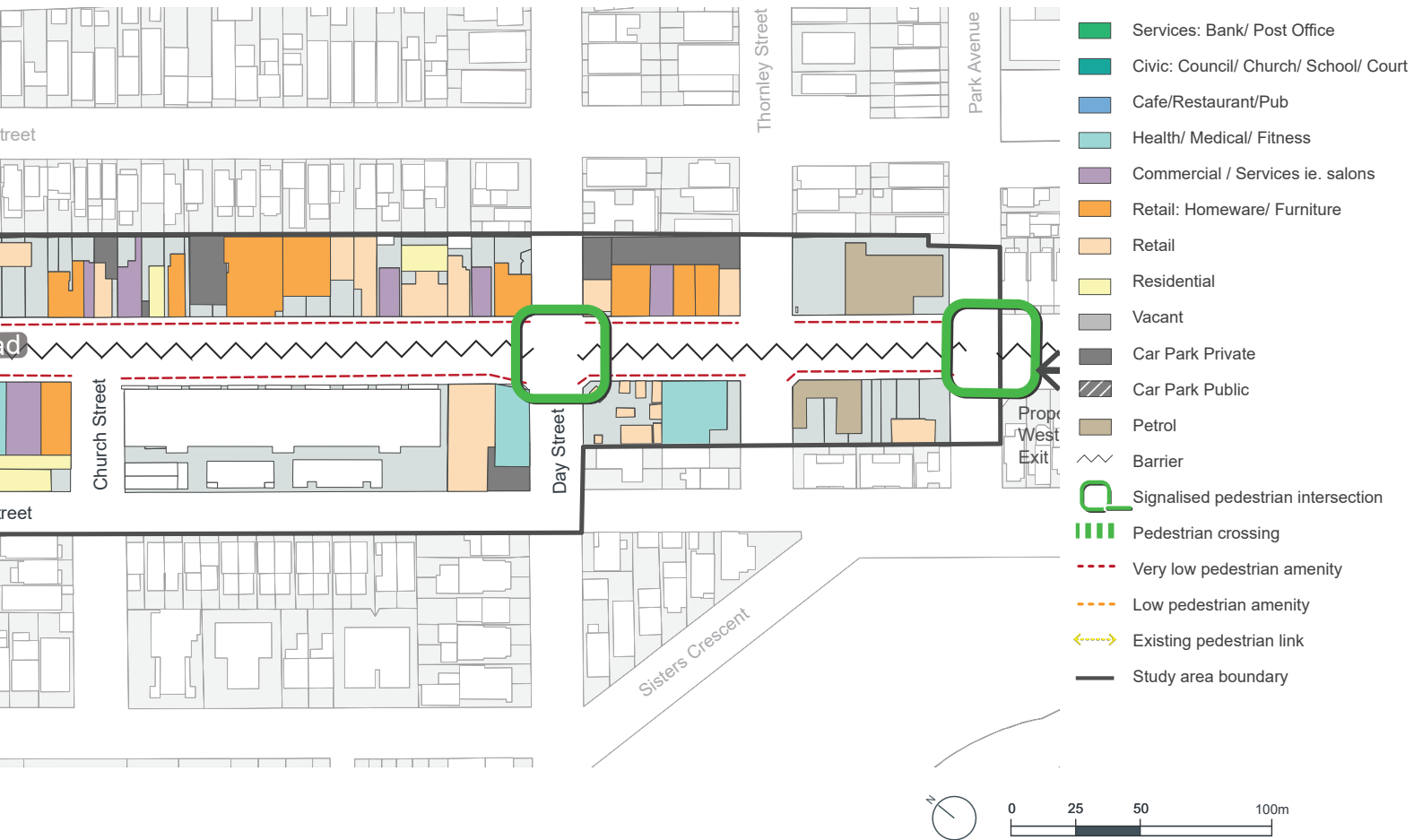
This diagram presents the current mix of uses along Victoria Road (October 2018) and the public domain conditions that affect the pedestrian experience along it. Victoria Road is a major barrier to pedestrian access further exacerbated by the movable concrete road divider.

Safe pedestrian access across Victoria Road is limited to five signalised intersections. The intersection of Lyons Road and Victoria Road is a key intersection for pedestrians, especially due to the location of a major public bus interchange. However, anecdotal evidence from the community indicates that the signalised pedestrian crossing does not provide enough time for pedestrians to cross the entire stretch of Victoria Road. This makes it dangerous for pedestrians to cross safely, and people sometimes have to dodge vehicles turning left from Lyons Road onto Victoria Road.

The Edwin Street intersection with Victoria Road is also identified as dangerous for pedestrians, especially for school children walking to the Drummoyne Public School.

Two pedestrian zebra crossings are found on Formosa Street which is a quieter, more residential street. A few pedestrian through-site links are located within the study area, usually along desire lines and between key destinations. A pedestrian link between Formosa Street and Victoria Road exists adjacent to the Da Vinci Medical Centre and another connects the Marlborough Street carpark and the IGA with Victoria Road and the bus stop. However, these have poor amenity and could be significantly enhanced by the provision of good lighting, signage, public art and activation.

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Victoria Road has a lack of pedestrian amenity due to limited street trees, inconsistent awnings and narrow footpaths which contribute to a poor pedestrian experience. The public carparks off Formosa Street create an interface which also hinders safety and accessibility for pedestrians.

Summary points

Victoria Road supports a wide array of existing uses, but also forms a significant barrier to pedestrian movement.

Pedestrian access across Victoria Road is limited to specific locations and highly controlled, so as to preference the high volume of traffic that utilizes this transit corridor.

3-13 Sectional analysis

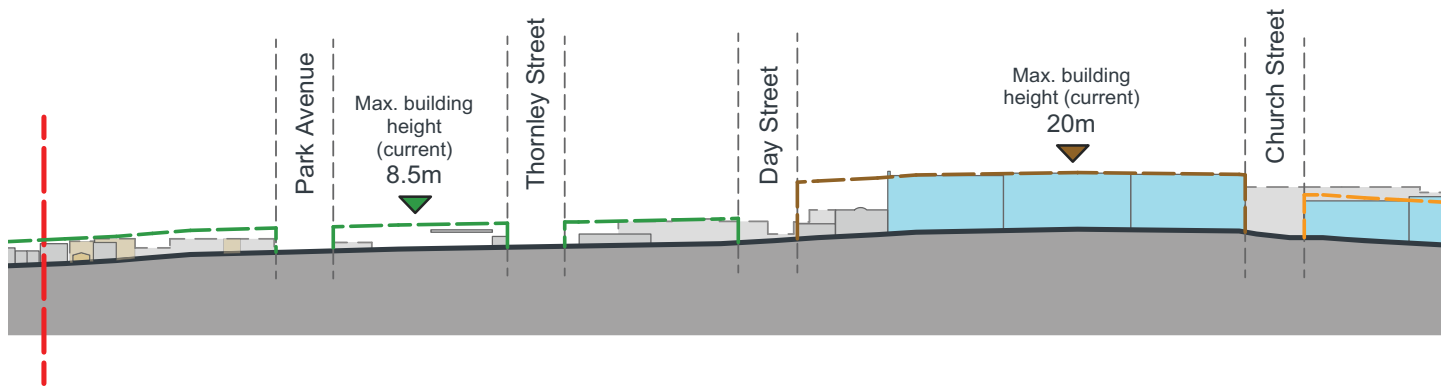


Figure 26 Section A through Victoria Road, looking south-west

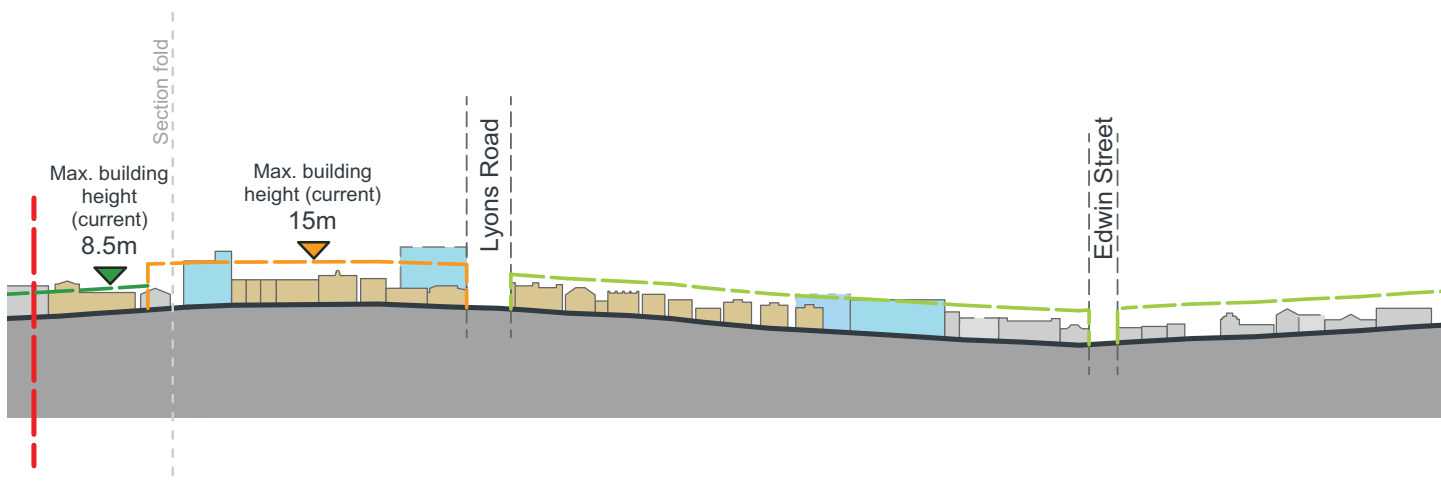


Figure 27 Section B through Victoria Road, looking north-east

Section A shows the south-west street elevation of Victoria Road. It illustrates that the maximum building height ranges from 8.5 to 20m on this side of the street. Recent 5 and 6 storey developments have been built on either side of Church Street. The built form along this side of the street is noticeably taller than on the north-east side shown in Section B.

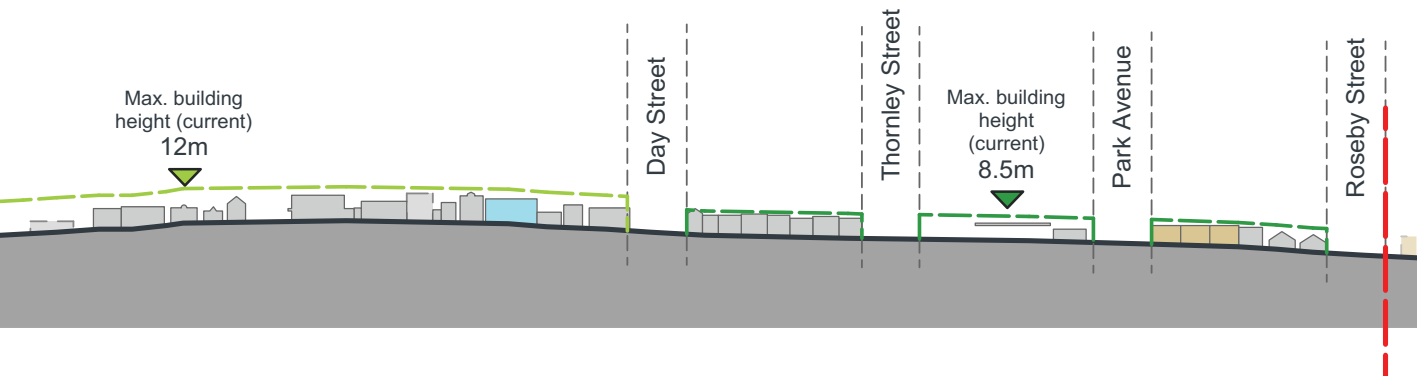
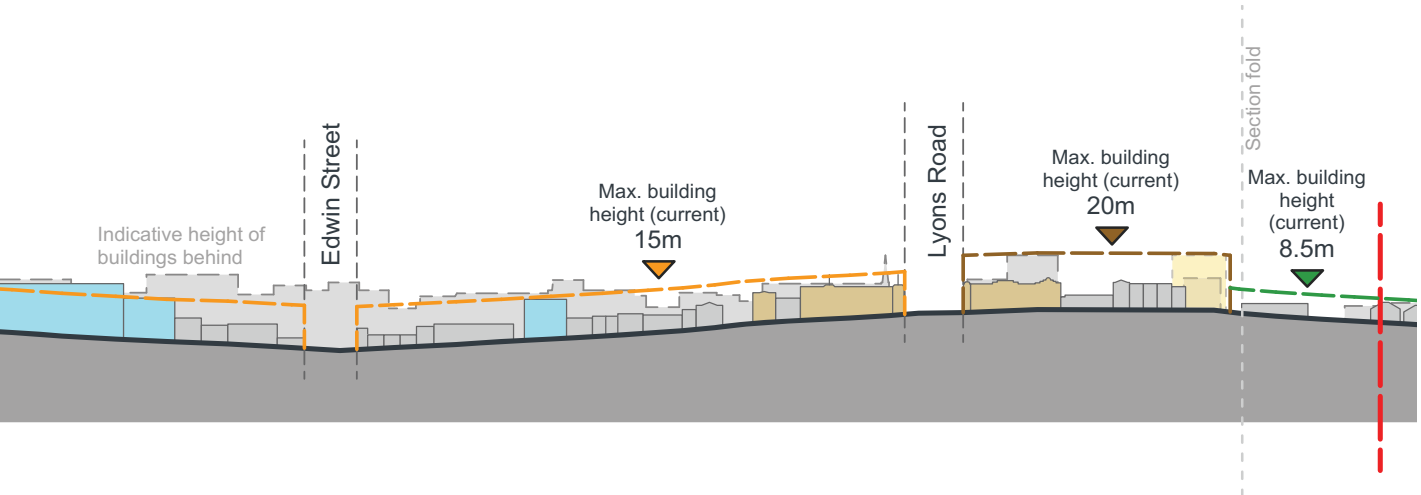
Section B illustrates a cluster of heritage listed properties at a topographical high point in the area. 15m and 20m maximum building height controls face each other at this Lyons Road intersection. The tallest building heights on both sides of the road occur at the high points or ridge-lines which further accentuates the height. Edwin Street is at a low point with lower building heights.

Summary points

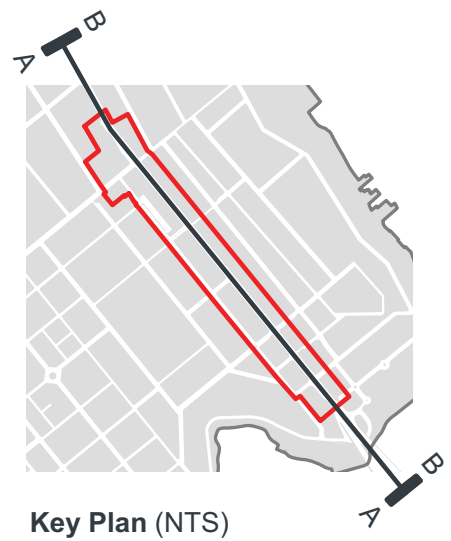
The difference in quantity of recent developments is clear, with a few large developments dominating.

The number of properties in the Heritage Conservation Area and the clustering of these properties is evident.

PLACE ANALYSIS



- Approved potential future development
- Recent development
- Heritage items/ properties within a conservation area



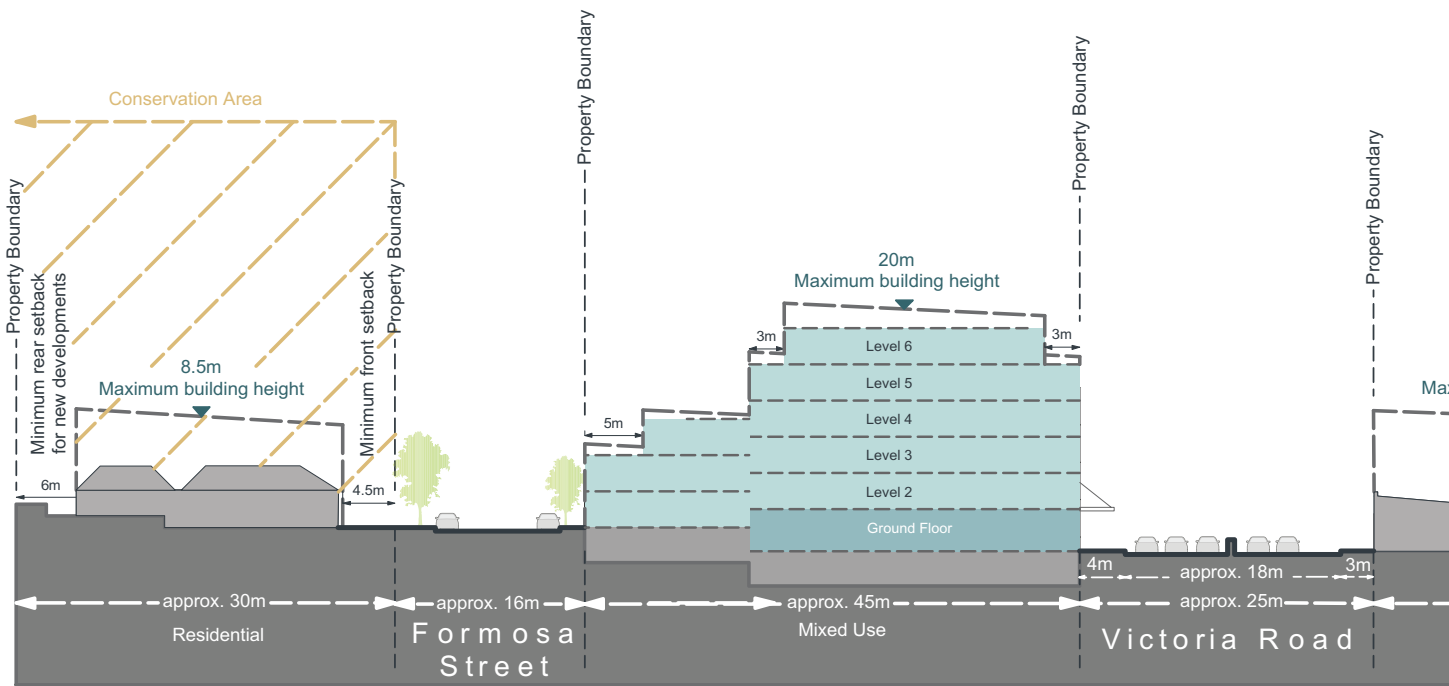


Figure 28 Section C through Victoria Road, looking north-west (western side)

Victoria Road has a wide road reserve of 25m with 18m being allocated for traffic and 7m for pedestrian footpaths. It is a six lane traffic road which includes a morning bus lane on its eastern side and a concrete road divider that is reconfigured during peak times.

Harris Farm is a popular retail anchor with five storeys of residential above. The top storey is setback 3m from Victoria Road to reduce the bulk and scale from the street. At this section of the road, awnings are present on the western side of the street along the facade of the building with Harris Farm at ground level. There are no awnings on the eastern side of Victoria Road due to the location of car-related commercial properties including car service and car wash.

Formosa Street has a road reserve of 16m and features street trees and footpaths on either side of the street. This area currently has single storey houses on its western side with an 8.5m height limit with a 5m setback on the eastern side which steps up to a 20m height limit closer to Victoria Road.

Renwick Street has a 20m road reserve with street trees and footpaths on both sides of the road. Properties on the western side of Renwick Street back on to properties along Victoria Road. Both Renwick Street and Formosa Street are within Heritage Conservation Areas.

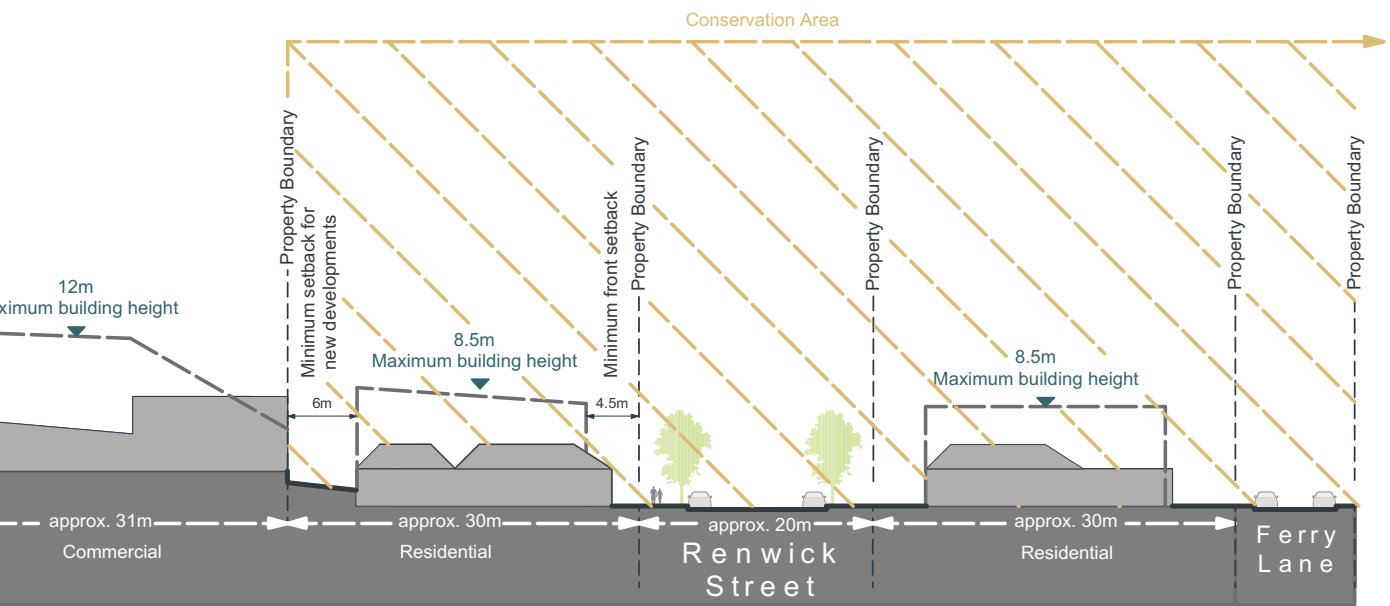


Figure 29 Section C through Victoria Road, looking north-west (eastern side)

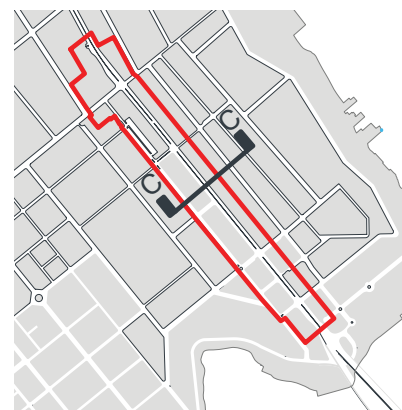
- Recent development
- Heritage conservation area

Summary points

There is a one storey rise of natural ground level between Victoria Road and Formosa Street and a one storey drop between Victoria Road to Renwick Street.

Shallow depth of lots on east side of Victoria Road.

There is an absence of any road or laneway along the rear boundary of properties on the eastern side of Victoria Road that divides the commercial zone from the residential zone.



Key Plan (NTS)



CHAPTER 4 COMMUNITY ENGAGEMENT

4-1 Summary

Introduction

As part of the Victoria Road Drummoyne Urban Design Review, stakeholder and community engagement activities were carried out in two stages: Round One – *Give us your Ideas!* in October 2018 and Round Two – *What do you think?* in March/April 2019. The engagement with stakeholders were planned to ensure that the community was aware of the limitations to development in the area and that the outcomes of consultation were realistic and achievable.

A Community Engagement Plan that outlined the consultation process in detail including who, how and when activities would occur, was submitted to Council prior to commencing the engagement.

Community Engagement Objectives

The community engagement program for the Victoria Road, Drummoyne – Urban Design Review considered the following objectives:

- Involve the community in determining the future of Victoria Road, Drummoyne;
- Use traditional media and social media to maximise awareness and participation;
- Provide information about the planning process in suitable formats for non-technical experts to use in the decision-making process;
- Inform the community of the site constraints & limitations of the project;
- Include innovative strategies for engagement, including targeted consultation for special groups and demographics;
- Get unique insider perspective from local experts on the target area along Victoria Road; its assets, strengths and weaknesses;
- Encourage participants to generate ideas that while being inspirational, are realistic and achievable; and
- Generate excitement about the project and achieve stakeholder alignment and consensus.



Promotion material

Stakeholders

The stakeholder groups for Victoria Road Urban Design Review were to involve where possible:

- Decision makers – Councillors, Council officers, State Government, Roads and Maritime Services
- People and groups with a (financial) interest in the CBD - property owners, tenants, business owners
- Regular users - people who live or work in Drummoyne
- Occasional users - people who visit/use the facilities in the target area
- Other groups and communities which are active in the area:
 - Community Reference Group
 - Youth & Children who live in the target area or go to Drummoyne Public School or St. Mark's Catholic School
 - Seniors who live in or visit the area.

4-2 Round one process

Round one of the engagement process aimed to introduce the project to stakeholders and the community, raise awareness and encourage participation in engagement activities to ensure valuable community and stakeholder input into the process. This stage focused on understanding what people like, what they don't like, their ideas for improvements and/or changes within the study area and to develop a deeper understanding of the key issues affecting this area.

Community members were invited to be actively involved in the consultation, in person and online, and their ideas, comments and aspirations were collected by the design team at Studio GL. A summary of the engagement outcome from these events is found below.



Consultation displays

Community Drop-in Session

A drop-in session, held in the forecourt outside IGA, allowed people to provide their input and ideas. An estimated 100 people gave their input during this event. We also received 21 completed surveys.



Drop-in session at the forecourt outside the Drummyne IGA

COMMUNITY ENGAGEMENT

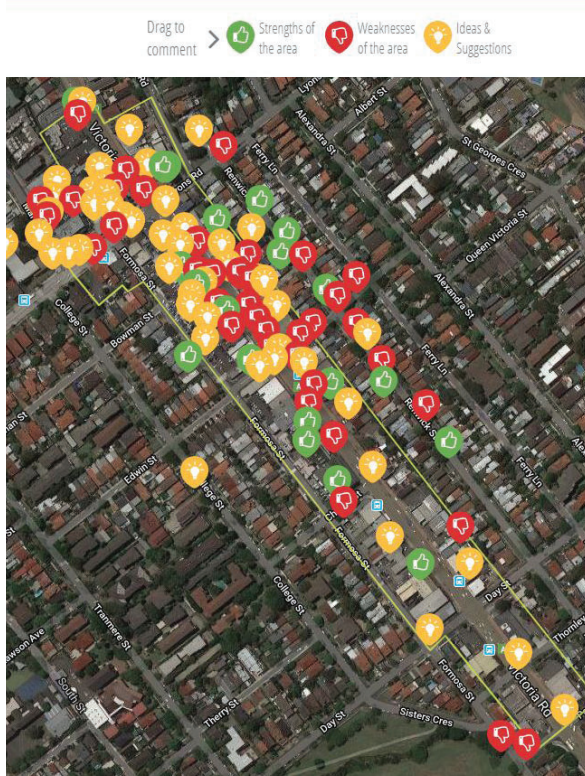
Community & Stakeholder Workshop

The stakeholder workshop held at the Greg Davis Stand at Drummoyne Oval followed an enquiry by design format and provided a more focused opportunity for the community to discuss specific issues and share their vision for Victoria Road Drummoyne through a variety of activities including placecheck mapping, ideas post-it boards, a travel routes map and a visual character exercise. 40 people registered for the event and 30 people participated in the workshop. We also received six completed surveys.

Studio GL team also conducted a workshop with the local Community Reference Group (CRG), which was attended by 8 members. The workshop collected ideas and concerns from the CRG members for the future of the area, and are included in the consolidated engagement findings. Detailed minutes from the CRG meeting are included in Appendix A.



Community and Stakeholder Workshop



Online mapping screenshot

Online Consultation

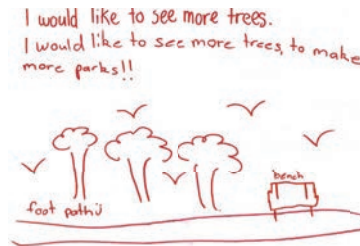
In addition to the in-person consultation an online mapping interactive tool, Social Pinpoint, was used as a method to gather community input. On this website community members could identify places and features they considered strengths and weaknesses, as well as share their ideas for improving the town centre. 248 people visited the website out of which 44 people submitted 107 comments.

Youth & Children Consultation

The design team approached local schools and encouraged them to participate in the engagement. Students on the Student Representative Council (SRC) from Drummoyne Public School participated in an hour long consultation. They were encouraged to draw their ideas for what they would like to see on Victoria Road. A survey was also issued with questions relating to how students interact with their town centre. In total 50 students from grades K through 6 submitted 36 drawings and 29 survey responses.



Youth & Children Consultation



Drawings from students



Intercept Survey Session

Intercept Survey

The design team conducted an intercept survey session (research method used to gather on-site feedback) at the Drummoyne Community Centre to allow for input from senior residents in the community. The session was attended by 20 senior community members. In addition to a number of comments, we received 17 completed surveys.

4-3 Round one findings

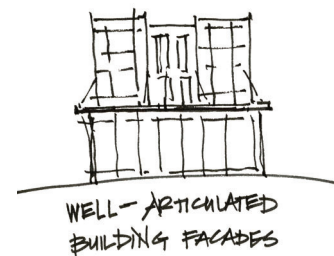
The engagement activities presented at each consultation event were designed to gather community feedback in a variety of forms. Activities like the placecheck map and travel map provided place-based information where the participants marked areas of concern, their commute routes, places with future potential and other important information on the maps.

The Visual Character Aspiration Boards provided a collection of images of built forms, public domain, streetscape, etc., that indicated what

the participants would like to see in the study area in the future, as well as building forms or public domain characteristics they do not think appropriate for their centre.

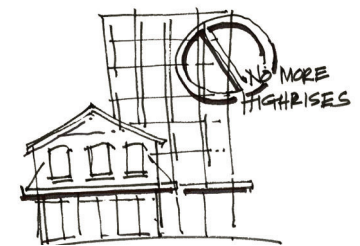
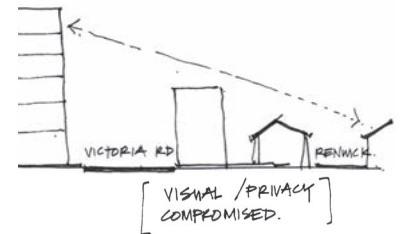
In addition to this the participants submitted hundreds of comments on what they thought about the strengths & weaknesses of the centre as well as their ideas for the future character of the place. Following is a summary of the most repeated community responses collated into three categories: strengths, weaknesses and ideas.

| What are the strengths of this area? |
|---|
| Heritage charm, character and a sense of place - heritage buildings in the area including commercial, churches and beautiful houses |
| Plaza in front of IGA is the closest thing we have to a town square |
| Access to the Bay Run & waterfront |
| Health benefits of public amenities in the area |
| The area is pedestrian friendly and safe |
| Harris Farm is a great asset and has created a community feel in the area |
| As Victoria Road is a key street, transport to CBD or western suburbs is great |
| Heritage homes in Drummoyne are some of the best in Sydney |
| Buildings along Victoria Road are a good mix of old and new |
| The pedestrian connectors through buildings work well |
| A central Post Office is good |
| People are friendly and interested in buying and promoting local businesses |
| Formosa St is safe & often used to access the medical facility along Vic Road |
| Strong community feel |
| Love the planters with poetry on them |



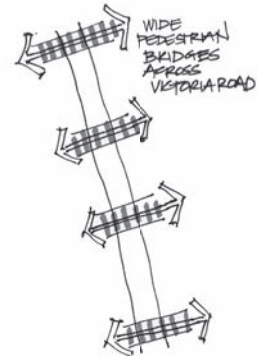
COMMUNITY ENGAGEMENT

| What are the weaknesses of this area? |
|--|
| Dangerous intersections for pedestrians: Lyons Rd, Edwin St & Park St |
| Carparks are insufficient for the number of users & need improvement |
| Parking system needs to be reevaluated - 1hr (near medical centre) or all day parking (at Birkenhead & for business owners on Formosa) is not preferred |
| Edwin St is dangerous cycle route - esp at the school start & finish times |
| Lack of vision for Victoria Rd has left it ad hoc & it has lost its community feel |
| Traffic on Formosa St is a mess - most people don't realise/obey one ways |
| Traffic flow across Victoria Rd is difficult |
| Vehicles turning right on Victoria Rd have a very long wait |
| Lack of safe cycleways |
| No more development - Do not overdevelop into a dormitory suburb |
| New development is too tall & affecting quality of life for adjacent properties (privacy, sun access, visual, overlooking, shadowing, parking & traffic) |
| The infrastructure of Victoria Rd is not capable of handling more development |
| Height restrictions on new developments should be strictly adhered to, to minimise the pressures of people and cars on already overloaded streets |
| New development needs to respect & respond to existing adjacent heritage buildings especially Renwick St as there is no access Rd between development & heritage homes, and topography makes new buildings seem taller |
| Vacant shops are not desirable - shops closing due to lack of access for visitors & landbanking by land owners |
| New development does not seem to follow the DCP regulations |
| Shops along Vic Rd have the feeling that all you see is the "back" of shops |
| Victoria Rd needs trees and more greenery |
| Drummoyne lacks a gathering place/passive recreation space away from the busy Victoria Rd |
| Pedestrian environment along Victoria Rd is not desirable; footpath needs repairs |
| Footpath in front of the post office is too narrow and feels unsafe |
| Garden beds and new planting is all weeds and poorly maintained |
| IGA frontage to Victoria Rd is a blank wall, and two of three pedestrian entry points to Sutton Place are blank-walled tunnels |
| Edwin St presents a number of issues for motorists and pedestrians |
| Trees that were removed in Edwin St, College St have not been replaced |



COMMUNITY ENGAGEMENT

| What are your ideas & suggestions to make this place better? |
|--|
| Ped bridges/underpasses across Vic Rd at key intersections; esp. Lyons Rd |
| Need more pedestrian connectors like at IGA - the one between chemist & Radiologist (with coffee shop) could be developed into a nicer space |
| More parking and policing of parking times - reconfigure parking in back streets, build parking garages (Formosa St/ IGA) |
| New development should provide parking for its businesses and residents |
| Retain and upgrade the existing village character of Drummoyne |
| No increase in height limits for new development |
| Do not reduce or change the heritage conservation areas |
| Heritage facades and facades of buildings in heritage conservation areas should be retained and integrated in new development |
| Limit heights on eastern side of Victoria Road to three-four storeys |
| Encourage night-time activity - restaurants and bars with healthy food options |
| Side facades need to be addressed; kookaburra - good, blue panels - ugly |
| Encourage small local businesses - no more kitchen stores |
| Vic Rd is too noisy for outdoor seating - consider mixed uses on cross streets |
| Need more shade (trees and continuous awnings) in the town centre |
| Need more trees on footpaths particularly along Victoria Rd & Lyons Rd - to soften the aggressive traffic environment, mitigate noise & slow traffic |
| Use public domain to display community history through furniture & public art |
| Need a small square or plaza in the town centre with more colour & vitality |
| Consider closing Church St and making it a public park/plaza |
| Install planters along Vic Rd to serve as barriers between pedestrians & traffic |
| 189 Victoria Rd has always been an empty lot - could become a public plaza |
| With ground space limited, develop public open spaces at upper levels |
| Consider closing Formosa St from Lyons Road to Bowman and convert to a public plaza with outdoor dining |
| Improve IGA forecourt and make it into a village centre |
| Improve Edwin St with better safety, more pedestrian amenity & canopy trees |
| Victoria Road needs more colour and vitality in its public domain design |
| Mixed zoning on nearby cross-streets such as Day St, Edwin St, Marlborough St |
| Senior mostly visit the Lyons Road Vic Road area for all their needs. Any new services related development will need to consider access for seniors |



4-4 Round two process

The second phase of the engagement process involved presenting the short-term and long-term ideas & strategies under consideration for Victoria Road to the community and stakeholders.

The materials for these presentations were in the form of A1 sized posters, clearly indicating the options being reviewed, some built examples of the ideas and the specific issues to be considered, along with space for feedback & comments. A brief summary of the outcomes from Engagement Round One was also included.

Using the facilitation technique of dot-voting, the participants were asked to give their opinion on the presented ideas using green and red dots to determine preferences; green being for projects they liked and would like to see move forward, and red being for ideas they did not like. They were also provided an opportunity to present any new ideas they might have for the place. Discussions were facilitated to determine public opinion on the design components and options.

The outcomes of this engagement stage are included in this chapter in brief and in the Appendices in detail. These indicate how well the design components were received by the community and their comments on the ideas.



Consultation display posters

COMMUNITY ENGAGEMENT

Community Drop-in Session

A drop-in session, held in the forecourt outside IGA, allowed people to provide their feedback on the presented ideas. Again an estimated 100 people gave their input during this event in the form of green and red dots on the different ideas as well as a number of comments on post-its. Additional ideas for the study area were also received.

Community & Stakeholder Workshop

The stakeholder workshop held at the Greg Davis Stand at Drummoyne Oval provided a more focused opportunity for the community to discuss the range of ideas and strategies put together by Studio GL. For this engagement the ideas were presented in a more detailed format where Studio GL staff went through each concept and discussed it briefly.

This allowed for a better understanding of what each concept entailed, thus allowing the participants more clarity in their voting using the green and red dots. 16 community members participated in the workshop. Council also received a few emails following the workshop with comments and feedback.

The Studio GL team also held two meetings with the CRG as a part of Engagement Round Two. The first meeting, attended by 8 members, was held prior to the community drop-in and workshop. In this meeting the members were introduced to the draft future development ideas proposed for presentation to the community. The second meeting involved reporting to the CRG members the community feedback on the presented ideas. Detailed minutes from both CRG meetings are included in Appendix A.



Drop-in session



Community and Stakeholder Workshop

4-5 Round two findings

Community members voted on the ideas and strategies using green & red dots, and provided comments to support their vote. In general, there was consistency in the voting between the Drop-in session and the workshop. There were a few ideas, however, which might have been misunderstood in the drop-in session, where there was no opportunity to present the ideas in detail, given the nature of a drop-in consultation. As a result there was a difference in votes for those ideas between the votes collected from the drop-in and the workshop.

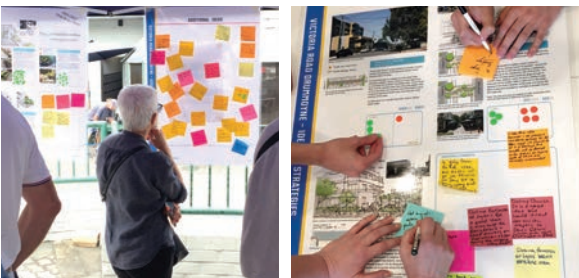
The number of votes (combined votes from the drop-in session and the workshop) and key comments for each idea are presented on the following pages. A more detailed listing of the voting for each event and a complete list of all comments received are included in the Appendices section.

Top Ideas liked by the community (highest number of green dots):

- Public Space - Closing Church Street
- Public Space - Closing Formosa Street
- Streetscape upgrade - Along Victoria Road
- Streetscape upgrade - Along Formosa Street
- Pedestrian links - Upgrade existing links and add new links
- Night time economy - Along Victoria Road
- Tunnel under Victoria Road

Top Ideas disliked by the community (highest number of red dots):

- Building heights - Update LEP controls for east side of Victoria Road
- Building heights - Additional heights along west of Victoria Road
- Privacy screening - Update DCP controls
- Public Art - On building façades and public spaces



COMMUNITY ENGAGEMENT

| No. | Ideas & Strategies | Green dots | Red dots | Key Comments from drop-in session and workshop |
|----------------------------|---|------------|----------|---|
| Access and Movement | | | | |
| 1 | Car parking- Multi-deck parking on Council site | 28 | 6 | It should go at least two storeys underground to reduce the bulk |
| | | | | Could include a bike storage shed |
| | | | | Multi-deck car parking could create more traffic |
| 2 | Car Parking- Traffic Management | 9 | 15 | If this is enforced, residents should be allowed at least two parking permits |
| | | | | Current parking limit of 1 hour in Sutton Place discourages shopping - increase to 2 hours |
| | | | | Not good for those who don't live in the area and like to park here and take the bus to the CBD |
| 3 | Intersection upgrade- Between Victoria Rd and Lyons Rd | 15 | 2 | Countdown is a good idea |
| | | | | Need dedicated right hand turn green arrows from Lyons St to Victoria Rd |
| 4 | Cycle EOT facilities- At corner of Edwin St and Victoria Rd | 15 | 12 | EOT is at the wrong end of CBD commute |
| | | | | Day St on east side of Victoria Rd might be a better location |
| | | | | A safe, wide and pleasant cycleway to the city and improved bike paths are more important to the locals |
| | | | | Should be located on east side close to Bay run, Birkenhead, and local cycle routes |
| 5 | Pedestrian links- Upgrade existing links and add new links | 27 | 0 | Pedestrian links in the area need an update |
| | | | | Owner of Sutton Place has neglected the premises |
| 6 | Safer crossings- Overpass at Edwin St intersection | | | Considered but not progressed |
| 7 | Safer crossings- Widening Edwin St | | | Considered but not progressed |
| 8 | Tunnel under Vic Road Drummoyne | 36 | 11 | Focus on public transport /metro and not cars |
| | | | | Destroying local areas for the sake of the car |
| | | | | Better to make Victoria Rd pedestrian friendly with slower speed, pedestrian crossings, etc. |

COMMUNITY ENGAGEMENT

| No. | Ideas & Strategies | Green dots | Red dots | Key Comments from drop-in session and workshop |
|----------------------|--|------------|----------|---|
| Public Domain | | | | |
| 9 | Public Space- Closing Church St | 36 | 6 | <p>This reduces the ability to enter and exit Drummoyne</p> <p>A good idea but it would direct too much traffic to Day St & Edwin St, which are already quite busy</p> |
| 10 | Public Space-Closing Formosa St | 40 | 5 | <p>Would help fix problems of cars driving the wrong way in the one way sections of Formosa St</p> <p>College St is already a no-entry from Lyons Rd, and this would be a further no access</p> |
| 11 | Streetscape upgrade- Along Formosa St | 30 | 1 | <p>Shops to front Formosa St between Lyons Rd and Edwin St, making it centre of pedestrian activity</p> <p>Underground power lines should be a condition of consent</p> <p>Add cycle lanes and bike racks</p> |
| 12 | Streetscape upgrade- Along Victoria Rd | 38 | 0 | <p>Incentivise shop owners to water and maintain the planters</p> |
| 13 | Street trees- Along Formosa St, Renwick St and Wright St | 25 | 0 | <p>Need more trees!</p> <p>If street trees are planted, Council needs to care for them, unlike the case with new plantings in the area</p> |
| 14 | Bus Shelter | 23 | 2 | <p>Need commuter friendly bus shelters that 'shelter' from rain and sun, with glass that provides view of approaching buses when seated</p> <p>Need a bus shelter at the post office</p> |
| 15 | Awning controls- for the entire study area | 10 | 9 | <p>Awnings co-existing with trees is a good idea - with No Stopping zones, shoppers have to walk on the footpath in all weather conditions</p> <p>Awnings combined with trees, if well-maintained, could have a big impact on improving Victoria Rd as a walkable area</p> <p>Shopkeepers prefer continuous awnings to prevent stock damage</p> |

COMMUNITY ENGAGEMENT

| No. | Ideas & Strategies | Green dots | Red dots | Key Comments from drop-in session and workshop |
|-------------------|---|------------|----------|---|
| Activation | | | | |
| 16 | Showcasing heritage- Along Victoria Rd | 22 | 2 | Preserve the heritage |
| | | | | Refurbish heritage buildings rather than demolish and build new |
| | | | | Highlighting the old will look wrong with modern units behind it |
| 17 | Showcase lighting-for the entire study area | 12 | 1 | New street light poles needed |
| | | | | Improve current street lighting that is hidden in trees |
| 18 | Night time economy- Along Victoria Rd | 30 | 1 | Use solar lighting where possible |
| | | | | Develop Formosa St as the hub of Drummoyne with cafés and restaurants |
| 19 | Public Art- On building façades and public spaces | 9 | 18 | Most locals dislike the Kookaburra mural |
| | | | | More pedestrian scale and interactive artworks needed in Drummoyne |
| | | | | New public art should not clash with heritage features |
| | | | | Shouldn't be used to justify reduced open space in new developments |
| 20 | POPS- Privately Owned Public Spaces | 25 | 1 | Good idea - provided the POPS are not associated with additional height approved for buildings |
| 21 | Sutton Place Community Hub | 21 | 1 | Development shouldn't be too tall - could block views for neighbouring apartment buildings |
| | | | | Include a green village square and replace the tree that was cut down a few years ago |
| | | | | It should be focused on the community aspect and not on being a commercial/ retail shopping centre |
| | | | | A good spot for a metro station |
| 22 | Smart Poles - Along Victoria Rd | 15 | 7 | Waste of money - update poles and use funds elsewhere |
| 23 | Signage & Advertising - Along Victoria Rd | 10 | 3 | Reduce visual clutter |
| | | | | Helps emphasise that this is a local area and that despite the traffic, shops & locals take pride in their area |

COMMUNITY ENGAGEMENT

| No. | Ideas & Strategies | Green dots | Red dots | Key Comments from drop-in session and workshop |
|--------------------|--|------------|----------|---|
| Development | | | | |
| 24 | Building heights- Update LEP controls for east side of Victoria Rd | 7 | 65 | No increase in heights |
| | | | | The sunken floor is greed not an issue with planning heights |
| | | | | Using planters to raise sight lines is a good idea |
| | | | | If there is an increased height limit, developers will push further, from 13.5m to 15-16m |
| | | | | Council needs to approve development within their own rules |
| 25 | Building heights- Additional heights along west of Victoria Rd | 6 | 57 | Keep height limits as they are |
| | | | | Development currently exceeds planning controls - an increase to heights will lead to an increase in height waiver requests |
| | | | | The charm of Drummoyne is its heritage feel and the community vibe - high rise buildings take this away |
| | | | | Why did Council approve an apartment building without parking? |
| | | | | Prefer lower heights with staggered setbacks |
| 26 | Privacy screening- Update DCP controls | 9 | 15 | Good but should not be used to justify further height to new development |
| | | | | View into rear gardens and homes is invasive |
| | | | | Stop approving high rises - that will prevent overlooking issues |
| | | | | Sounds good but only if DCP controls are adhered to |
| | | | | Please limit height of apartments; We do NOT want beautiful Drummoyne to turn into high-rise ugliness |

COMMUNITY ENGAGEMENT

| Selected additional ideas & comments |
|---|
| Need libraries and community run facilities like Community Day Care |
| More pedestrian scaled public art |
| Planter boxes, gardens along cafés/shops on Lyons Rd between Marlborough and Victoria Rd |
| Encourage more shops/cafés/retail rather than industrial tile shop; lots of empty run-down shops |
| Need a music venue - place for music events and festivals |
| Solar powered developments - at least public spaces - community coming together to put solar |
| Existing shopping centre looks tired - needs an updated. Retain courtyard. |
| Need pedestrian overpass at Edwin St - safe for children and elderly |
| Two lanes turning right from Lyons Rd to Victoria Rd |
| Make the signage for the turns onto Victoria Rd from the south much further back; so the drivers know which lane to use |
| Weekend clearways to reduce traffic gridlock |
| Provide setbacks along Victoria Rd above 3 storeys |
| Make provision for a Metro north station |



CHAPTER 5 FUTURE CHARACTER

5-1 Character framework

This chapter identifies the Desired Future Character for the centre focused around Victoria Road, Drummoyne and the design principles that will help deliver this vision. Identification of the future character has been informed by the community values and priorities and the overarching drivers. The vision is also informed by a detailed analysis of the elements that contribute to the existing character.

The design principles are the critical next step in establishing the framework for the centre. The principles are both strategic (responsive to policy) and spatial (responsive to place).

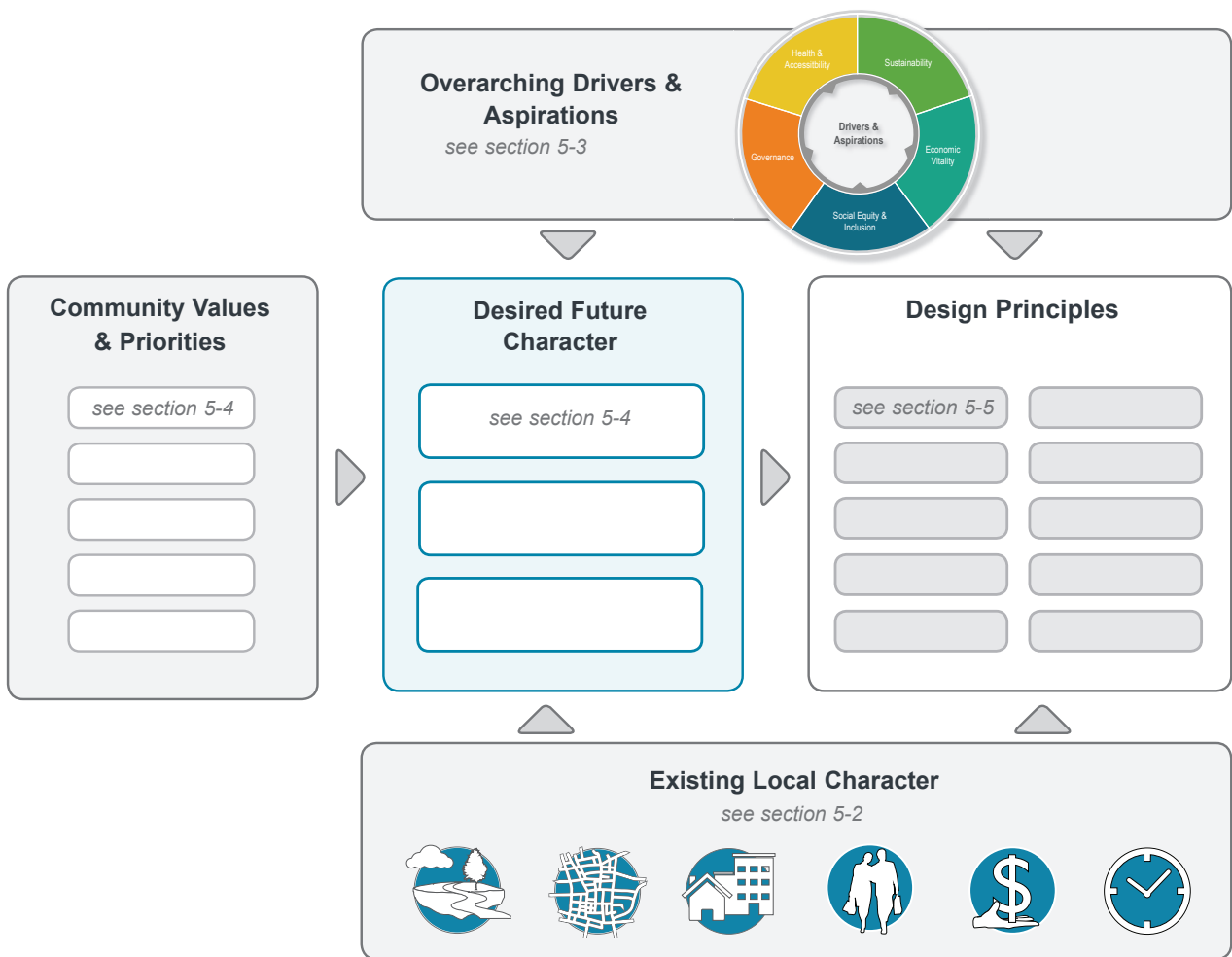


Figure 30 Diagram explaining the relationship between the desired future character, design principles, drivers, community values and the existing local character

5-2 Existing local character

THE LAND



The centre is located on a peninsula that juts into Sydney Harbour. The character of the centre is influenced by the topography, which drops significantly from two local high points. One ridge is located close to the alignment of Lyons Road and the other southern ridge that is located between Church and Day Streets. The low point occurs generally along the alignment of Edwin Street. The southern ridge rises steeply from the harbour to

the south and east, and although the area south of the study area is relatively flat on approach to the Iron Cove Bridge, it falls steeply towards Brett Park and rises to the north east.

Victoria Road, the major road that crosses the study area, travels over both ridges and rises and falls following the landform, this results in challenges for access and development as it attempts to step to follow the slope.

URBAN FORM



The street pattern in the area predominantly follows a north-west / south-east rectilinear grid. Whilst most streets and lanes are connected, blocks in the area are generally very long and narrow, resulting in limited opportunities for east/west movement across Victoria Road and shallow sites, which limit opportunity for larger developments.

There is a wide range of street widths within and surrounding the study area. Victoria Road is almost 27m wide, Lyons Road, west of Victoria Road, is 21.4m

wide. Edwin Street is the narrowest, being 10m east of Victoria Road. All streets within the study area have footpaths, but they can be narrow with little opportunity for street trees and landscaping.

There are no public open spaces within the study area, although there is a small area of private open space located outside the IGA at the northern end of the study area.

BUILT FORM



Development along Victoria Road is generally more intense than that in the immediate vicinity. The built form is one to six storeys in height along the western side and at the intersection with Lyons Road, with retail uses on the ground floor. With zero setbacks these buildings form a continuous street wall edge, which contrasts to the buildings along the eastern side of Victoria Road which have a lower height, of one to four storeys, and a fragmented street wall edge broken by multiple areas of on-grade car-parking.

Smaller scale detached, semi-detached and attached building typologies surround the study area. Many of these surrounding streets are protected by a heritage conservation area listing. Birkenhead Point, which is located to the south of the study area, is characterised by large-scale, former warehousing which has been adaptively re-used as an outlet shopping centre.

PEOPLE



Census 2016 identified the median age of the population of Drummoyne at 40, which is slightly higher than the median for both NSW and Australia. The population was 11,950 on Census night, with the Community Strategic Plan (2016-2036) forecasting a small increase over the next twenty years. Overall the population has a low unemployment rate of 3.6%, and a higher than average median weekly income. The Eastern District Plan identified Victoria Road as a City Serving Transport Corridor and Drummoyne as a Local Centre, serving the local community.

The consultation and engagement process undertaken as part of the Urban Design Study helped to capture the community's observations of day to day life in the centre. It identified key community values including the existing sense of community and the desire to protect the heritage nature of the conservation areas surrounding the study area.

ECONOMICS



Census 2016 identified that Drummoyne has a highly educated local workforce, with almost 40% of residents holding a Bachelor Degree or higher. The majority of residents leave the area to work, although just over 5% work from home. The most common occupation for working residents was Professionals (36%), Managers (22.2%), Clerical and Administrative Workers (13.3%), Sales Workers (8.7%) and Community and Personal Service Workers (7.2%).

The Drummoyne centre provides a number of destination retail locations, such as Harris Farm Markets and Whitworths Marine, along with stores catering for local residents such as the IGA. There is also a strong presence of shops that support residential renovations, such as bathroom and kitchen showrooms.

TIME



Drummoyne has a rich history, as evidenced by the heritage items and the well established heritage conservation areas established to protect sections of consistent detached single residential development. As is occurring across Sydney, there is increased pressure to provide additional housing and this is resulting in new developments being primarily higher density apartment blocks. These developments are situated along Victoria Road, due to the existing well serviced, public transport corridor.

Over time Global Mega Trends will trickle down to areas like Drummoyne. A CSIRO report "Our future world, Global megatrends

that will change the way we live" (2012) identifies six interlinked trends. Trends relevant for Drummoyne include: More from less (using the earth's limited supply of resources more efficiently); Going going gone? (climate change and impact on ecological habitats); Forever young (the challenges and benefits of an ageing population); Virtually here (the impact of increased connectivity on access to services, shopping and working and on labour markets, retail and transport) and Great expectations (the rising demand for experiences over products). These trends will see changes occurring that have impacts all across Sydney, including Drummoyne.



5-3 Drivers and aspirations

Overarching drivers help to ensure that key principles including health, accessibility, community interaction and sustainability are reflected in the strategic framework and future built form of the centre. They lead to design principles that reflect the needs of all user groups and support community health and wellbeing. For the centre the following five key drivers have been identified.

1. Health & Accessibility

An efficient road structure allows people and goods to easily access key areas within the centre and access sufficient parking that does not compromise the quality of the pedestrian environment. The environment encourages walking and cycling which contributes to the social and physical health of all residents.

2. Sustainability

Environmentally sustainable design is effective, energy efficient and high performing. It is adaptable and durable and mitigates adverse environmental conditions such as high winds and provides shade in summer and access to winter sun. The public domain incorporates opportunities for landscaping and street trees.

3. Social Equity & Inclusion

The needs of all user groups is reflected in the design of the public domain and civic spaces. Informal gathering spaces help to activate the centre and contribute to the life of the community. The design of buildings and spaces ensures passive surveillance occurs along pedestrian access routes.

4. Economic Vitality

The centre provides the focus for employment and business opportunities. Revitalisation and reinvigoration of the retail functions, services and community facilities provide compelling reasons to work in and visit the centre.

5. Governance Structure

Effective governance processes are established to ensure clarity in decision-making and achieve specific aims in the centre over time. Outcomes are realistic and achievable for the community.

5-4 Future character

The future character of Victoria Road Drummoyne was developed by considering the strategic and spatial context, studying the existing local character, gathering input during community consultations, and by factoring in the universally applicable external drivers, to provide a vision for the future of Drummoyne.

Community values and priorities

Preserving the heritage character of the area and respecting the existing low-rise scale of development adjacent to the eastern side of Victoria Road, especially along Renwick Street, were some of the most stridently expressed ideas that come from the community. The community also placed value on the sense of community and the convenience of the location, and the mix of retail and commercial offerings, such as the Harris Farm Markets.

Desired future character

The adjacent statement has been developed based on the input from the local community and the findings of the literature review and place analysis.

"The Drummoyne centre is an attractive centre, with a plaza or open space at its heart. It respects and celebrates the heritage items within the centre, as well as the adjacent conservation areas.

It is a comfortable and attractive place to access and walk around, with a sustainable mix of retail and commercial premises that provide activation into the night, throughout the week. New development is respectful of the interface with adjacent areas. Sufficient parking is provided for all users".

5-5 Design principles



Design Principles

Ensure new development responds appropriately to the challenges of the urban structure (ie block size, block depth, lot width) and the undulating landform.

Retain / reinforce the fine grain comfortable human scaled built form.

Identify opportunity to develop an open space in the centre as a focus for the community.

Ensure new development is designed to mitigate the impacts of Victoria Road and maximise the benefits of this well used route.

Encourage new development and adaptive re-use that retains and supports the character of the area.

Provide development controls that ensure new development complements the scale of the centre, the surrounding streets and the heritage buildings.

Celebrate and highlight the local heritage character and encourage the refurbishment of existing heritage buildings.

Locate taller building forms where they minimise impact on the surrounding low rise heritage character.

Improve access across Victoria Road, for pedestrians and vehicles and encourage new pedestrian linkages to Victoria Road.

Provide landscape 'buffer' between pedestrians and Victoria Rd and plant street trees wherever possible to mitigate the impact of traffic and congestion.

Utilise colour and vibrancy in street landscaping and improve the quality of the pedestrian environment. Incorporate public art.

Create a more bicycle friendly environment in the centre. Improve active transport connections to key open space areas such as the Bay Run.

Ensure new development is designed so that the diversity of commercial and retail facilities is maintained and expanded.

Encourage developments that provide facilities for night time uses such as restaurants and small bars.

Identify potential late night venues (ie shopping, library, galleries) and encourage development that provides these facilities.

Support the appropriate development of key catalyst sites to strengthen the centre.

Ensure the planning controls provide an improved interface between development on Victoria Rd and on adjacent residential streets, such as Renwick St.

Consider impacts such as solar access and visual privacy of new development on adjacent existing residential development.

Encourage amalgamation of sites, to provide additional benefits (such as new links) and sufficient space for a transition in scale to lower density areas.

Recommend design excellence standards for larger developments.

Where possible maximise vehicular and construction access off Victoria Rd (subject to RMS approval).

Encourage provision of sufficient parking within a development for retail, commercial and residential uses.

Manage occasional parking on adjacent residential streets, via time limited parking.

Increase provision of public parking and encourage developments that have reduced vehicular use (ie car sharing) to provide options such as car sharing.



CHAPTER 6

ACTIVATION INITIATIVES

ACTIVATION INITIATIVES

6-1 Introduction

The following chapters present the implementation initiatives recommended for the successful revitalisation of Victoria Road Drummoyne. Activation initiatives, along with planning framework modifications influence both the physical design (hard infrastructure) and how the place is managed, governed and experienced (soft infrastructure).

This section of the report responds to the three most important considerations within the Design Principles for Victoria Road Drummoyne: retaining the character and heritage; improving the public domain; and revitalising the retail.

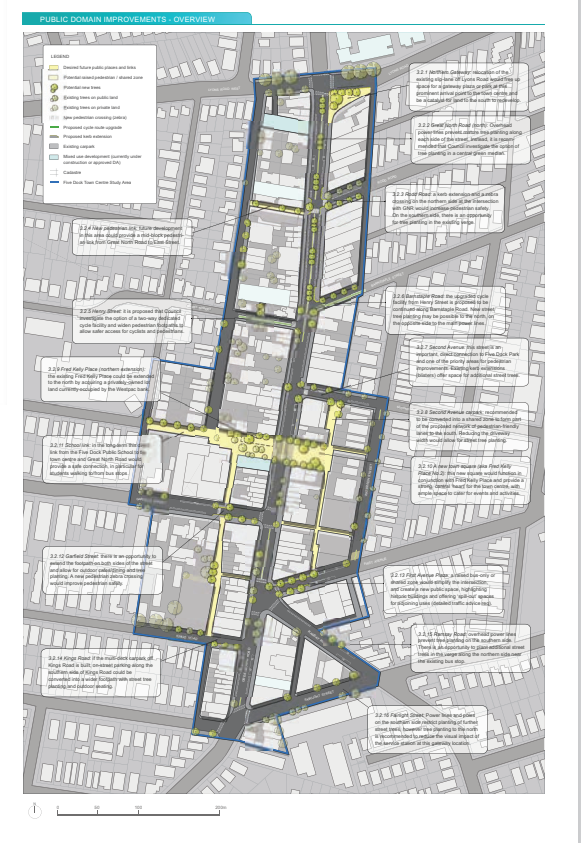
The table below summarises all the proposed strategies for the revitalisation of Victoria Road Drummoyne; the Activation Initiatives, the LEP and DCP Modifications and the Parking & Access Improvements. This chapter, Chapter 6 focuses on the activation initiatives while the next chapter, Chapter 7 presents the planning framework modifications. Chapter 8 outlines the suggested Parking & Access Improvements.

| | Retaining Character and Heritage | Improving the Public Domain | Revitalising Retail | | |
|------------------------------------|----------------------------------|-------------------------------------|------------------------------------|------------------------------|----------------------------|
| Activation Initiatives | A-01 Street trees | A-04 Victoria Road upgrade | A-10 Sutton Place community hub | | |
| | A-02 Showcasing heritage | A-05 Public space on Church Street | A-11 Multi-deck council carpark | | |
| | A-03 Formosa Street upgrade | A-06 Public space on Formosa Street | A-12 Privately owned public spaces | | |
| | | A-07 Upgraded pedestrian links | A-13 Night-time economy | | |
| | | A-08 Public art | | | |
| | | A-09 Smart light poles | | | |
| | LEP Modifications | L-01 Active frontages | L-02 Building heights (east) | L-03 Building heights (west) | |
| | | DCP Modifications | D-01 Building heights and setbacks | D-04 Street interface | D-08 Facades and exteriors |
| | | | D-02 Heritage and conservation | D-05 Landscape quality | D-09 Awnings |
| D-03 Bulk, scale and floor heights | D-06 Showcase lighting | | D-10 Advertising and signage | | |
| | D-07 Sustainable design | | | | |

Parking & Access Improvements

- P-01 Car parking management
- P-02 Bus shelters
- P-03 Cycle EOT facilities
- T-01 Intersection upgrade
- T-02 Tunnel project

6-2 Public Domain Plan



Example public domain improvements overview for the Five Dock Town Centre

Any strategy only becomes effective if and when it is implemented. Once the community has agreed on a direction, action needs to occur in order to build support for change. At its essence, revitalisation is dependent on the level of commitment of stakeholders and the community, combined with the support and encouragement provided by Council's plans and policies.

To support action, the vision for change needs to be reflected in the physical design (via a Public Domain Plan), how the place is governed (via LEP & DCP controls) and how it is managed and experienced (via an Activation Plan).

To successfully deliver the proposed physical improvements, especially the initiatives from A-01 to A-09, it is recommended that a Public Domain Plan be prepared by Council. This plan would need to be prepared for the entire area by a suitably qualified consultant and the plan would need to include a strategy for how the works are to be staged/phased. This would ensure that the priority works could be completed within a relatively short timeframe. The plan would also enable Council to work with developers so that a consistent, high quality, public domain is delivered across the entire centre.

The Public Domain Plan will be supported by an Activation Plan which provides the steps for temporary activation and/or street closures to be prepared by Council. Temporary activation and rapid prototyping will ensure ideas can be reviewed and refined in "real time" (see page 92).

6-3 Activation Initiatives

A-01 Street trees



Plan of existing and potential future street trees along Renwick, Formosa and Wright Streets



Quality shade provided by large canopy street trees



Street trees improve walkability between 'active' uses

Victoria Road is a car dominated, major transport route with narrow footpaths and limited opportunities for street trees, making it unattractive as a pedestrian area. Adjacent streets have a more appealing atmosphere, supported by a significant number of heritage buildings and contributory items in heritage conservation areas. These side streets, specifically Formosa Street, Renwick Street and Wright Road, offer a more pleasant pedestrian experience, and alternative access to the facilities on Victoria Road.

To improve the walkability across the entire Victoria Road, Drummoyne business core, and to make it more appealing for pedestrians to access shops and services, it is recommended that additional street trees be planted to increase the tree canopy. A street tree masterplan could be developed to implement this.

This idea received good support from the community, who generally expressed the opinion that increased greening of this area would be of benefit. The responsibility and cost of maintenance was raised as a concern.

Estimated costs

Tree planting: \$750 per tree

Part of Public Domain Plan: \$200,000

Note: Costing is indicative only and subject to tree selection and size

A-02 Showcasing heritage



Examples of information boards to highlight the history of a heritage area

Heritage items and contributory items within heritage conservation areas provide a link to the history of a place. Victoria Road, Drummoyne at the intersection with Lyons Road, has a pocket of these items that provide a point of difference along what is otherwise a relatively common non-descript streetscape. These buildings include the Sutton Building, the Oxford Hotel and the former bank building on the west side, and a range of smaller contributory items on the east side.

Showcasing the heritage in this area includes consideration of appropriate external paint schemes, sympathetic building signage, and provision of additional information signage. The use of external lighting, to highlight the architectural features of these buildings, would also improve their visibility and contribution to the local area.

The community was strongly supportive of this initiative. The historic charm of Drummoyne was a key element valued by the wider community and seen as an essential component of the character of the area.

Estimated costs: \$ variable

A-03 Formosa Street upgrade

The presence of a tangle of power lines along Formosa Street is visually intrusive and limits the options to plant significant street trees, and also reduces the overall amenity of the area.

One option would be to underground the power lines, removing the impediment completely. An alternative is to 'bundle' the lines, so that their impact is reduced. Either of these options would enable recently planted trees to reach maturity, without the unfortunate and extensive cropping required to prevent damage to power lines.

Additional tree planting, based on a street tree masterplan, could also be undertaken in conjunction with modifications to the street kerbing to allow landscaped 'blisters' to be installed at the various intersections. The overall intent is to provide additional landscaping throughout this area, which would result in improved amenity and walkability.

The community was very supportive of this idea, and proposed extending the upgrade to include bicycle parking racks and identified cycle lanes. Some also suggested that this type of upgrade should be a condition of consent on new development.

Estimated costs



\$50,000 approx. for minor landscape upgrades

Part of Public Domain Plan: \$200,000

Note: Costing is indicative only and dependant on final design and engineering requirements. If power lines are required to be relocated underground then the cost will be significantly higher.



Example of a Living Landscape Blister (with street tree) integrated with street parking

-  Possible new street trees
-  Possible landscape 'blisters'

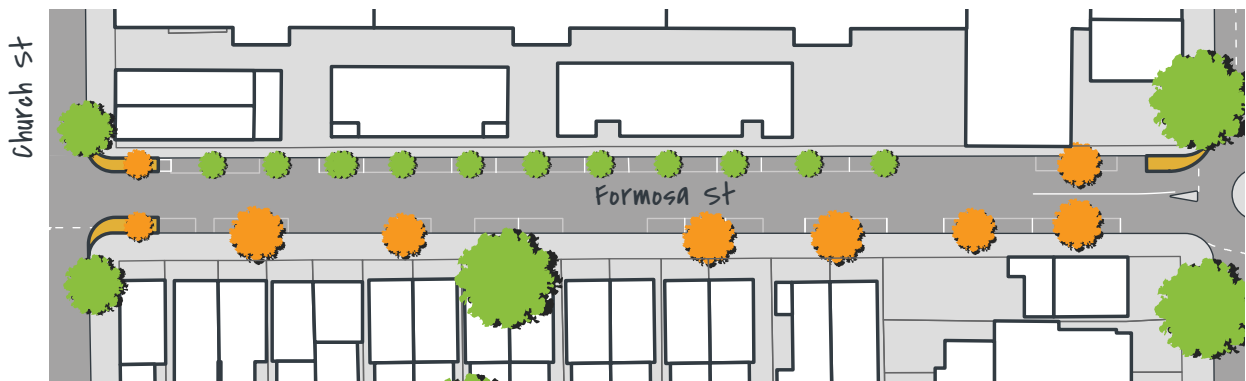


Figure 31 Plan of potential landscape 'blisters' and new street trees along Formosa Street

A-04 Victoria Road upgrade



Existing situation

A key issue for Victoria Road, Drummoyne is the extensive hard surfaces. Overall the pedestrian experience is unpleasant, there is a lack of greenery and the footpaths are in poor condition and are a patchwork of inconsistent materials, colours and textures. Recent installation of extensive 'No Stopping' zones along the west side of Victoria Road has increased this loss of amenity, as pedestrians are now immediately adjacent to active traffic lanes at all times along significant lengths of the footpath.

It is recommended that low planting be installed where the 'No Stopping' zones are, to provide some relief from the traffic, and to improve the safety and amenity of pedestrians who choose to walk in this area. The provision of additional planting, in the form of 'green walls' or planters fronting Victoria Road would provide additional visual relief and enhance the pedestrian experience.

A footpath upgrade program is also recommended, to improve the surface finishes of all footpaths along Victoria Road. Updating of the DCP controls, to require developments to undertake these upgrades as part of their projects, to Council's specifications, would allow this to occur incrementally.



Figure 32 Artist's impression

ACTIVATION INITIATIVES



- 01 Landscape treatments along 'No Stopping' zones and threshold treatments improve the 'look & feel' of the street
- 02 Improved surface finishes with standard colour and texture encourage pedestrian movement
- 03 Wall planting provides visual relief in the otherwise concrete dominated Victoria Road streetscape



The community was very supportive of this idea, but there was some concern that these improvements would not be maintained. Previous experience has indicated that upkeep is not carried out, so the landscaping fails and garden beds and planters become a place for cigarette butts and rubbish. If upgrades are undertaken, there needs to be a clear understanding on how the landscaping will be maintained.

An example streetscape is Military Road in Mosman which has a similar cross section, six lanes of traffic and traffic volumes, where street trees and planter boxes have improved the pedestrian environment.

Estimated costs

\$3-4 million approx. for footpath upgrade

Note: Costing is indicative only and dependant on final design including materials, landscaping, scope of work, engineering requirements, renewal of Telstra pits and traffic control etc. Works can be staged and undertaken incrementally.

A-05 Public space on Church Street



The business/ retail core on Victoria Road, Drummoyne is primarily a linear traffic corridor, that provides limited opportunities for the provision of a community gathering space, which was a key element desired by the local community.

This recommendation explores the option to create a public space, via the partial or complete closure of Church Street, between Victoria Road and Formosa Street. The resulting pedestrian space would be large enough to contain trees, seating, public art and a children's play area, and would encourage cafes to spill out into the space, adding to the activation and amenity of the local area.

Additional traffic studies would be required to determine what the ideal option would be. It may be possible to consider a hybrid option that would involve a timed directional flow, whereby traffic could access Church Street from Victoria Road at certain times, but at other times there would only be access out of Church Street, onto Victoria Road. Temporary activation (refer to page 92) could be a highly effective way of testing options.

Overall the community was supportive of this concept, although there was concern about the potential impacts on traffic flows, and possible increases in congestion on other streets. The need for detailed traffic analysis was considered essential.

Estimated costs

Part of Public Domain Plan: \$200,000 approx.

Note: Costing is indicative only and dependant on final design and engineering requirements.

Figure 33 Artist's impression

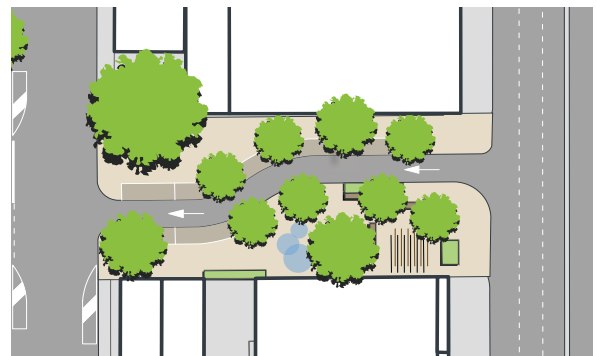
ACTIVATION INITIATIVES



- 01 New public space on Church Street to provide a 'green square' character to Victoria Road
- 02 New street trees to enhance amenity of the centre and encourage pedestrian activity
- 03 Water features for noise attenuation from busy Victoria Road



Proposed public space on Church Street



Proposed alternative option for Church Street with public space and one-way traffic from Victoria Road

A-06 Public space on Formosa Street

Formosa Street, between Lyons Road and Bowman Street, is currently one way south, it then transitions to being one way north between Bowman Street and Edwin Street, with traffic heading south being diverted into a parking area to the east. This convoluted traffic pattern, whilst resulting in significant available on-grade parking, results in a range of issues including illegal car movements and driver confusion. Right turn access from Lyons Road into Formosa Street, is also restricted during the morning peak.

This proposal would close the northern end of Formosa Street, whilst retaining vehicular access to adjacent properties via a shared, low speed zone. The resulting public space would be redesigned to provide seating opportunities, landscaping and other amenities suitable for a public square.

The development of the adjacent former Commonwealth Bank building, into a mixed use four storey development with open plaza, would also enhance any proposal to create a public open space at this location, in the 'heart' of Drummoyne. An alternate option could involve retaining one lane of through traffic, but designing it so that it operates as a slow speed, shared space. The current width of the road could then be significantly reduced, and the remnant space developed for public use.



Temporary activation (refer to page 92) could be a highly effective way of testing the impact of different options.

All of these options would need to be tested for traffic impacts, as the benefits would need to be considered in relation to any changes in traffic movement. The community were supportive of this proposal, although there were concerns expressed about the impact on traffic flow, and closing off a road that is currently one of the few access points off Lyons Road.

Estimated costs

Part of Public Domain Plan: \$200,000

Note: Costing is indicative only and dependant on final design and engineering requirements.

ACTIVATION INITIATIVES

- 01 New paved plaza on Formosa Street to create a 'walkable' neighbourhood character
- 02 New street furniture around existing trees to provide spaces for respite especially for the elderly
- 03 Outdoor 'spill out' areas to promote vibrant streets for future retail and cafe spaces

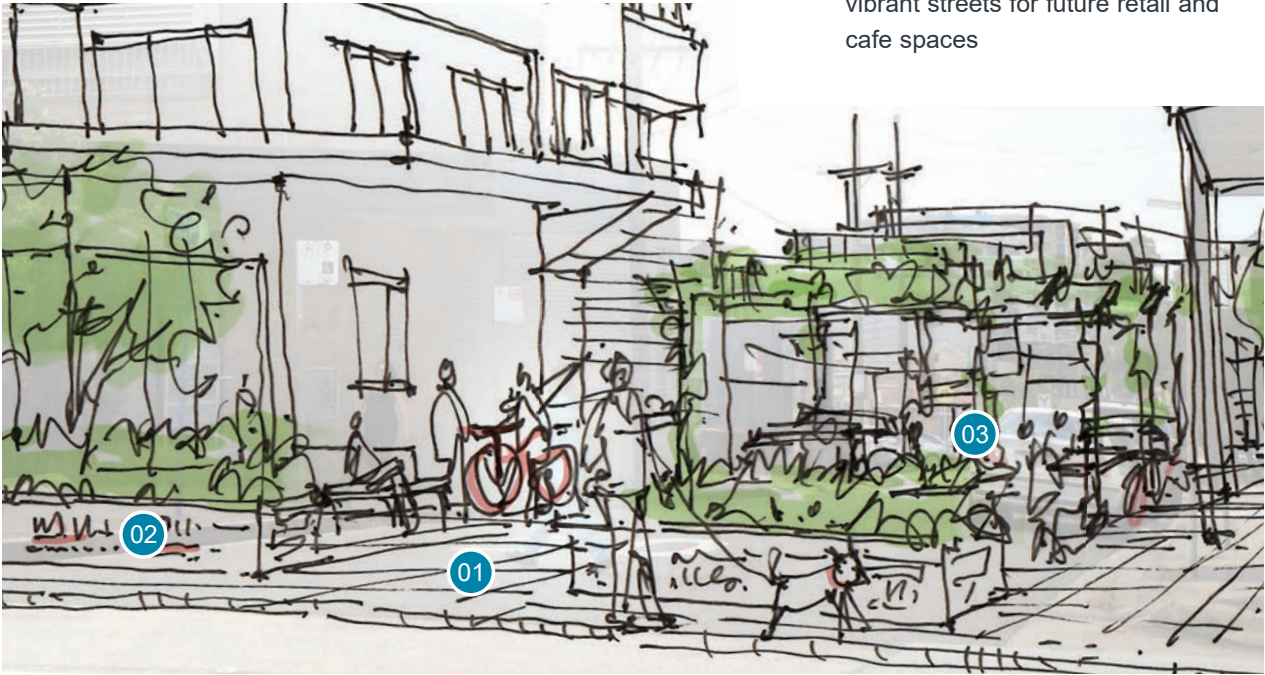


Figure 35 Artist's impression

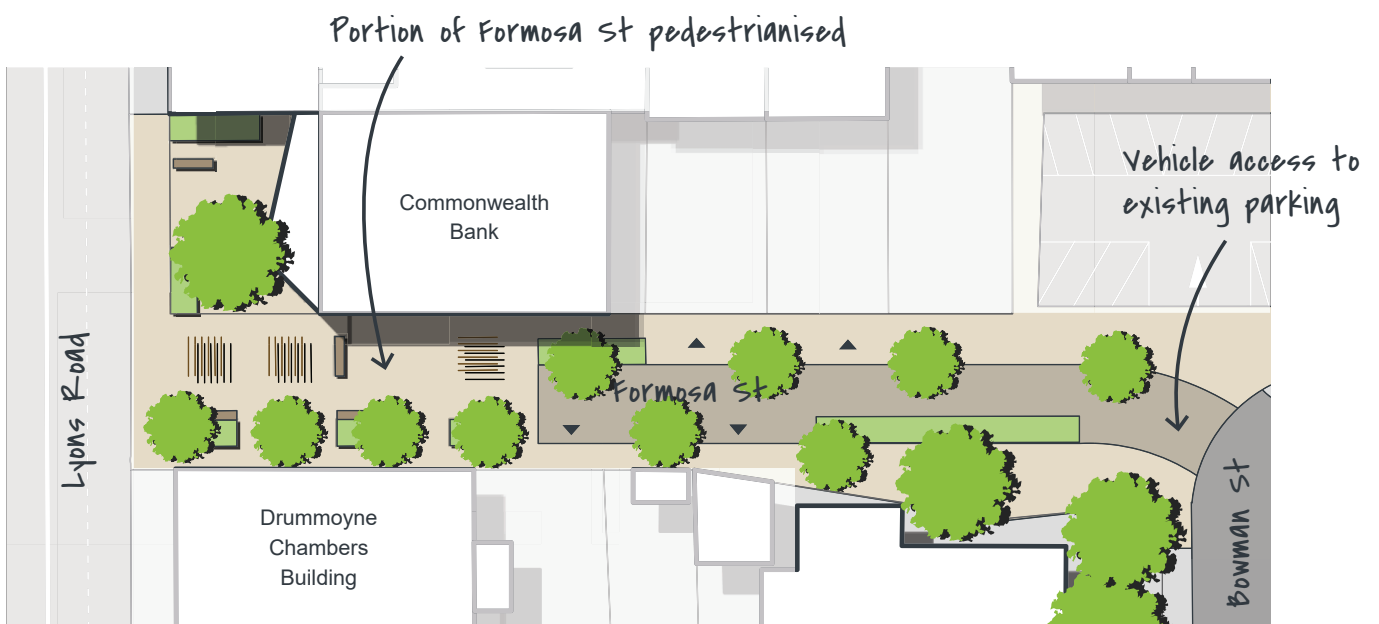


Figure 34 Plan view of proposed public space on Formosa Street

Temporary Activation

All over the world, temporary projects are taking on the role of an invaluable design tool allowing more tactical participation and experimentation, while giving the community, designers and decision makers the opportunity for immediate feedback through observation, reiteration and revision. While larger scale efforts are a necessary part of improving the living environment, incremental, small-scale improvements are gaining momentum as well.

Temporary activation allows for quick reviewing and refining of the ideas. This takes design out of the design studio into real life situations for the public to experience. It can be implemented, studied and measured in real-time, inexpensively and temporarily. The immediate feedback from short-term rapid prototyping can be invaluable for evaluating and identifying potential changes to the design. Rapid prototyping of ideas in physical space presents opportunities to test new concepts before making significant spacial and financial commitments.

Temporary construction projects can be in place for a few hours or a few days or for a longer duration of a few weeks, and can help test the pulse of the community and the strength of an idea. It can prioritise areas of concern/opportunity, some evident only when a space is used by its inhabitants. Seeing a project develop within a short duration from drawings on paper to tangible 'things' and spaces encourages participation, exploration of unconventional ideas, and working towards creating long-term stewardship within a community.

For improvements like the Public Space on Church Street idea, there could be value in spending \$10,000 on temporary changes before investing significantly more on changes that are permanent. This way if the improvement does not work as planned, there can be refinements made to the project, layout, traffic, etc, while the temporary space is still in use. If done in collaboration with long-term planning, temporary activation can act as a first step towards creating long-term improvements.



'Streets for People' project in Los Angeles that converted a roadway and carpark to an urban plaza (Image courtesy of Nate Berg as published in 'Doing it Differently' by Kylie Legge)

ACTIVATION INITIATIVES

A-07 Upgrade of pedestrian links



Artist's impression of potential future upgrade of the arcade leading to IGA



Example of an activated pedestrian link



Example of a green pedestrian link

Estimated cost: \$5,000 - \$100,000 approx.

Note: Costing is indicative only and dependant on final design.

Pedestrian links, into and through the centre, are essential to facilitate movement within and around the centre. Drummoyne currently has a couple of existing links, that facilitate access along established desire lines, often between parking and destinations such as the IGA or medical facilities.

Enhancing these links, by the provision of good lighting, signage, public art and temporary activation would make them more vibrant and encourage pedestrians to leave their car and move around the centre, enjoying the amenities on offer. This vibrancy makes the centre more attractive, which is a self-fulfilling cycle as more people are attracted to the centre, thereby making it more attractive.

Options for improving links are varied, and can include landscaping, activities, public art at a pedestrian scale, as well as the well-established upgrades relating to improved safety and security through upgraded lighting and visibility. There was significant support for this strategy, as the community understood and saw the advantages of improving existing links, so that they were more attractive and enticing, and safe.

A-08 Public art

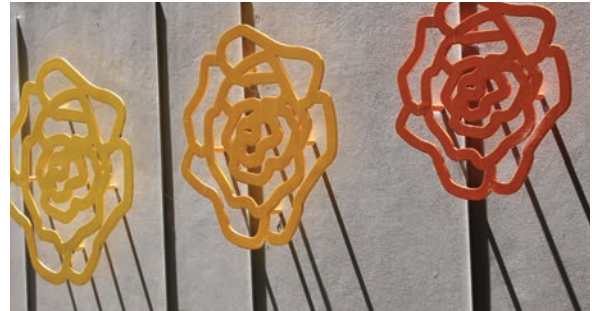
The installation of public art pieces can provide an opportunity to enliven an area and disguise elements that are less attractive, they can also enhance the pedestrian experience.

Large expanses of blank wall can be utilised as a 'blank canvas' for art that has meaning and connection with the local community. Smaller pieces of sculptural art, or items that are multi-functional such as pieces children can play on or with, provide entertainment and activity at a pedestrian scale.

Initially the idea presented to the community proposed art works to be installed on prominent building facades. During consultation, we were advised that the large building-scale art work were less attractive to the local community, than small scale pedestrian level public art, such as murals, or play pieces in the proposed public open spaces, so this recommendation is that this type of installation be considered.

Estimated cost: \$10,000 - \$100,000 approx.

Note: Costing is indicative only and dependant on final design and selection.



Example of an art installation that provides interest at pedestrian level along a blank wall



Interactive play piece

ACTIVATION INITIATIVES

A-09 Smart light poles

Victoria Road currently has the old-style timber light poles, many of which are starting to develop a distinctive lean and carry outdated street lights.

This proposal is to replace these poles with 'smart poles', similar to those used by the City of Sydney. These new poles are metal, so they do not deteriorate. They would support modern updated street lights, which are more energy efficient and throw light in a more targeted manner. They can also be designed to carry banners, which can provide information on community events, or advertise various Council initiatives.

These poles are tidier, lower maintenance and more adaptable. Replacement of street lighting along this stretch of Victoria Road would require consultation and co-ordination with RMS, but would significantly improve the presentation of this area, and provide the City of Canada Bay a venue to showcase the Council area to passing traffic.

Community support was variable, and many expressed concerns that this would be an expensive exercise and a 'waste of money'. The recommendation is therefore that this option be considered when replacement of the existing poles is required.

Estimated cost: \$25,000 per smart pole plus installation (estimated \$40,000)

Note: Consideration would need to be given to the costs associated with any changes to power supply needed to enable the installation of smart poles.



Tilted timber pole along Victoria Road



Smart poles such as these in Five Dock, feature banners which provide opportunities to showcase public events

A-10 Sutton Place community hub

Sutton Place is a valued area of community open space, although it is actually privately owned and managed. This space is currently surrounded by an IGA, a range of retail and commercial premises, as well as an on-grade Council owned carpark fronting Marlborough Street.

The proposal put to the community was complete redevelopment of this site, that would require all the owners, as well as Council, to consider this site as a whole and redevelop it to provide a community facility, such as a Library, significantly more public carparking, a supermarket, retail and commercial premises, and several storeys of apartments. Parking, and the supermarket with its associated loading requirements, would be accessed off Marlborough Street. The community facility would front Victoria Road, and be a prominent feature with direct access to an enlarged and greener Sutton Place.

Overall there was strong community support for this sort of development of this prominent location. Many felt that it was an obvious location for a community facility and many also liked the idea of improving what they feel is the 'heart' of Drummoyne. Some concern was expressed about potential blocking of views from the existing apartment building to the north east, but this proposal does not propose any height above that allowed for in the current DCP operable on this site.

This type of joint venture development, where multiple parties are involved and there is significant reworking of land parcels, can take many years, but it would be a good outcome for this much-loved location and would help to create a true 'heart' for this community.

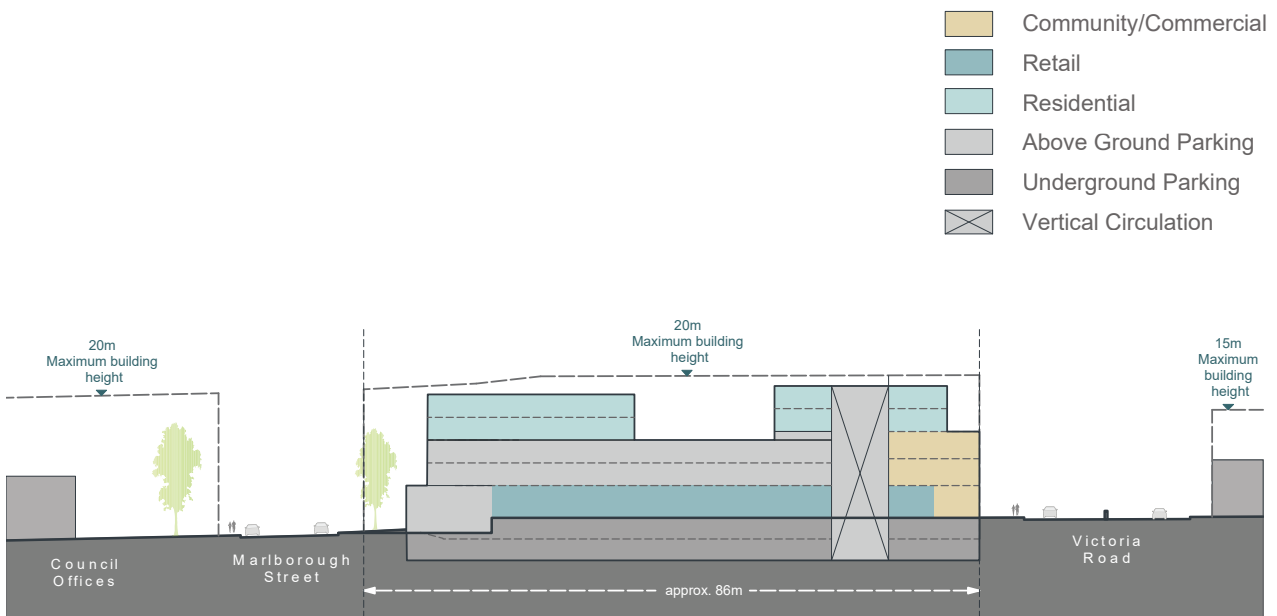


Figure 36 Section of potential future community hub at Sutton Place

ACTIVATION INITIATIVES

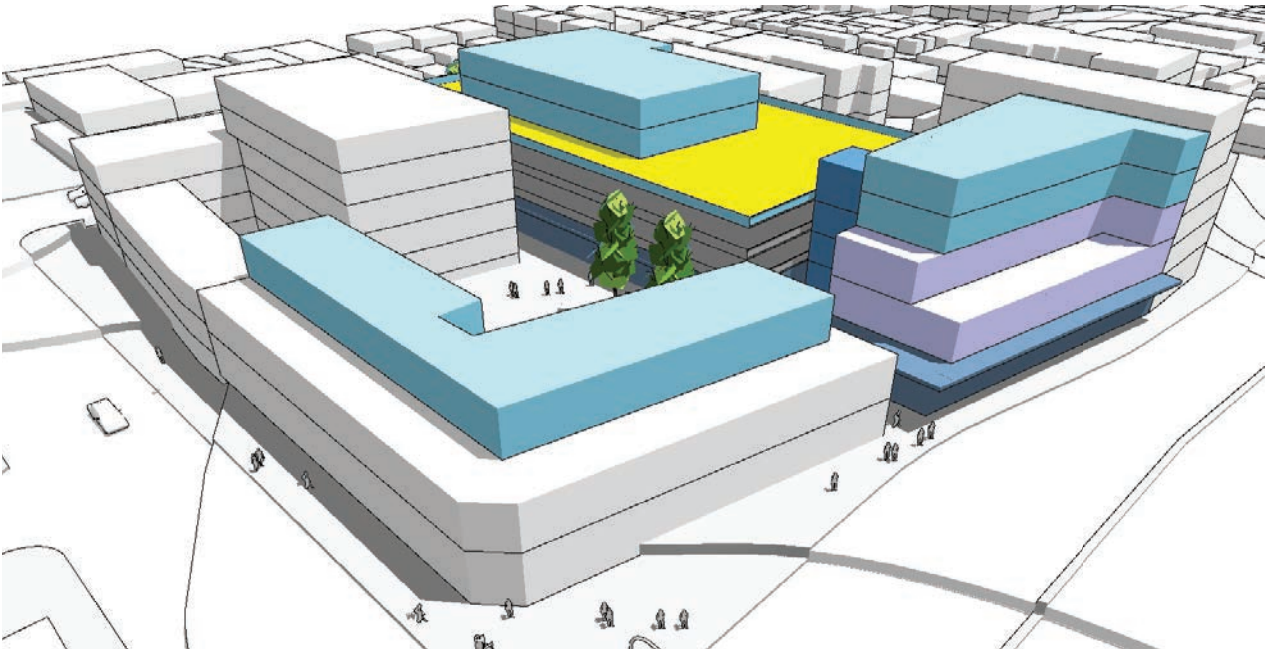


Figure 37 3D view of potential community hub at Sutton Place

A-11 Multi-deck council carpark

The Council owned on-grade carpark on Marlborough Street is conveniently located behind the Sutton Building shops and the IGA, and has the potential to be used by a larger user group including shoppers, office-goers, those visiting Council as well as residents. Currently time controlled, there is also the potential for use by commuters if additional non-timed spaces were available.

It is proposed to use this site as the location of a larger, multi-deck carpark. To minimise impacts a number of storeys would be below ground, which also utilises the site's natural topography. Additional spaces could be provided to support commuter parking, and timed parking could be retained to support visitors accessing the nearby retail and commercial premises.

A major challenge for this proposal is the need to retain access through the site to the supermarket, for loading and waste removal.

Generally the community was supportive, although there was some opposition due to concerns about additional parking generating additional congestion, and the impact of additional height in this location. There was also general comments regarding the need for retail / commercial premises throughout the area to provide sufficient parking for visitors / customers.



Woolworths carpark in Crows Nest, Sydney



Green-walled multi-deck carpark

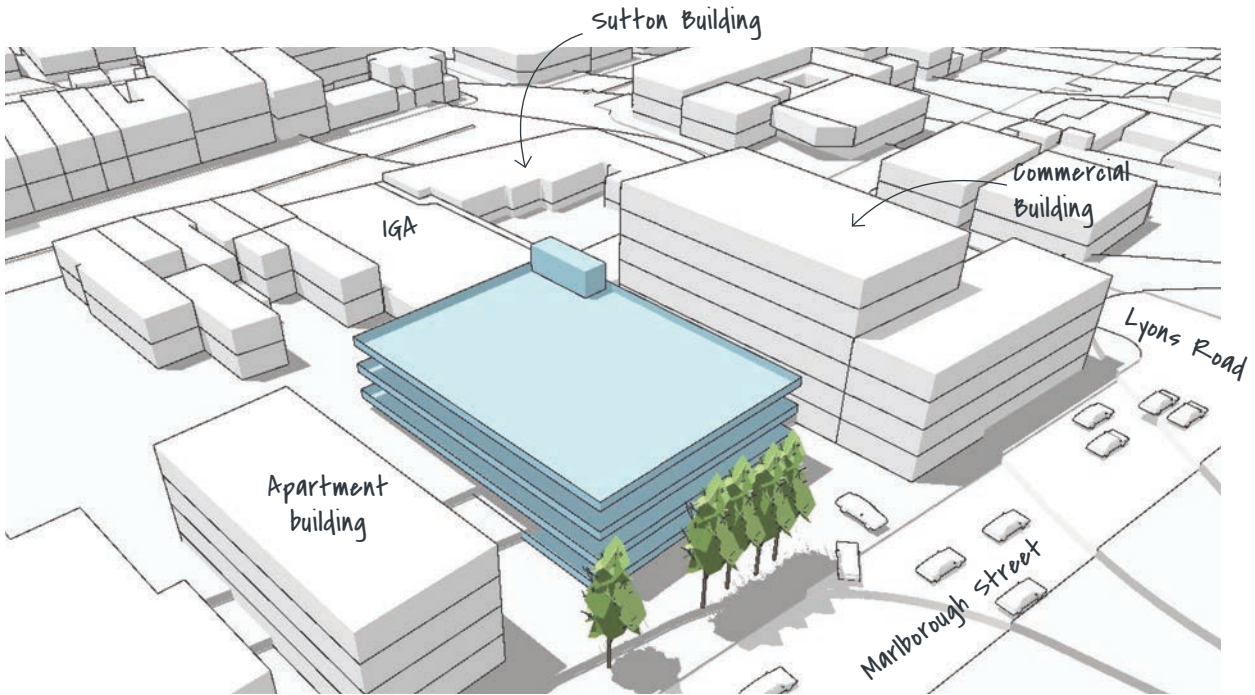


Figure 38 3D view of potential future multi-deck car park off Marlborough Street

A-12 Privately owned public spaces

POPS, or privately owned public spaces, are areas that are publicly accessible, but privately owned, managed and maintained. These are usually landscaped areas, providing seating, outdoor dining and play areas. Sutton Place is an example of such a space.

Another example of a POPS would be the proposed plaza outside the soon to be redeveloped former Commonwealth Bank site. This is proposed to be an outdoor dining space associated with a café in the development, and indicates the benefit of these 'break-out' spaces in what is otherwise a traffic dominated area with little respite.

Other opportunities for POPS include roof top gardens or internal courtyards that are publicly accessible. These are especially successful if they incorporate elements such as a play area, an eating venue or are located along pedestrian desire lines, as this activates the space.

POPS can be delivered via negotiated variations to maximum building heights and setbacks in the DCP where developments propose to deliver an attractive and well located public open space.

There was little support or opposition to this idea from the community. They were generally supportive of anything that improved the 'greening' of the area, but POPS were only supported if they didn't result in additional height or scale of new development.

Estimated cost: no cost to Council



POPS can include all forms of public spaces such as plazas, arcades, small parks, rooftop gardens and atrium



The Sutton Place courtyard is an example of a POP and has potential to increase living landscape

A-13 Night-time economy



An artist impression of night-time activation of a laneway

Providing stimulus for a night-time economy within a centre is a long-term process. Currently the night time economy opportunities along Victoria Road are widely spaced and operate as stand-alone destinations. To improve the chances of success for night time venues within this area, suitable locations need to be identified, and supported with associated parking. Lighting of these areas is also of primary importance, to ensure that people who use these venues feel safe in accessing them.

The best outcomes are achieved when there is a cluster of such venues, as they then create a vibe that is attractive and interesting for patrons, whilst offering variety. 'Eat Street' type development is commonly associated with the night-time economy, as are live music venues and small bars. Currently allowable in this area, the unavailability of suitable existing buildings, lack of parking and neighbour objection would seem to be the biggest de-motivators for this type of development.

This recommendation focuses on the opportunities that might arise for night-time economy venues, provided by elements such as public spaces, to hold weekend markets, small festivals and the like, venues for live music such as hotels and restaurants, and public facilities such as a library or community centre. The community were generally supportive of this idea, although there were opposing views on possible locations for increased night time venues. Some supported Formosa Street as a location for



Lighting helps create a vibrant & active night-time settings

increased activity and pedestrian focus, while others were concerned about the impact of increased noise and people in this area. Council could also consider establishing rules that encourage night time activity similar to the new night trading rules developed by the City of Sydney. These could include:

- Establishing a late night trading area (to midnight or 2 am) for shops, businesses and low-impact food and drink venues that trade off Victoria Road.
- The new hours would only apply if patrons enter and exit the venue from Victoria Road and not via a laneway or residential area.
- Establish a new category of trading hours for unlicensed shops, like bookstores and clothing shops, and businesses like gyms, drycleaners and hairdressers. This would allow them to trade until midnight along Victoria Road. Later trading hours for these types of businesses would attract a wider range of people out at night.

If these types of changes are approved by Council, businesses will need to submit a development application for the new trading hours and will need to demonstrate how they will mitigate any impacts on surrounding neighbours. This may involve noise assessments, sound-proofing and other mitigation measures.

Estimated cost: variable



CHAPTER 7 PLANNING FRAMEWORK

7-1 Introduction

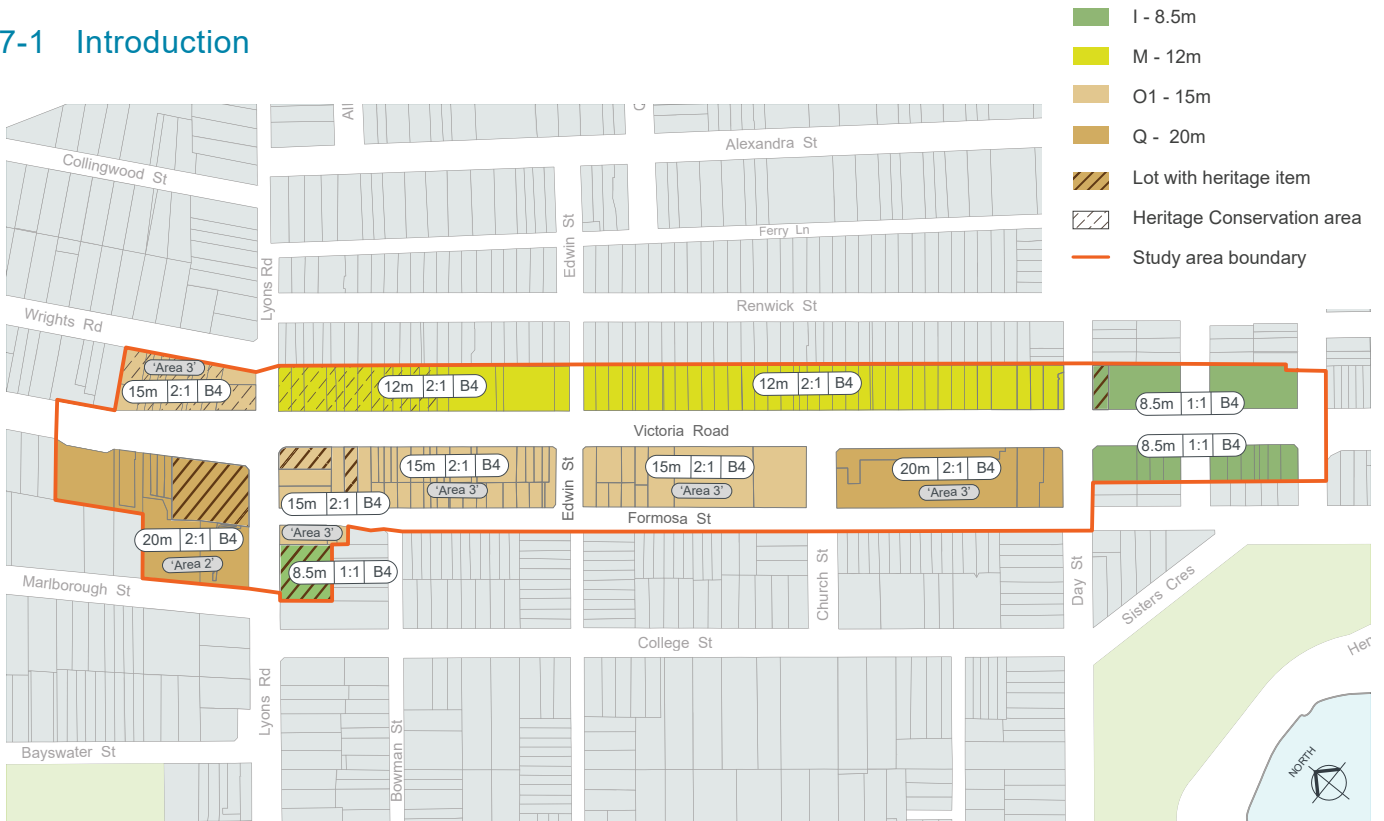


Figure 39 Current key LEP controls overlay

This section outlines recommendations on potential amendments to two key policies: the Canada Bay Local Environmental Plan 2013 (CBLEP) and the Canada Bay Development Control Plan (CBDPC). The recommendations focus on ways to deliver the desired character of the study area by setting clear expectations for how future private development will be designed, and how it will perform in the context of the centre and the surrounding area.

Typically, controls in the LEP cover a larger area and are predominantly spatial. They guide the land uses appropriate for an area, seek to protect natural resources and heritage places, and set upper limits to building height and permissible density of development. The DCP provides detailed planning and design guidelines to support the LEP and Council can identify provisions that are tailored to local issues.

The diagram above overlays the current key LEP controls over the study area boundary, including building height limits, land use zoning, density expressed as a floor space ratio (FSR), lots with a listed heritage item and the heritage conservation zone.

Most notably, the overlay identifies that properties to the north-east and south-west of Victoria Road have the same FSR (at 2:1) while height limits are different and range from 12m to 20m. The diagram also shows that listed heritage items are predominantly located near the intersection of Lyons Road and Victoria Road, and that only a small part of the study area to the north east is subject to a heritage conservation zone.

7-2 LEP modifications

Heritage and Conservation

There are five heritage items and a small portion of heritage conservation area within the study area, that predominantly occur near the intersection of Lyons Road and Victoria Road to the north-west of the study area. These items and the heritage conservation area contribute to the character of the study area. No changes to current provisions are recommended.

Floor Space Ratio

The existing maximum FSR is 1:1 along Victoria Road from Day Street to Park Avenue and on land south-west of Lyons Road/ Formosa Street, and 2:1 across the remainder of the study area. Provisions in the LEP allow for a higher FSR of up to 3.5:1 for sites marked as 'Area 2' and up to 3:1 for sites marked as 'Area 3' on the basis of site area.

These FSRs are high compared to the surrounding low-density residential areas. Site testing has found that it can be difficult to achieve the FSR for a mixed use building (one level of retail/commercial with residential uses above) within the current height limit controls. However, the current FSR controls along Victoria Road are considered appropriate and no amendments to the current provisions are recommended.

Land Zoning

The study area is zoned B4 Mixed Use which allows for a range of business, office and retail and residential uses. A recent change to the CBLEP means that buildings can be fully commercial or a mix of uses but cannot be entirely residential, requiring a non-residential use on the ground floor. The B4 zone is considered appropriate and no changes to the zoning is recommended.

Building Height

Building heights in the study area currently range from 8.5m to 20m. The number of storeys possible within this height is dependent on use as commercial uses typically require a higher floor to floor height. Assuming a commercial ground floor of 3.7m floor to floor and a 3.1m floor to floor height for residential uses, these heights allow for buildings ranging from 2 (8.5m) to 6 (20m) storeys in height.

The eastern side of Victoria Road, from Lyons Road to Day Street, currently has a maximum building height of 12m which allows for a 3 storey mixed use development with a flexible retail / commercial space on the ground floor. The western side of Victoria Road, from Lyons Road to Day Street, currently has maximum building heights ranging from 15m to 20m. The greatest height has been located at the highest parts of the centre, capturing views to the CBD but also increasing the visual impact of the buildings.

A review of recent developments has shown that variations to the building heights have been approved. These variations have included concessions on the maximum height of buildings and flexible application of the DCP controls particularly where the heights in the DCP were much lower than those shown in the LEP.

Concerns about building heights were consistently raised during the consultation and engagement process. There was very clear feedback that there is strong community support not to change the current planning controls, as well as a desire that the current controls should be applied more consistently during the assessment of proposals.

L-01 Active frontages

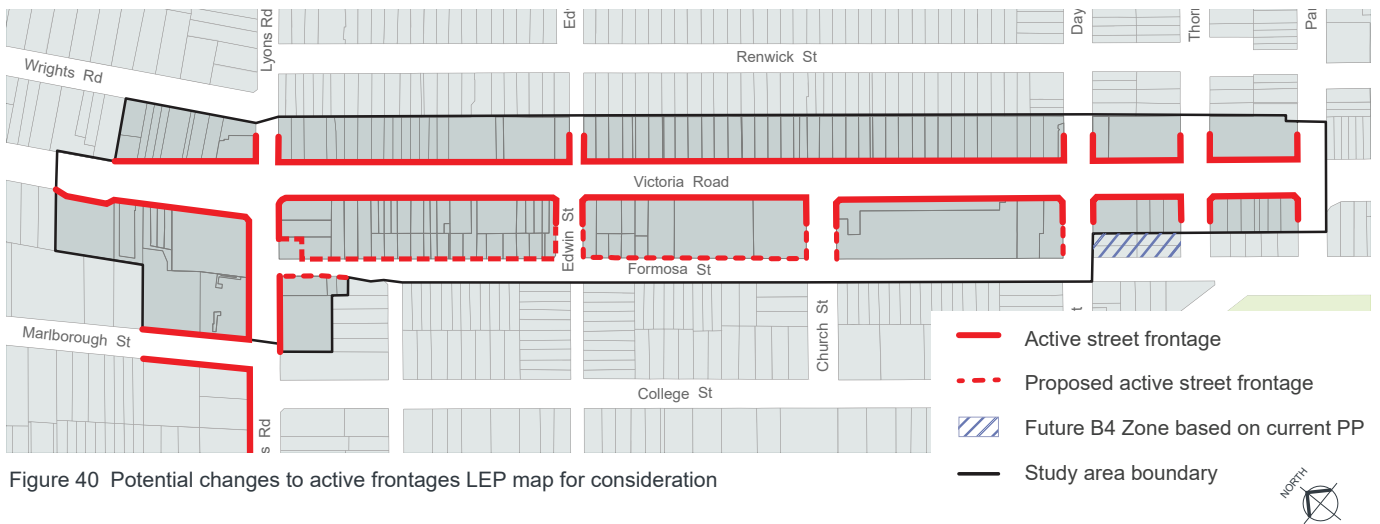


Figure 40 Potential changes to active frontages LEP map for consideration

Detailed pedestrian-scale visual environments, viewed at walking speed and combined with ‘active’ uses such as shops, cafés and restaurants, are key success factors in creating (and sustaining) pedestrian activity which is an essential element for the economic success of a retail area. The ground floor needs to invite interaction through entries and windows, create diversity, limit vehicle access points and shelter pedestrians from the weather through the use of continuous awnings.

Current LEP

The Active Frontages LEP map identifies areas of active frontage along Victoria Road Drummoyne and extends to the cross streets being Lyons Road, Edwin Street, Church Street, Day Street and Thornley Street.

Issues and opportunities

Formosa Street currently is zoned as B4 Mixed Use on the eastern side and has no active frontages. Since Formosa Street is parallel to Victoria Road and provides a pleasant walking environment, there is an opportunity to activate parts of this street through active ground floor uses.

Recommendations

It is recommended that active frontages extend along both sides of Edwin Street and Church Street and along Formosa Street to encourage greater pedestrian movement within the study area and to activate Formosa Street. This also reflects the requirement for non-residential uses on the ground floor. It is noted that due to the presence of existing driveways, garages and carparks, it may not be possible to locate active frontages continuously along Formosa Street.

L-02 Building height (east)

The eastern and western sides of Victoria Road are distinctly different and therefore the planning controls need to consider and reflect their different character. Properties within the study area on the eastern side of Victoria Road are relatively shallow (approximately 31m deep) and back directly onto low density houses within a conservation area. Very few properties on the eastern side of Victoria Road have secondary road access.

Issues and opportunities

Although the DCP allows a maximum 12m height (3 storeys) along the eastern side of Victoria Road, some recent developments have been approved for 4 storeys with a partly set down ground floor at street level, resulting in compromised retail spaces and a poorer quality streetscape.

In recent approvals the DCP stepped building height has also been varied bringing more of the development closer to the rear boundary and impacting on the privacy and amenity of neighbours.

The existing DCP sets a 30 degree building height plane to reduce the bulk and scale of the building to the rear of the site. However this angle is set at one storey and an elevated ground floor and/or higher ceiling to the ground level can result in a



New taller development along Victoria Road overlooking detached residential housing along Renwick Street

larger building where the upper levels are closer to the rear boundary. Strengthening the DCP building height controls and providing more detailed height limits in the LEP is recommended.

The rear of properties on the eastern side of Victoria Road generally have a northern eastern orientation and potential water views. This means that there is a desire to locate living areas and balconies to capture this northerly aspect. This results in increased overlooking from these taller developments towards the residential properties / backyards on Renwick Street.

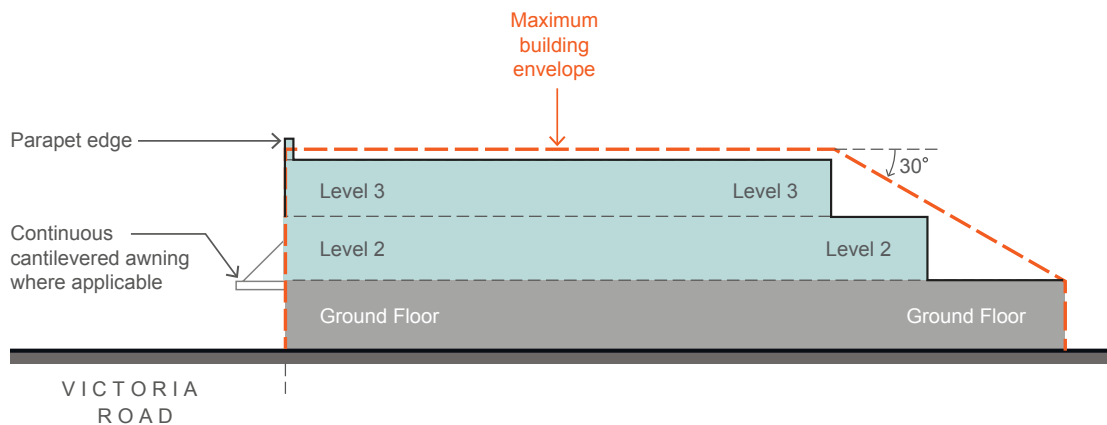


Figure 41 Existing minimum setbacks along Renwick Street between Day Street and Lyons Road (DCP, p F-143)

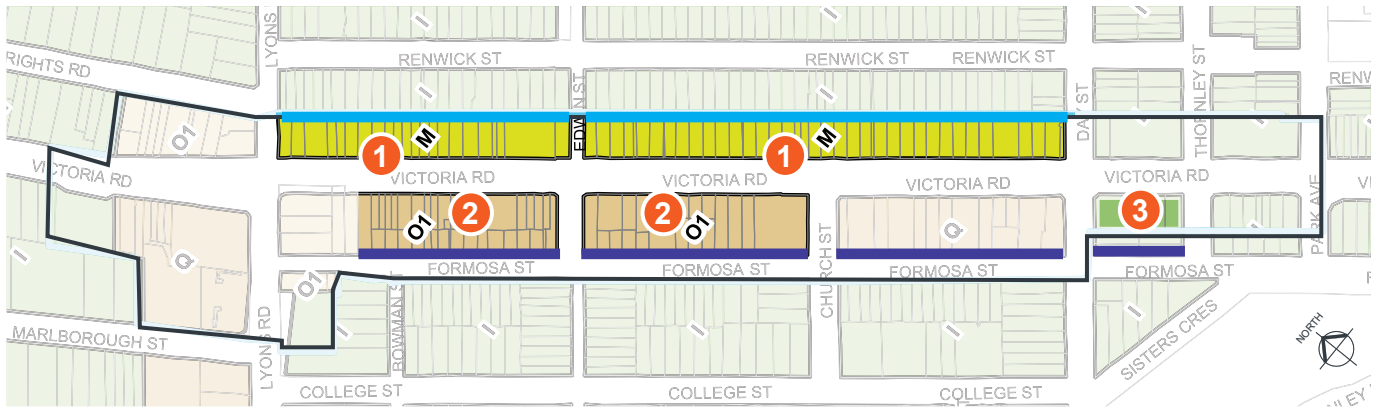


Figure 42 Blocks with proposed amendments to the building height limits

- | | |
|--|---|
| <ul style="list-style-type: none"> 1 Recommended reduction of maximum building height to 11m 2 Recommended increase of maximum building height to 20m 3 Recommended increase of maximum building height to 14m or 20m if amalgamated | <ul style="list-style-type: none"> Recommended 12m deep transition zone with a reduced maximum building height of 8.5m and a 3m landscape buffer Recommended 15m deep transition zone with a reduced maximum building height of 8.5m for 6m with 11m for the remainder Study area boundary |
|--|---|

Recommendations

It is recommended that Council more closely align the heights as set out in the DCP and LEP and provide more detail than the current height limit provisions in the LEP and the DCP.

To address the need to revitalise and reinvigorate the retail functions along Victoria Road, to facilitate higher quality ground floor retail spaces and reduce the chance of sunken ground floor frontages it is recommended that the DCP clearly identifies a minimum floor to floor height for ground floor retail/commercial uses along Victoria Road.

Studio GL initially recommended increasing the maximum height on the eastern side of Victoria Road between Lyons Road and Day Street from 12m to 13.5m. The aim of this change was to provide better retail / commercial development on the ground level and encourage development away from the rear boundary. During public consultation about this recommendation it was made clear that any increase in building height was not generally favoured by the local community, there were concerns that height controls would not be adhered to as existing controls had been overridden in the past during the Development Approval Process.

In response to community concerns and to reduce confusion, it is recommended that the existing 12m maximum building height along Victoria Road be reduced to 11m, as this reduction in height would discourage new development pursuing a fourth storey with the risk of a sunken ground floor which results in a poor quality streetscape and pedestrian activation. The existing DCP indicates a 3 storey street wall height is desirable along Victoria Road in this area and a 11m maximum building height would help strengthen this objective. To further discourage development breaching the height controls it may also be advisable to reduce the maximum FSR on these sites.

The existing 30 degree building height plane control in the DCP, as measured from a nominal 1 storey height, has resulted in perverse outcomes when the angle has been measured from an elevated level, sometimes over 5 metres above natural ground level. Clarifying that the 30 degree inclined setback is to be measured from 3.6m above natural ground level at the boundary should standardise the impact and ensure a reduction to the bulk and scale of development along the rear of sites in close proximity to lower scale residential development.

To limit impacts on surrounding lower density residential and heritage conservation areas, it is also recommended that a transition zone with a maximum building height of 8.5m, for a zone 12m off the rear boundary is introduced into the LEP. As this is an LEP control it would provide greater certainty that development will step down towards the rear boundary. It is also recommended that a 3m deep soil zone is required along the rear boundary.

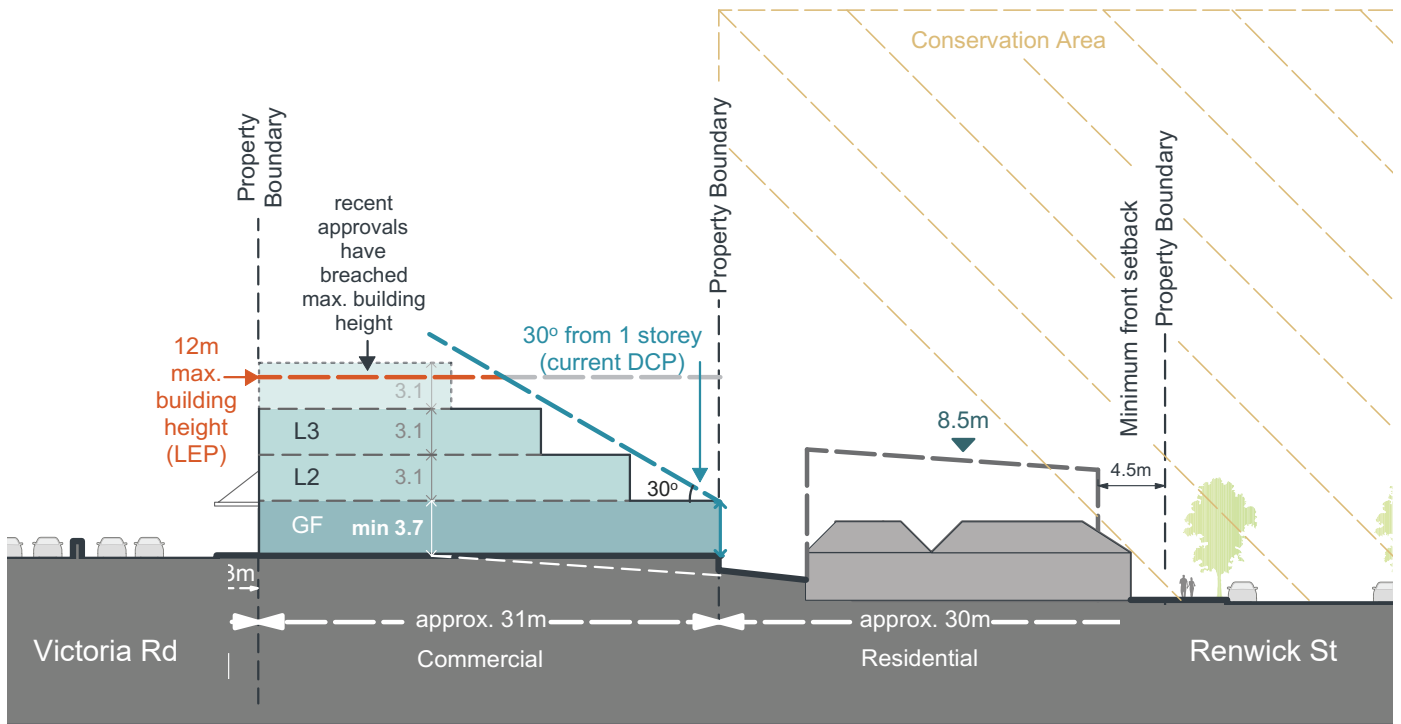


Figure 44 Sectional view showing existing LEP and DCP maximum building height controls on the eastern side of Victoria Road

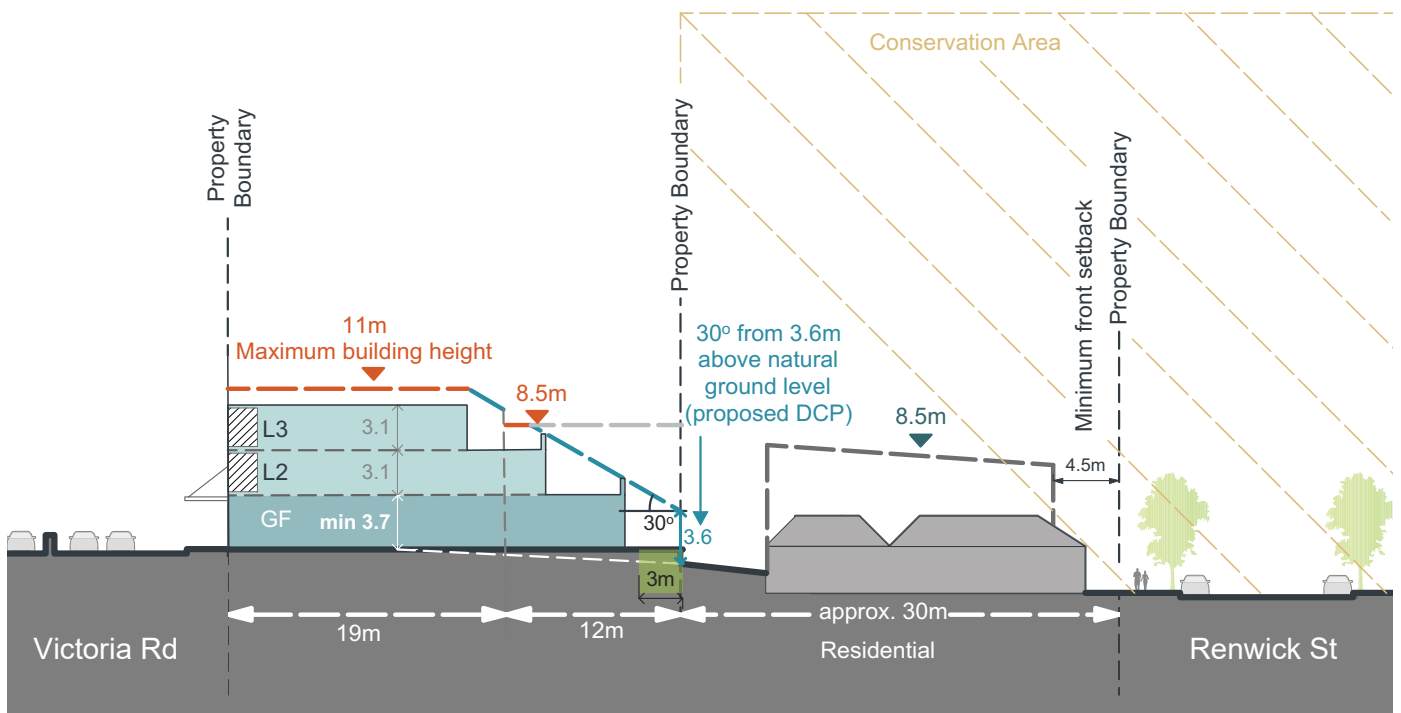


Figure 43 Proposed changes to DCP and LEP maximum building height controls on the eastern side of Victoria Road

L-03 Building height (west)

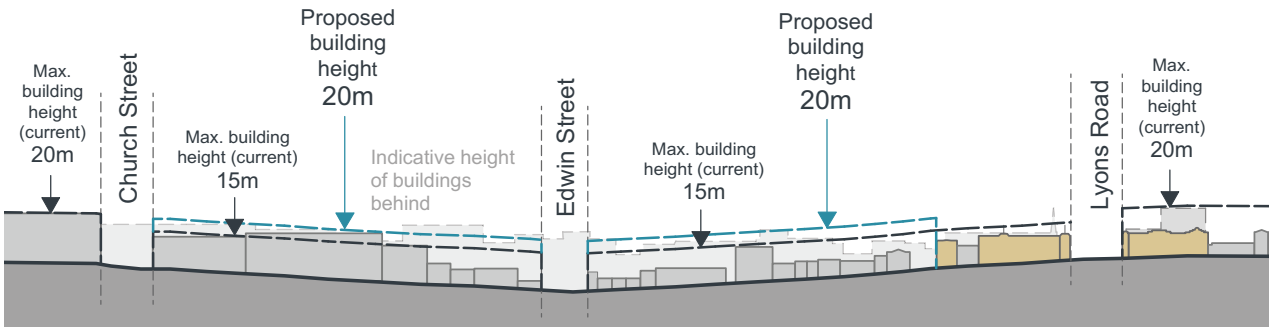


Figure 45 Sectional view of the western side of Victoria Road with proposed building heights

Issues and opportunities

Although the DCP allows a maximum 15m (4 storeys) along the western side of Victoria Road, between Lyons Road and Church Street, some recent developments have been approved for 6 storeys with a partly set down ground floor at street level, resulting in an increased bulk and scale and compromised retail spaces.

The DCP stepped building height has also been varied bringing more development closer to the rear boundary and impacting on the privacy, overshadowing and amenity of neighbours. Edwin Street is one of the few streets that cross Victoria Road. The undulating topography along Victoria Road creates an opportunity for taller buildings in "the valley" on either side of Edwin Street.

Recommendations

It is recommended that Council consider a 'dual-height' approach which adds more detail to the current height limit provisions in the LEP. Some areas and parts of properties are proposed to increase in height, while other areas are recommended to decrease.

The maximum building height of properties along the western side of Victoria Road, between Lyons Road and Church Street, is proposed to increase from 15m to 20m to match the heights permissible on this side of Victoria Road south of Church Street and north of Lyons Road and create a more consistent height modulation along Victoria Road that follows the topography rather than emphasising the ridges.

An 8.5m height limit for 6m from the boundary, with a further 9m having a height limit of 11m is also recommended along the north-eastern side of Formosa Street to enable a sensitive transition to lower scale areas to the south-west, limit overshadowing and deliver a 'human scale' along the street.

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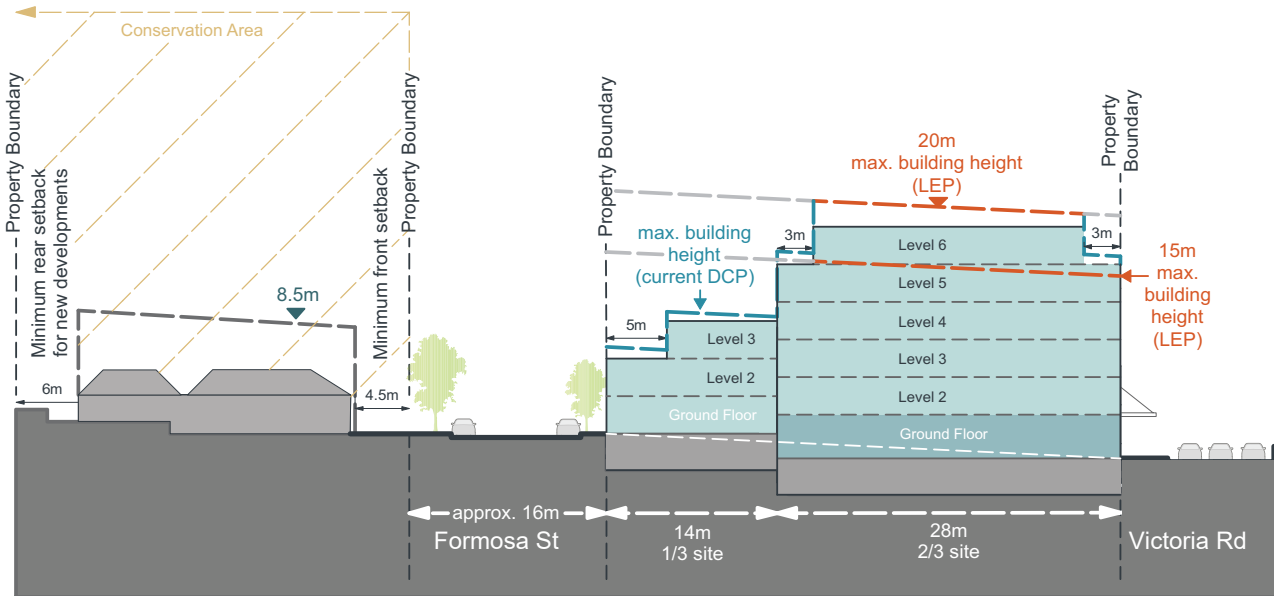


Figure 46 Sectional view showing existing LEP and DCP maximum building height controls on the western side of Victoria Road

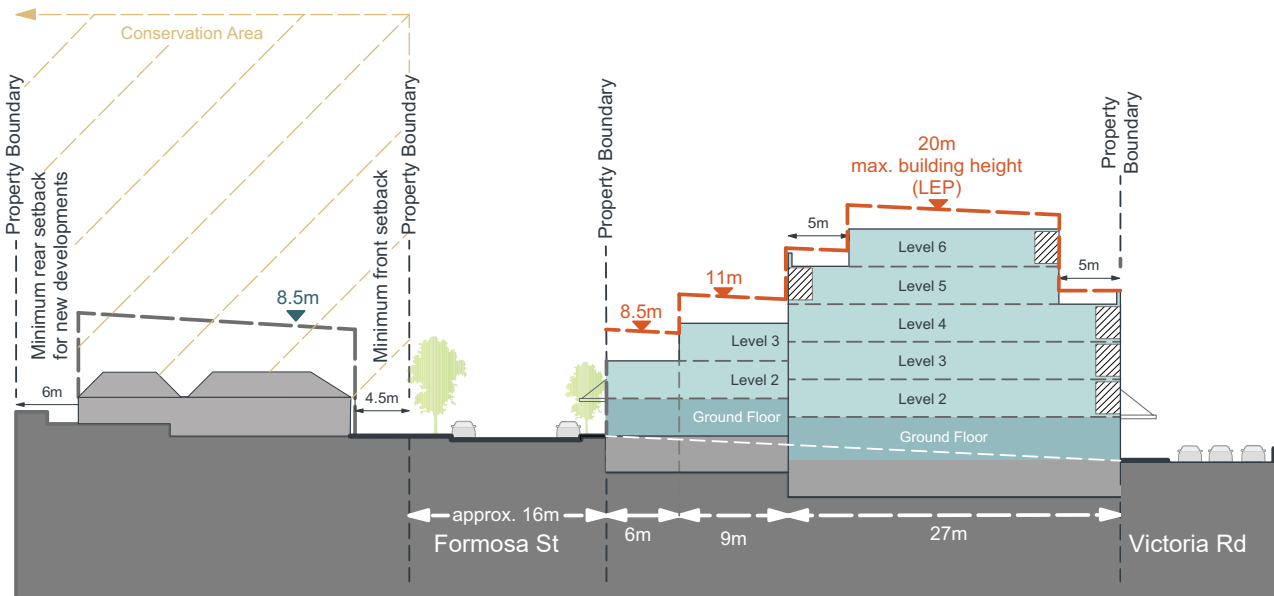


Figure 47 Sectional view showing proposed LEP and DCP maximum building height controls on the western side of Victoria Road

Day Street and Thornley St Planning Proposal Site

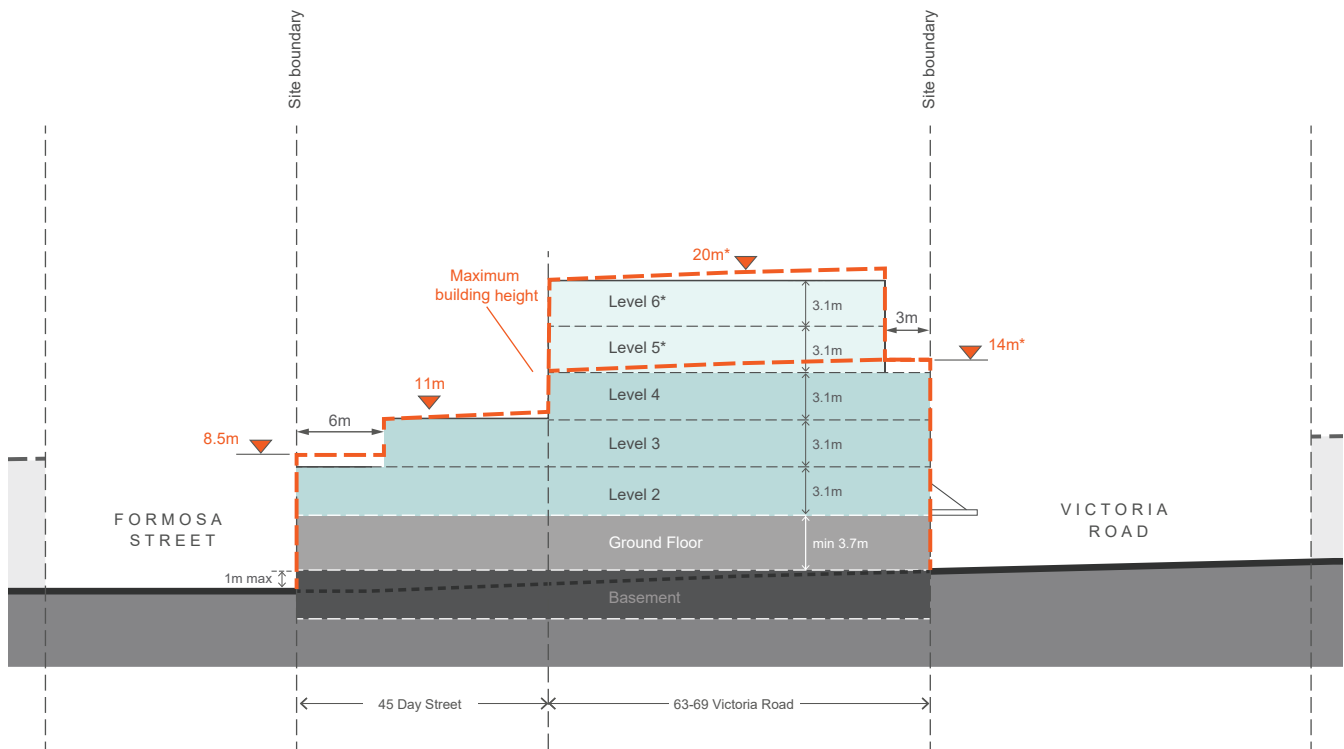


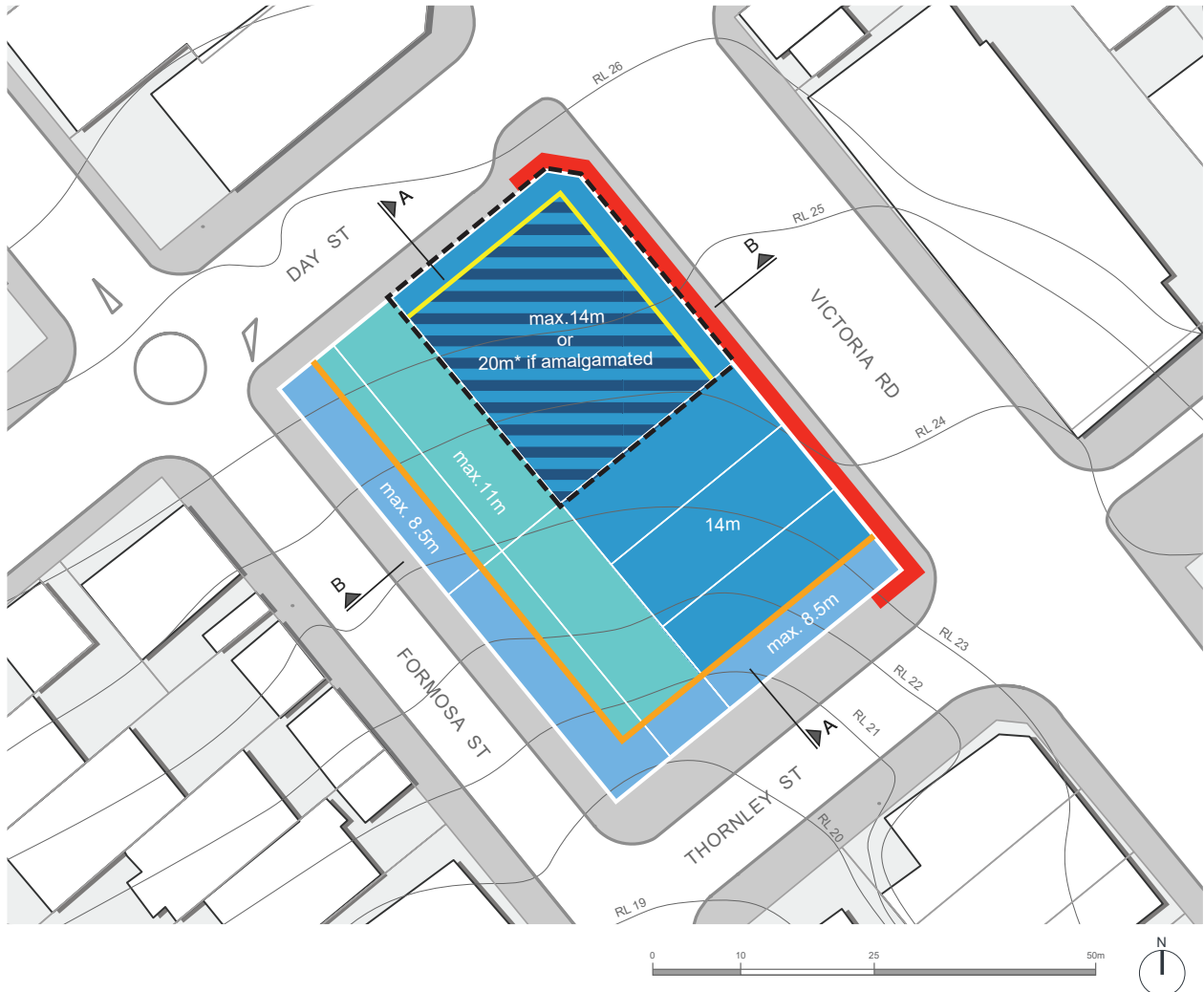
Figure 48 Section B between Formosa Street and Victoria Road showing proposed maximum building heights









A Planning Proposal has been lodged for a site on the western side of Victoria Road between Day Street and Thornley Street. This site occurs at the 'edge' of the local centre and creates a transition down to a lower scale of development along Victoria Road to the south. In this area the land also falls steeply to the south, which increases the impact of any development on this site.

While part of this site falls outside the study area boundary it is logical to include consideration of this site as part of this study.

To ensure constancy with the recommendations in this study for the western side of Victoria Road and to ensure that the proposed building heights reflect the number of storeys in the DCP it is recommended that the maximum building heights shown in the planning proposal are amended as follows:

- A maximum building height of 8.5m that is 6m deep along both Thornley Street and Formosa Street.
- A maximum building height of 11m (3 storeys) for the remainder of sites facing Formosa Street.
- A maximum building height of 14m (4 storeys) for properties facing Victoria Road with a bonus height of up to 20m (6 storeys) for the corner of Day Street and Victoria Road if the overall site is greater than 2,500sqm.



-  20m* max. building height (6 storeys)
-  Area where an increase to a maximum building height of 20m on sites greater than 2,500sqm may be possible
-  14m max. building height (3 storeys)
-  11m max. building height (3 storeys)
-  8.5m max. building height (2 storeys)
-  6m minimum upper level setback to boundary
-  3m minimum upper level setback to boundary
-  Active street front & continuous awning required

* Increased heights only applicable if total development site area over 2,500sqm

Figure 49 Proposed Development Controls for 63 -69 Victoria Road, 45 Day Street, 53 Victoria Road and 46 Thornley Street

Building Heights Overview

Existing height controls

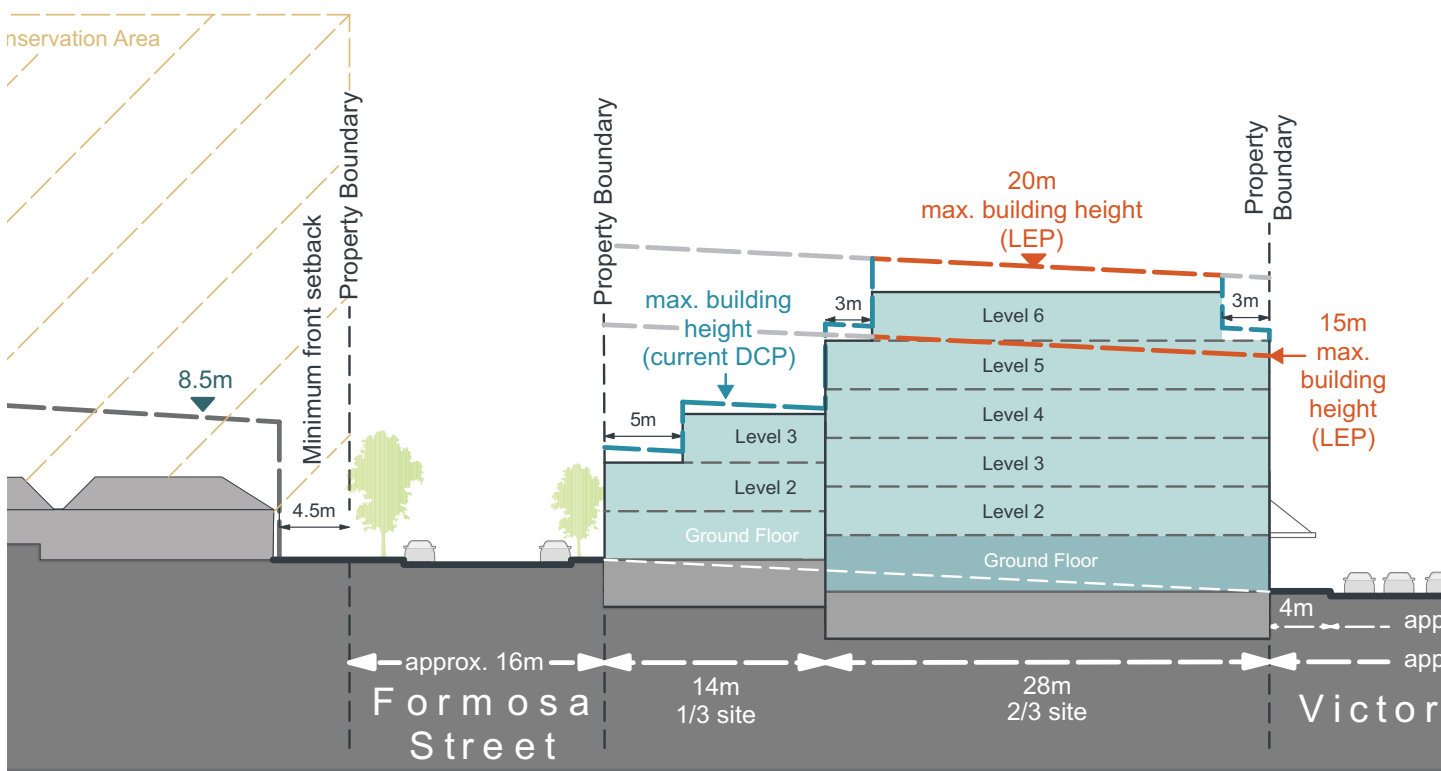
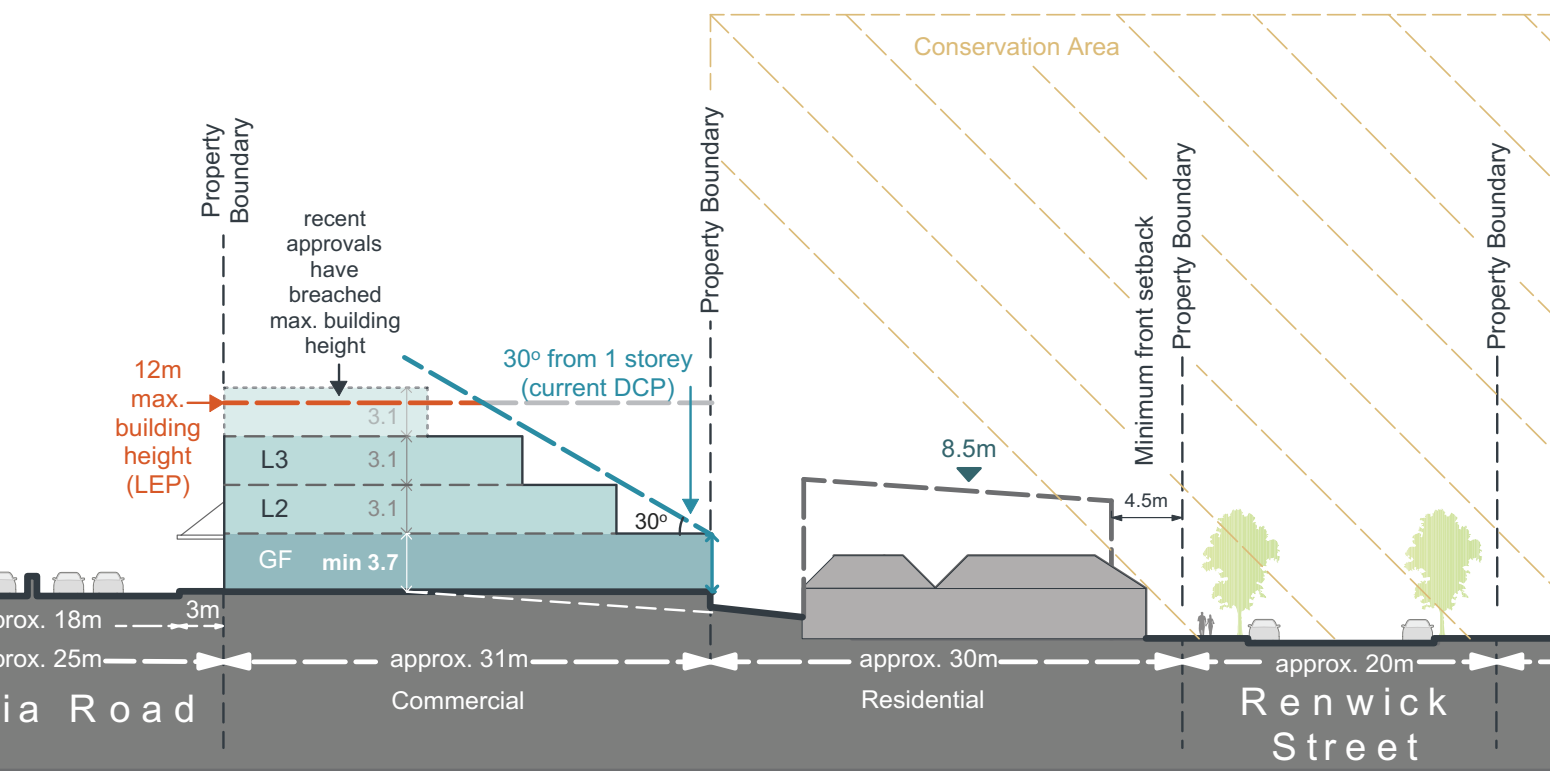


Figure 50 Section C through Victoria Rd, looking north-west showing existing height controls

PLANNING FRAMEWORK



Building Heights Overview

Proposed height controls

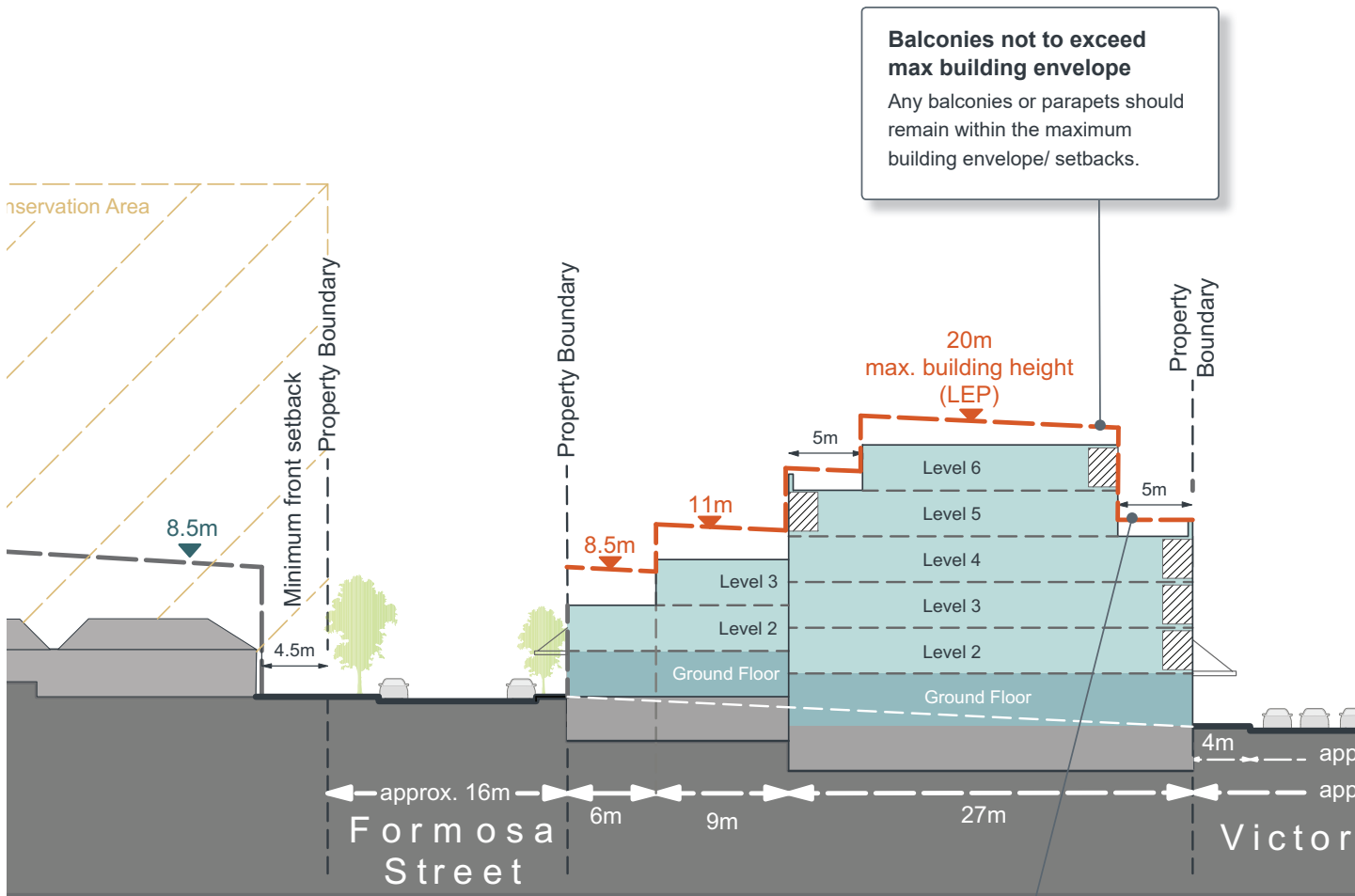
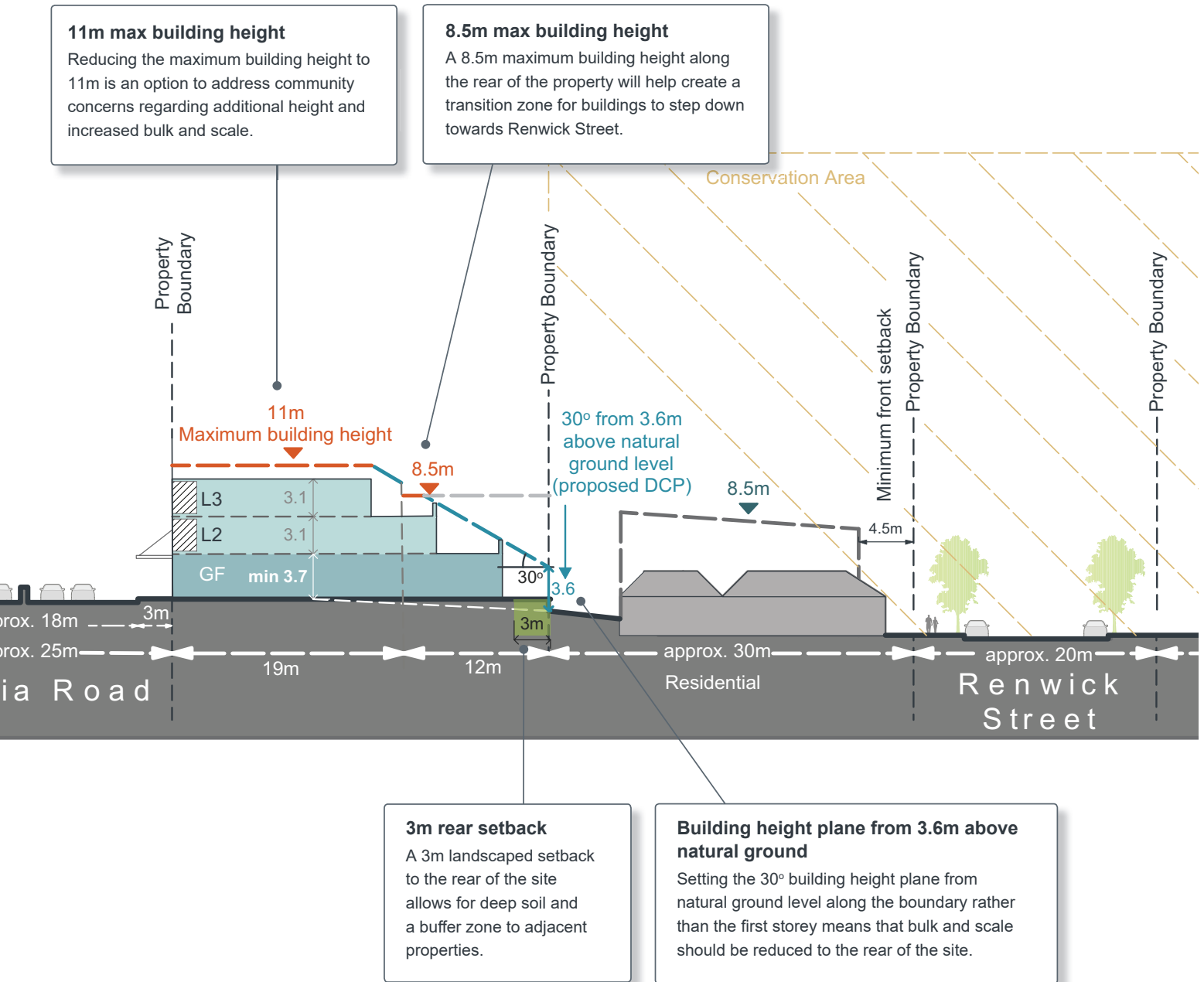


Figure 51 Section C through Victoria Rd, looking north-west (western side) showing proposed height controls



7-3 DCP modifications

Types of DCP controls



DCP controls can rely on numerical standards to achieve desired outcomes, or be based on 'performance criteria' and 'acceptable solutions' that describe how the desired outcomes can be achieved. There are advantages and disadvantages to both types of controls.

Performance criteria are more flexible and focus on the desired future vision, however, they rely on discretionary assessment/ interpretation and can be harder to 'defend'. Numerical standards are black and white making it easier to assess, however, they are more rigid and can hinder innovative solutions which may result in a better overall outcome or 'win-win' for everyone.

Best practice DCPs

Successful DCPs adopt a balanced approach and provide a combination of numerical controls and performance-based provisions. They aim to go beyond simply avoiding the 'worst development mistakes' and seek to provide a policy environment where development that is encouraged maximises the public benefit and generates a positive contribution to the surrounding area.

Best practice DCPs also incorporate design guidance on how new buildings can maximise activity and safety, improve the visual appearance of an area, encourage passive environmental design, facilitate fine grain pedestrian connectivity and be sympathetic to surrounding heritage values.

| | | |
|---|--|--|
|  | <p><i>Performance criteria</i></p> <p>20 Development supports the safety and passive surveillance of the public domain.</p> <p><i>Acceptable solutions</i></p> <p>a) Development contributes to the casual surveillance of streets, lanes and parks by locating doors, windows and balconies towards the public domain.</p> <p>b) Where development is set back, fences are a maximum of 1.2m high and 50% transparent, and windows and entries are clearly visible from the footpath.</p> <p>c) Residential uses on the ground floor can be raised to a maximum of 1.2 metres above the sidewalk level to improve internal privacy. Direct access from the footpath to individual dwellings is encouraged.</p> | <p>Performance criteria</p> <p>Acceptable solution</p> <p>Numerical standards</p> <p>Design guidance</p> |
|  | | |
| <p>Example of a residential dwelling on the ground floor that is directly accessed off the footpath, adding to the safety of the street</p> | | |
| <p>Windows and balconies on the upper levels overlook the street and provide passive surveillance</p> | | |

D-01 Building heights and setbacks

Building heights, in conjunction with setbacks, can impact on the amenity of streets, public places and neighbouring properties. Setbacks are particularly important when developing next to existing residential uses or lower scale areas, or if the neighbouring properties have windows that face the side boundary.

Rear setbacks can help establish (and/or protect) landscaped corridors and connected areas of deep soil that support mature trees, improving biodiversity and the local micro-climate. Rear setbacks also help ensure privacy between residents of adjoining properties, particularly for multi-storey development. Requirements for setbacks vary depending on the context of the development. Along Victoria Road, a continuous street wall is desired with side setbacks set to zero resulting in attached built form.

Current DCP

The site-specific controls for Victoria Road with regards to side and rear setbacks are as follows:

- The western side of Victoria Road between Day Street and Church Street requires a maximum building height of six (6) storeys with the sixth storey setting back by 3m from Victoria Road (p F-141). Along Formosa Street, the maximum street wall height is two (2) storeys and the third storey is required to set back by 5m from Formosa Street (p F-140).
- The western side of Victoria Road between Church Street and Lyons Road requires a maximum building height of four (4) storeys with the third and fourth storeys set back by 1/3 of the site depth from Formosa Street (p F-141).
- The eastern side of Victoria Road between Day Street and Lyons Road requires a maximum building height of three (3) storeys along Victoria Road tapering to one (1) storey at the rear boundary (p F-141)
- The eastern side of Victoria Road to the north of Lyons Road requires a maximum building height of four (4) storeys with the third and fourth storeys setting back by 1/4 of the site depth from Victoria Road and Wrights Road (p F-141).

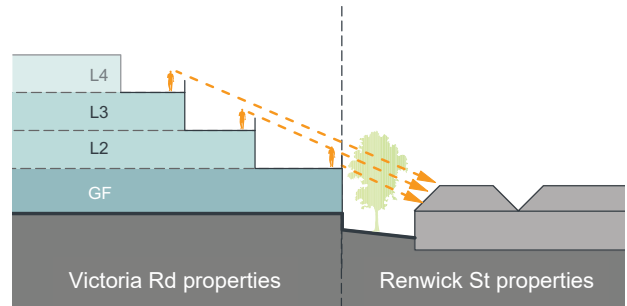


Figure 53 Potential existing overlooking from new development on the eastern side of Victoria Road

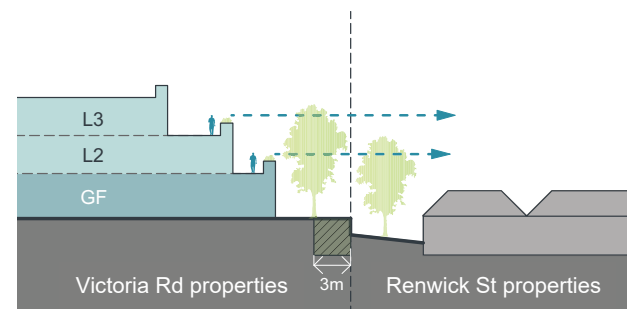


Figure 52 Proposed 3m deep soil zone and solid balustrades to reduce overlooking of neighbouring rear gardens

Issues and opportunities

Due to the importance of these issues changes to building height controls in the LEP and DCP have been extensively covered in L02 Building Heights (east) and L03 Building Heights (west). This section identifies additional design considerations to reduce visual impact and potential overlooking issues.

Recommendations

The revised DCP could include the following objectives and provisions:

- Development on the eastern side of Victoria Road to have deep planters and partially solid balustrades (minimum 80% solid) designed to prevent views from apartments into the rear gardens/ rear habitable rooms of properties on Renwick Street
- Separation of residential flat buildings (apartments) and residential components of multi-storey development satisfies the requirements of SEPP 65 and the Apartment Design Guide.
- Deep soil zone (min 3m) to be located adjacent to rear boundary to create consolidated landscaped/ biodiversity corridors over time.

D-02 Heritage and conservation

The heritage items and conservation areas within the study area make a significant contribution to the character, cultural value and identity of Drummoyne. Protecting heritage buildings and their visual setting or 'curtilage' is critical. Sensitive redevelopment of heritage buildings, also referred to as 'adaptive reuse', is encouraged. New development in the vicinity of heritage items or within a heritage conservation area, needs to respect and complement the heritage streetscape character.

Current DCP

Part D Heritage of the Canada Bay DCP contains detailed provisions for new development in and in the vicinity of heritage items and heritage conservation areas. Development must be appropriate in its scale, form, siting, materials and colour and detailing. There are no site-specific controls for the heritage items located on Victoria Road Drummoyne.

Issues and opportunities

- New development on the eastern side of Victoria Road has a difficult interface with the existing adjacent dwellings that form the heritage conservation area (HCA) encompassing Renwick Street.
- The Sutton Building shops and the Oxford Hotel are key corner landmarks. However, the facades of these heritage buildings are not well-lit at night. The other corner buildings on this prominent intersection, that although not heritage listed contribute to the HCA, are also not highlighted.



The Lyons/ Victoria Roads intersection creates an opportunity for celebrating the heritage character

Recommendations

In addition to Part D, the following provisions could be added to the revised DCP:

- Development in the vicinity of a heritage item, within a heritage conservation zone or a contributory zone, protects and enhances the cultural significance of nearby heritage items and streetscape character.
- Where development is adjacent to a heritage item, contributory building or within a conservation area, a variation to the street wall height of the new development may be required.
- Alterations and additions respond appropriately to the heritage fabric but do not mimic or overwhelm the original building. Designs are contemporary and identifiable from the existing building. Ways to separate the new work from the existing include providing generous setbacks between new and old, using a glazed section to link the new addition to the existing building and/or using shadow lines and gaps between old and new.
- Building and facade design responds to the scale, materials and massing of heritage items through aligning elements such as eaves lines, cornices and parapets, facade articulation, proportion and/or rhythm of existing elements and complementary colours, materials and finishes.
- Signs on heritage buildings, including painted lettering, should be carefully located and should be sympathetic to the historic nature of the building. Adjacent signs should be designed and applied sympathetically.
- Where new development directly adjoins a listed heritage building, the appropriate building setback and height will be determined on a case-by-case basis having regard to the views, vistas and context of the heritage item.
- Highlight the assets of heritage buildings at the intersection of Lyons Road and Victoria Road by lighting the facades after dark and providing an adjacent high-quality public domain.

D-03 Bulk, scale and floor heights

To complement the low-density character of Drummoyne, most development along Victoria Road Drummoyne should seek to reduce its perceived bulk and scale when viewed from streets, public places and neighbouring properties, apart from prominent corner locations which benefit from stronger and higher built form. Ways to achieve this include careful composition of building mass, height and (facade) treatment, including horizontal and vertical articulation, projections, recesses, eave overhangs and deep window reveals.

At the same time, buildings need to be able to adapt to a variety of uses over time. This is particularly important for ground floor uses in a Mixed Use zone, so providing generous ceiling heights is essential as this not only allows for greater flexibility and adaptability but enables a better relationship of the building with the street.

Current DCP

The following DCP controls are applicable to the study area:

- Transitional building heights should be provided between the commercial and residential areas to protect the amenity of surrounding neighbours (p F-138).
- Building mass should maintain the prevailing vertical character found in Canada Bay’s business centres (p F-133)
- Balcony balustrades should be of a light open material. Where possible, balustrades are to match predominant examples within the streetscape (p F-133)
- Existing verandahs and balconies should be retained and not infilled (p F-133)

Issues and opportunities

- Increased heights will provide panoramic views including some to the harbour
- Topography and slope impact on floor to ceiling height of Ground Floor

Recommendations

The revised site-specific DCP for Victoria Road could include:

- Objectives i.e. new development is to ensure the bulk and scale is consistent and in keeping with the desired future character and streetscape; reduce perceived bulk when viewed from public places and neighbouring properties (where appropriate); and be adaptable to changes in use in the future.
- Where frontages are more than 20 metres wide, building massing must be vertically articulated.
- If a development is more than 50m in length and higher than 4 storeys it should provide a vertical break in the built form for the upper two storeys.
- The upper-most level is set back and is visually unobtrusive. Ways to achieve this include the use of lightweight construction techniques, dark colours and/or roof elements that create deep shadows.
- Minimum floor heights are as follows:

| Use | Minimum floor to floor height | Minimum floor to ceiling height |
|-------------|-------------------------------|---------------------------------|
| Retail | 4.4m | 4m |
| Commercial | 3.7m | 3.3m |
| Adaptable | 3.7m | 3.3m |
| Residential | 3.1m | 2.7m |
| Community | 3.7m | 3.3m |



Generous floor to ceiling heights are particularly important for ground floors to be able to accommodate change of uses over time and maintain a positive relationship with the street

D-04 Street interface

Street setback zones are an integral part of the streetscape and their treatment is fundamental to the amenity and character of a place. Combined with street wall height and road reserve width, they define the proportion, scale and visual enclosure of the street.

The street wall height is the height of the building from the street ground level up to the first upper level building setback. Setting street wall heights in a mixed use environment is an important element to ensure a consistent building scale.

The street setbacks along Victoria Road Drummoyne are predominantly set at the property boundary (built-to-alignment), defining the street corridor with a continuous built edge and creating direct connections between grade-related retail/ commercial/ community uses and the public domain.

Street wall heights, in conjunction with upper level setbacks, also impact on the amount of sunlight that reaches streets, public places and neighbouring properties. New development needs to minimise overshadowing so street wall heights and upper level setbacks vary depending on the location of future built form. Buildings to the north of a street or public place, for example, should step back more than development located to the south, east or west.

Current DCP

The current DCP for Victoria Road Drummoyne requires buildings to extend to the property boundary along Victoria Road and Formosa Street. The site-specific controls with regards to street wall heights are as follows:

- The required street wall height along the western side of Victoria Road is four (4) storeys between Lyons Road and Church Street and five (5) storeys between Church Street and Day Street, and two (2) storeys between Day Street and Park Avenue (p F-140).

- The street wall height along the eastern side of Victoria Road is three (3) storeys between Lyons Road and Day Street and two (2) storeys between Day Street and Park Avenue (p F-141).
- The street wall height along the eastern side of Formosa Street is two (2) storeys (p F-140).

Issues and opportunities

- 139 and 121-125 Victoria Road have a higher street wall height (five storeys) than the stated street wall height in the DCP (four storeys).
- Upper storeys in recent developments have balustrades, partition walls and roof overhangs within the upper storey setback which creates a perception of a higher street wall height.
- The community has expressed concerns about the 'canyon' effect created by a high continuous street wall height, which is accentuated by the topography within the study area.
- The absence of street setbacks within the study area, an area already lacking public space, further prevents the development of a high quality public domain with landscaping, trees and outdoor seating.



A five storey street wall height is perceived as a higher street wall height due to topography and building articulation

Recommendations

The revised site-specific DCP for Victoria Road Drummoyne could include:

- Objectives i.e. new development is to help facilitate a gradual manifestation of consistent building scales and coherence along streetscapes; define and spatially enclose the street; respond to adjacent development; articulate building massing and help mitigate the pedestrian's perception of building height and bulk; and manage shadow impacts on streets, public places and nearby sites.
- Numerical standards, e.g. where built-to-alignments apply, buildings should have a minimum of 75% of their frontage built to the nil setback. The remaining 25% may set back up to 2.0 metres to provide areas for entrances, landscaping, bike parking, outdoor seating, etc.
- Any development above the street wall height applies a minimum upper level setback of 3m (depending on the site's orientation, greater setbacks may be required for development to the north of streets and public places to ensure sufficient sun access).
- Preparation of a built form section for inclusion in the DCP, showing the street wall height, upper level setbacks, minimum floor level heights and if applicable articulation/ projection zones.
- Encourage development which provides through site links from retail and commercial uses at ground level along Victoria Road to parking and access along quieter streets like Formosa Street. Where possible these links are to be at least 5m wide and provide a direct line of sight from Victoria Road and be accessible at least 12hrs a day (ie 7am to 7pm).

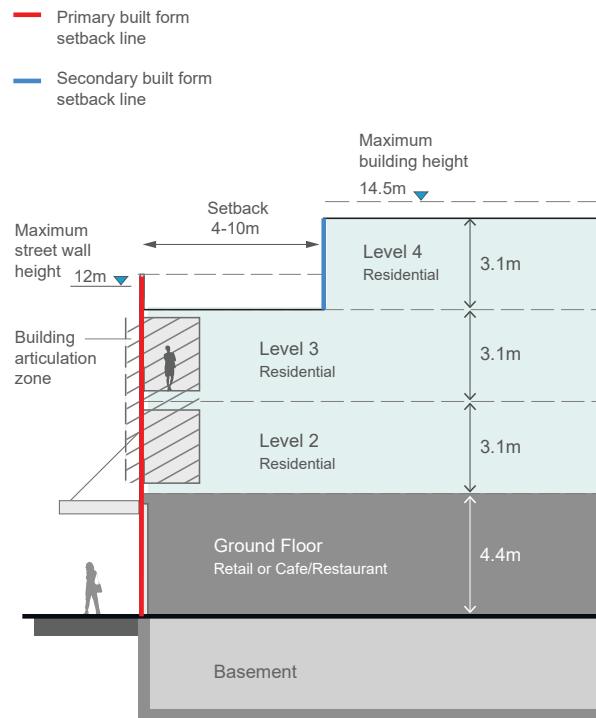


Figure 54 Example of a built form street interface section



Zero setbacks and narrow footpaths prevent the creation of a high quality public domain

D-05 Landscape quality

Landscape design plays an important role in the successful integration of new development. It enhances the appearance and amenity of the area, provides for recreation, preserves biodiversity and improves micro-climatic conditions. Landscape and built form needs to be designed together and landscaped areas should not be generated by 'left-over spaces' resulting from building siting. Attention to detail should be made while designing new buildings to accommodate strip planting, seating and lighting.

Current DCP

Part C5 of the Canada Bay DCP outlines controls for the preservation of existing trees and vegetation and recommended plant species. However, the DCP does not outline detailed landscape guidelines and requirements for new development or provision of street tree planting.

Issues and opportunities

The following issues are identified:

- A key issue within the study area is the lack of greenery. The streetscapes appear barren and are dominated by 'hard' materials such as asphalt and concrete. Also, the movable concrete median on Victoria Road prevents landscaping on the median.
- The zero building setbacks further prevent the provision of landscaping within the study area
- Trees that were removed along Edwin Street and College Street have not been replaced.



Existing condition of footpath along Victoria Road



Quality footpaths and paving

Recommendations

The revised site-specific DCP for Victoria Road Drummoyne could include the following:

- Preparation of a 'landscape priority streetscapes and areas' map
- Objectives i.e. new development is to promote high quality landscape design as an integral component of the overall design; improve the local micro-climate, provide native fauna and flora habitats and control climatic impacts on buildings and outdoor spaces; and allow adequate provision on site for infiltration of stormwater, deep soil tree planting, landscaping and areas of communal outdoor recreation.
- Maximum site coverage provision, e.g. ranging from 100% permissible site coverage along Victoria Road to 70% along Formosa Street.
- Provision of a 1m setback along Formosa Street and a 0.5m setback along Victoria Road at appropriate locations to allow for landscaping
- Encourage new development to provide vertical planting and facade indentation for landscaping.
- New developments on street corners to provide high-quality landscaping and public seating.
- Use of landscaping to separate pedestrians from traffic, especially in 'no stopping' zones.

D-06 Showcase lighting

Lighting enhances safety and discourages anti-social behaviour. Good light levels from a variety of sources, such as under awnings or shopfronts, increase the amenity of the footpath and enhances the overall sense of safety after hours. Uplighting and facade feature lighting is an effective way to highlight architectural landmarks and heritage buildings in the centre.

Night lighting of buildings and pedestrian paths along Victoria Road could provide an additional dimension to its built form. The creative lighting of buildings could also assist in promoting a strong civic image and improved pedestrian activity.

Current DCP

There are no site-specific DCP controls with regards to lighting for the study area. Some general controls identified are:

- Pedestrian ways and car parking, particularly those used at night, are to be direct, clearly defined, visible and provided with adequate lighting (p F-135)
- Ensure lighting is provided to all pedestrian paths, shared areas, parking areas and building entries for multi unit development (pE-99)

Issues and opportunities

- Lighting within the study area is minimal and lacks consistency.
- Limited use of lighting to highlight architectural features, fenestrations, etc.

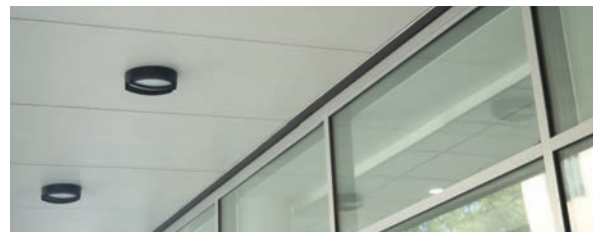
Recommendations

It is recommended that good practice design guidance (examples and photographs) is provided in the revised DCP to help visualise the benefit of various lighting sources and treatments.

The provision of lighting that is at pedestrian level, and designed to be compatible with heritage items and the desired character of the street would improve the amenity of the area, and increase the sense of safety and security for those who move through it.



Various lighting including facade and indirect lights create a pleasant atmosphere in the evening



Bunker lights are a simple, cost effective and vandal resistant option for under awning lighting



A combination of illuminated signage, uplighting of upper level facade, wall-mounted above awning spot lights and interior lighting on a heritage building

D-07 Sustainable design

Sustainable design refers to development that is environmentally responsible and resource efficient, reducing the overall impact on the environment. Development along Victoria Road is encouraged to apply environmentally sustainable design measures in the areas of energy and water efficiency, passive solar design, natural ventilation, stormwater reduction and management, solar access, orientation and layout of development, building materials and waste minimisation.

Current DCP

Although there are no site-specific controls for the study area with regards to sustainable design, the Canada Bay DCP promotes ecologically sustainable development (ESD) as one of the aims of the DCP (pA-10). Parts C4 Waste Management and C7 Flooding Control include objectives to foster the principles of ESD to minimise overall environmental impacts of waste and manage flood liable land respectively (pC-37, 50).

Sustainable design is encouraged for mixed-use and industrial development. Minimising the extent of hard paved areas and facilitating rainwater infiltration is outlined as a control within residential development (pE-118). Appendix 2 Engineering Specifications of the DCP includes controls on:

- Stormwater drainage and management including detailed considerations of on-site Stormwater Detention Systems, on-site Stormwater Absorption Systems, Charged Systems and Mechanical Pump-out Systems;
- Scouring, Erosion and Water Quality Control including details on Rainwater Re-use, Stormwater Drainage Design, Stormwater Pollution and Erosion Control, and Water Sensitive Urban Design (WSUD).

Issues and opportunities

Sustainable design elements have not been widely used in recent development within the study area, beyond those required by code.

Recommendations

The site-specific DCP could include:

- Objectives, e.g. new development is required to incorporate environmentally sustainable development (ESD) principles wherever possible; utilise sustainable construction materials and techniques; and encourage passive environmental design through site layout and orientation of buildings.

Passive solar design:

- Buildings are located so that they benefit from passive solar heat gain during winter months.
- Insulation is to be used in external walls and roofs to reduce heat escaping from a building in winter and to maintain a lower internal temperature in summer.
- All window and door openings are adequately sealed.
- Overhangs and shading devices such as awnings, blinds and screens protect from sunlight during summer months.

Energy conservation/ efficiency:

- Materials are selected considering their thermal performance.
- Solar hot water systems are encouraged.

Natural ventilation:

- Natural cross ventilation is optimised.
- At least 30% of all windows in a building are operable from the inside (by building users).

D-08 Facades and exteriors

Each building along Victoria Road makes a contribution to the streetscape character of the study area. The quality of these contributions vary widely and depend not only on the scale of the development, but also the detailed design of facades and exteriors.

The form, scale, proportion and pattern of building facades and elements, including doors, windows, balconies, roofs and decorative elements is important. So is the choice of materials, finishes and colours, which need to be carefully selected for their robustness, durability, energy performance and compatibility with the surrounds.

Current DCP

There are no site-specific controls for Victoria Road with regards to facades and exteriors. The following general DCP controls are applicable with regards to facades and exteriors within the study area:

- Infill development in the Drummoyne Commercial Area should include parapet skylines and use non-reflective materials (p F-138)
- New shopfronts should be designed to make maximum use of vertical elements, i.e. windows should emphasise a vertical proportion (height greater than width) (p F-134)
- Original early shop fronts in existing buildings should be retained and conserved (p F-134)
- If security shutters are required, they should be visually permeable (75% permeability) to allow viewing of windows and allow light to spill out onto the footpath. Open grilles (concertina style) are preferred. Note: Block-out roller shutters are not permitted (p F-134)
- Openable first floor windows and doors as well as balconies should be located so as to face the front or rear of the building (p F-134)

Issues and opportunities

- The scale of some new development is perceived as overpowering, partly due to choice of materials and colours
- Some shopfronts along Victoria Road are of poor quality
- Blank walls
- Limited use of public art and/or facade articulation

Recommendations

The revised site-specific DCP for Victoria Road could include the following controls:

- The composition of facades balances solid and void elements and does not display large areas of a single material, including reflective glass.
- Side walls are designed as an architecturally finished surface that complements the main building facade.
- Visually prominent elements such as balconies, overhangs, awnings, and roof tops are to be of high quality and low maintenance design.
- Roof plant, lift overruns, utilities, vents and other service related elements are to be integrated into the built form design and complementary to the architecture of the building.
- Along designated active frontages, permanent opaque coverings on windows and doors at ground level that prevent views into buildings are restricted to less than 15% of glazed area, including window film.
- Colours should be selected from a designated palette, with an emphasis on light/ neutral colours that harmonise with the heritage context.
- Highly reflective finishes and curtain wall glazing are prohibited.

D-09 Awnings

Awnings significantly affect the amenity and appearance of streetscapes and the pedestrian environment. Continuous awnings providing weather protection along pedestrian routes and are characteristic of shops, cafes, hotels and other buildings that invite pedestrian interaction.

Current DCP

The current DCP identifies the following:

- Street edge development is required along Victoria Road and Formosa Street (p F-140)
- A continuous cantilevered awning is to be provided on the western side of Victoria Road between Church Street and Day Street (p F-139)

Issues and opportunities

Discontinuous awnings along with inadequate building articulation, excessive length of shopfronts, and opaque displays reduce pedestrian interest.

While continuous awnings are useful for pedestrians, they restrict the growth of trees. New trees planted in front of the development at 81-110 Victoria Road have been achieved by providing breaks in the awnings.

Recommendations

The following design guidance and controls are recommended:

- Preparation of a street tree masterplan which would identify priority areas for street trees. Particularly along Victoria Road, street trees may only be possible if awnings have 'breaks' (see adjacent example photos and illustrations).
- Awnings are positioned to integrate with the facade design. Awnings should have a minimum height of 3.0m and a maximum height of 4.5m. Entry awnings may increase up to 5m in height to provide legibility.
- Low profile awnings with slim vertical fascias and/or eaves (not to exceed 300 mm) are encouraged. Steps for design articulation or to accommodate sloping streets are to be integrated with the building design and should not exceed 700mm per step.
- Awnings are designed to respond to the rhythm of shopfronts/ vertical articulation of the development and provide continuous weather protection
- Where possible, awnings allow upward views to the facade of a building through the use of transparent materials (e.g. patterned glass,

PLANNING FRAMEWORK



Example of an awning design with 'breaks' and street tree planting at 81-110 Victoria Road



Example of a continuous awning outside Harris Farm, Victoria Road



Figure 55 Continuous awnings



Figure 56 Awnings with 'breaks' allow for street tree planting

D-10 Advertising and signage

Well-designed signage adds visual interest to an area and contributes to a lively atmosphere. Signs are also necessary to identify buildings and businesses, and to communicate messages. Signage needs to be sensitively incorporated into the context, as the cumulative visual impact of many signs of varying sizes, shapes and colours is damaging to how the centre is perceived and detrimental to its character.

Current DCP

Part H of the Canada Bay DCP outlines requirements for signages and outlines controls summarised as below:

- The number of advertisements displayed on any site should be minimised in order to avoid visual clutter and duplication of message.
- Signs should be designed to provide clear property and business identification without dominating the site or the streetscape.
- Signage should be visually sub-ordinate to the building as a whole and its façades.
- Supporting structures should not dominate the sign, building or streetscape.
- Where commercial areas adjoin residential areas, signs should not be permitted on walls facing adjoining dwellings.



Intrusive signage such as the above example along Victoria Road is recommended to be prohibited under a revised DCP

Within conservation areas:

- Signs should be minimalist in their scale and design and be constructed with a high standard of materials and graphics.
- New signs should not be placed on the side of buildings and should not break the historic parapet or roofline.
- Illuminated signs are only permissible within shopfront/ behind glazing.

Issues and opportunities

Some signage along Victoria Road distracts pedestrians and blocks views to and from the upper storeys. There is no consistency in the design, size and type of signage along the road.

Signage if unconstrained and uncoordinated can make a streetscape cluttered and visually unappealing.

Recommendations

Building upon the current DCP controls, the following is suggested:

- Preparation of diagrams (see adjacent examples) that identify appropriate and inappropriate types of signage.
- Audit of existing signage within the study area, with any unauthorized signage to be subject to potential enforcement action.



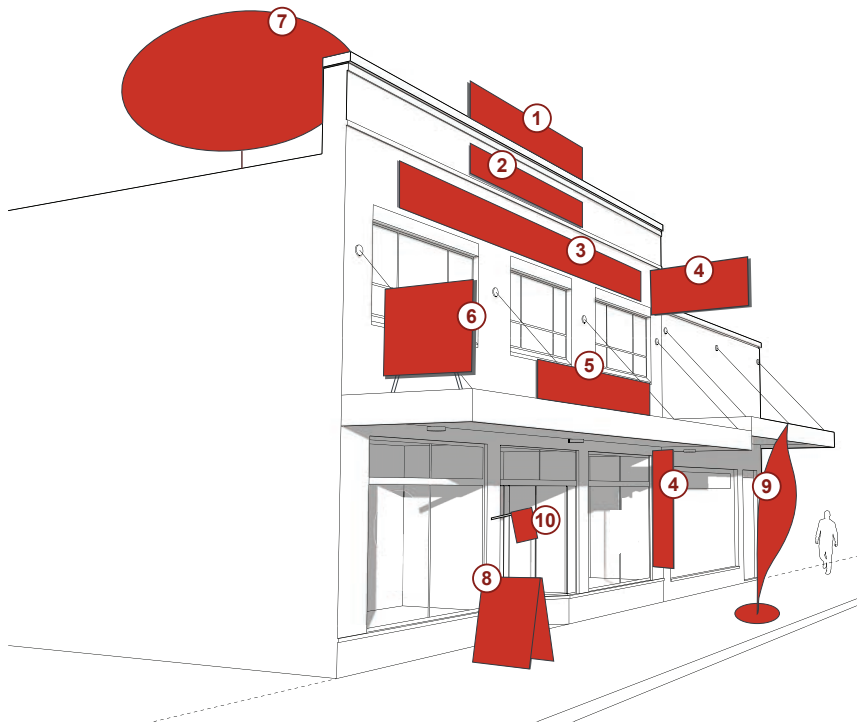
Examples of desirable advertising: transparent window signage (left) and an illuminated under awning sign (right)



Appropriate

- ① Fascia
- ② Top hamper
- ③ Window
- ④ Under awning
- ⑤ Side walls

Figure 57 Diagram illustrating appropriate signage options



Inappropriate

- ① Above parapet
- ② Parapet
- ③ Wall
- ④ Projecting off wall
- ⑤ Above fascia/awning
- ⑥ On top of awning
- ⑦ Rooftop
- ⑧ A-frame
- ⑨ Feather fan
- ⑩ Flagging

Figure 58 Inappropriate signage detrimental to the desired streetscape character



CHAPTER 8 TRANSPORT AND TRAFFIC

8-1 Introduction

Victoria Road is identified as a City Servicing Transport Corridor in State strategic planning documents and as such its value is in its role of moving large quantities of vehicles efficiently. The NSW Government has instigated a 'cluster' approach to transport authorities post the 2019 election, and the new operating model is intended to support integrated service delivery across all modes, with greater focus on transport's role in 'making places'.

These changes may influence how the Roads and Maritime Service (RMS) views main roads and how it seeks to integrate the needs of this road network with the needs of the communities these roads service and traverse.

Some of the most significant challenges facing the Drummoyne centre relate to parking and traffic, as these are challenges that cannot be overcome without significant support from a number of State agencies, and some of these challenges are unfortunately unable to be mitigated completely. Traffic congestion and the need to support traffic flows impacts on parking adjacent to retail, and on access to parking on-site when secondary roads are not available. The following suggestions were raised by the community and are provided to round out this review.

Council is currently undertaking a Parking Study that will investigate these issues in more detail and provide recommendations.



The width of Victoria Rd makes it difficult for pedestrians to cross within the green walking time (image source: Google)



Existing bus stop along Victoria Road (image source: Google)

8-2 Parking and access improvements

The design and quantum of vehicular access and carparking has a significant impact on pedestrian safety and the quality of the public domain. Vehicle access points need to be integrated carefully to avoid potential conflicts with pedestrian movement and the desired streetscape character.

The current DCP identifies that existing carparking areas in Drummoyne generally detract from the streetscape and landscape, and that there is an over-emphasis generally on vehicular passage through the study area, which detracts from what could be a much more pleasant pedestrian environment. Many existing parking access points are in conflict with pedestrian movement along footpaths.

Current DCP

There are no site-specific DCP controls with regards to parking and access for the study area. Some general controls identified are:

- Parking should not detract from the streetscape qualities, while meeting the needs of visitors and employees in the commercial areas (pC-27)
- On site car parking should be provided below ground or located within the building and well screened (pC-27).
- Bicycle parking should be made available to customers and staff (pC-28).
- Pedestrian access to buildings should be separate to vehicular access to the site (p F-135)
- All entrances and exits are to be made clearly visible (p F-135)

Issues and Opportunities

The following issues and opportunities were identified within the study area:

- Insufficient suitable parking spaces for the existing and projected population within the study area.
- The current parking restrictions of 1 hour parking between 8am - 6pm, or all day parking is not efficient.
- The intersection of Lyons Road and Victoria Road is a key intersection for pedestrians, especially due to the location of a major public bus interchange. However, the community has indicated that the signalised pedestrian crossing does not provide enough time for some pedestrians to cross the entire stretch of Victoria Road. This makes it dangerous for pedestrians to cross safely, and people sometimes have to dodge vehicles turning from Lyons Road onto Victoria Road, in either direction.
- Absence of dedicated cycle lanes or cycle parking infrastructure within the study area. There are a number of quieter and relatively flat roads suitable for cycling along the Drummoyne peninsula and neighbouring suburbs.
- There is an opportunity for providing a high quality interchange between cycling and buses to encourage a wider catchment to use public and active transport.
- The eastern side of Victoria Road has limited vehicular access and parking due to the restrictions imposed by the Roads and Maritime Services and the absence of a secondary road to the rear.
- The bus stops on either side of Victoria Road at the Lyons Road intersection are significant commuter stops, neither of which currently has a bus shelter available. The stop to the west side utilizes the awnings on the adjacent shops, but the east side is significantly exposed.

Recommendations

Given the significant impact that vehicular infrastructure and parking has on the local character, the following recommendations were discussed with the community:

P-01 Carparking management

To discourage non-residents from parking overnight along Renwick Street and Formosa Street, it was proposed to extend the hours during which the 2-hour parking restriction is operational, from 8am to 6pm, to 8am to 10pm. These areas have restricted timed parking to discourage commuter parking, whilst encouraging visitors and shoppers to access the centre.

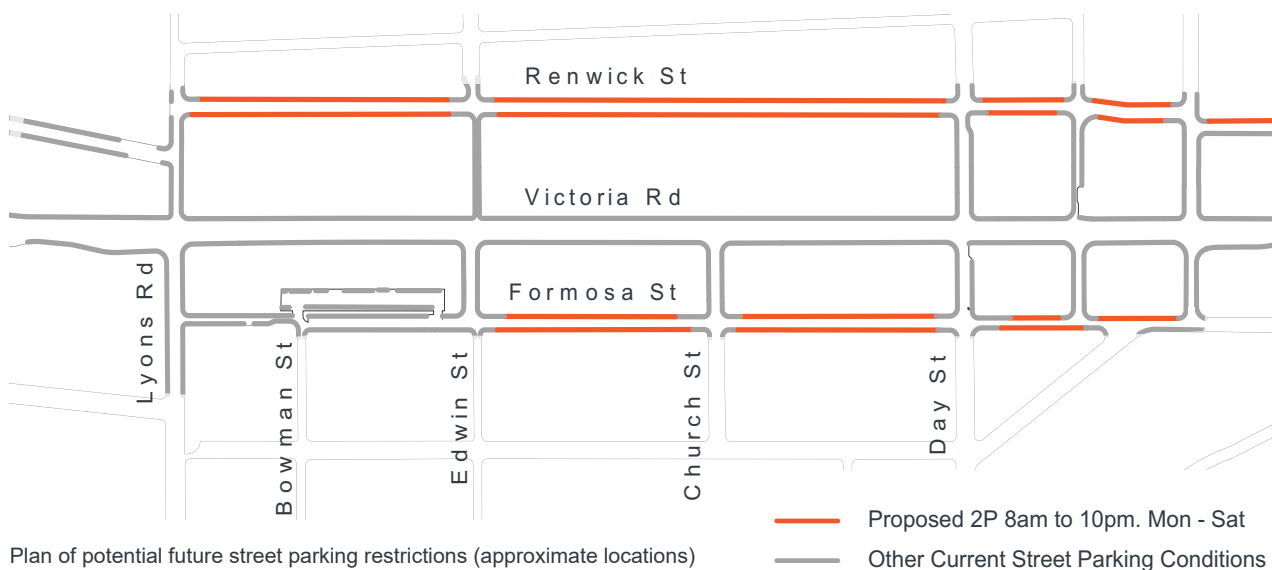
Extending the hours the restrictions are operational will ensure turnover continues into the evening. This idea would need to align with the parking study being undertaken by the Council and would also involve liaison with residents and traffic committee.

This management would increase the total number of users by increasing turnover in the valuable early evening for the local retail and dining/ night-time economy.

This idea was not generally supported by the community, as some indicated this could impact on access for evening visitors. Due to this concern it is considered that changing the time restrictions to 8am - 8 pm would be more suitable. It is recommended that this be investigated further as part of the parking study that is currently being undertaken.

Other carparking related suggestions include:

- Consider encouraging a common level (RL) for basement car parking development, so that adjacent small car parks can be linked in the future. This might also reduce the number of ramps and footpath driveway crossings
- Ensure that traffic and open space improvements are child-friendly. Currently, school children and carers are discouraged from walking along and across Victoria Road
- Encourage new land uses that do not require substantial car parking, such as boarding houses and local retail that can use local walking and buses



P-02 Bus shelters

Provision of a bus shelter at the intersection of Victoria Road and Lyons Road, adjacent to the Post Office, would improve amenity for the numerous residents who utilize public transport.

Bus shelters improve the amenity of users and increase patronage by improving the seating, comfort and security, and by increasing the visibility and cognisance of bus services to potential users.

Many in the community supported this idea, but there was concern about how a shelter could be incorporated in this heritage conservation area.



More bus shelters along Victoria Road will enable commuters, especially the children and the elderly, to comfortably use public transport

P-03 Cycle EOT facilities

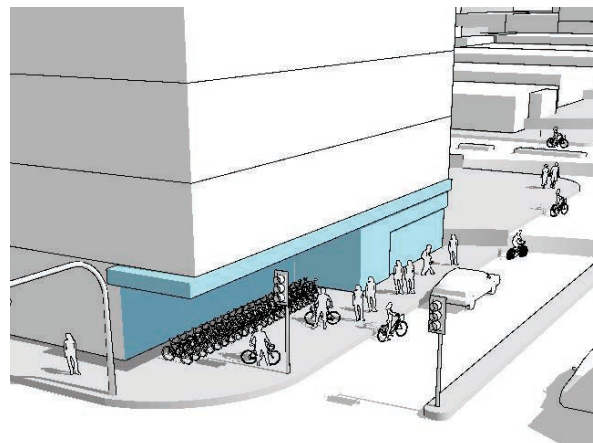
End Of Trip (EOT) facilities for cyclists encourage cycling by providing facilities such as secure parking, lockers and changing rooms. Providing these facilities near a bus stop on Victoria Road could encourage local residents to cycle and then use the regular bus service to commute to the CBD and other centres. This initiative would involve collaboration between private landowners, the Council and State Government agencies such as the RMS.

EOT facilities increase the catchment of buses from 800m walk to at least 2km by bicycle, an increase of over 600% in potential bus coverage.

This idea received conditional community support, with many liking the idea but indicating it would be better located closer to the Iron Cove Bridge and the Bay Run route.



EOT cycle facility at University of Newcastle



3D view of potential future End Of Trip cycle facilities integrated into future development

8-3 Traffic

T-01 Intersection upgrade

Intersections generally within the study area are problematic for pedestrians, due to congestion and physical issues such as changes in road width (as seen at Victoria Road and Edwin Street). The community raised a range of specific concerns including:

- The roundabout at Renwick and Roseby needs an upgrade - currently identified as dangerous by the local community.
- Lyons Road & Victoria Road intersection - right turn green arrow, two right turn lanes, and signage from further back to inform drivers.

Also of concern was the intersection of Victoria Road and Lyons Road, where the shear width of the road (6 active lanes + 2 slip lanes) provides a challenge. One suggestion to improve this involves a pedestrian countdown timer, which could be installed so that pedestrians and motorists can make an informed decision while crossing and turning respectively. This would improve safety and efficiency. This idea received community support.



An example of a pedestrian countdown timer in George Street, Sydney (photo source: Sydney Emerald City)

T-02 Tunnel project

As per the Roads and Maritime Services, Victoria Road is the busiest road in Australia. To divert traffic and improve amenity along Victoria Road, one option to consider is the construction of a tunnel for vehicles between the Iron Cove Bridge and Gladesville Bridge.

Constructing a tunnel would reduce vehicle numbers by allowing some vehicles to bypass this stretch of Victoria Road, although significant traffic would remain on-grade, such as buses and vehicles accessing Lyons Road.

The downside of this idea is that tunnelling would require the demolition of a number of properties to accommodate additional access lanes and tunnel ventilation. Tunnelling would also reduce visibility and patronage of retail and commercial establishments along Victoria Road.

There was strong community support for this idea, although this was not as strong at the community workshop, where greater information could be provided.

Any change to a State road of this magnitude, would require significant investment from the State or Commonwealth Government, and would take many years to be realised.



Aerial map of the Lane Cove Tunnel entrance (source: nearmap.com)

APPENDICES



APPENDIX A COMMUNITY ENGAGEMENT

A-1 Engagement plan

This Engagement Plan was submitted to Council prior to the commencement of the Consultation process.

Approach to Engagement

This Community Engagement Plan outlines the approach, methods and the process for the community engagement. It will ensure that the engagement is carried out in ways that help the community understand the constraints of the existing situation, so that the outcomes of the consultation process are realistic and achievable. This plan sets out who, how and when activities will occur so as to provide as much outreach as possible within the time available.

Community Engagement Objectives

The community engagement for Victoria Road, Drummoyne – Urban Design Review will be carried out in two stages: Round One – *Give us your Ideas!* and Round Two – *What do you think?* The engagement with stakeholders will be planned so 'as to ensure that the community is aware of the limitations to development in the area and outcomes of consultation are realistic and achievable'.

The community engagement program for the Victoria Road, Drummoyne – Urban Design Review will consider the following objectives:

- Involve the community in determining the future of Victoria Road, Drummoyne;
- Use traditional media and social media to maximise awareness and participation;
- Provide information about the planning process in suitable formats for non-technical experts to use in their decision-making process;
- Inform the community of the site constraints & limitations of the project;
- Include innovative strategies for engagement, including targeted consultation for special groups and demographics;

- Get unique insider perspective from local experts on the target area along Victoria Road; its assets, strengths and weaknesses;
- Encourage participants to generate ideas that while being inspirational, are realistic and achievable; and
- Generate excitement about the project and achieve stakeholder alignment and consensus.

Stakeholders

A critical step early in any community engagement process is stakeholder identification. Council and the project team will be required to identify individuals, groups and organisations who should be engaged with throughout the process.

The stakeholder groups for Victoria Road Urban Design Review are likely to involve:

- Decision makers – Councillors, Council officers, State Government
- People and groups with a (financial) interest in the CBD - property owners, tenants, business owners
- Community Reference Group
- Regular users - people who live or work in Drummoyne
- Occasional users - people who visit/use the facilities in the target area
- Other groups and communities which may be active in the area such as community groups, local indigenous communities, ethnic or faith-based communities and professional communities. The two groups that we have identified for specific intercept surveys and consultation event are:
 - Youth & Children who live in the target area or go to Drummoyne Public School or St. Mark's Catholic School
 - Seniors who live in or visit the area using the Bay Rider Shuttle service
- Roads and Maritime Services (RMS)

APPENDIX A - COMMUNITY CONSULTATION

Community Engagement Outcomes

All the content generated for and during the community engagement, is included in the *Engagement Summary and Analysis Report*. It will outline the engagement process and findings from both rounds of consultation, including the community's feedback for the design principles and ideas. It will also outline key themes that have emerged from the consultation and how these have influenced the recommendations of the study.

Managing content for media

Advertising the project and community engagement opportunities are key to successful engagement. To introduce the project, raise awareness and encourage community members to participate in the proposed engagement activities, traditional media and social media need to be used with equal urgency. Studio GL will provide the following content for media, submitting relevant material to Council for distribution:

| Activity | Description |
|----------------|---|
| Media releases | As Council has a dedicated Media and Communications team, graphics that may be included in media releases will be given to Council's media and communication coordinator for distribution in Council newsletters, website etc. |
| Advertising | Images for advertising in local newspaper(s) to advise the community about the masterplanning process. |
| Posters | A3 posters can be prepared for shops and service providers within the target area in Drummoyne so they can be displayed in shopfronts, library, other public buildings etc. These posters will advertise the masterplanning process and the engagement activities that people can get involved in, including Social Pinpoint, where the community can participate from the comforts of their own homes. |
| Social media | Digital versions of the posters can be provided to Council for distribution and upload to community website, Facebook pages, community forums and email lists. |

Community Engagement Round One

Tell us your ideas!

The purpose of the first phase of engagement for Victoria Road, Drummoyne will be to document the ideas & vision of the community, and to develop a deeper understanding of the main issues affecting this area. The key events will include a community & stakeholders workshop, an outdoor drop-in session, youth engagement event, an online survey and a few key intercept surveys in key areas at busy times. At these events, different facilitation techniques and activities will be used for successful consultation.

The outcomes of this engagement stage will generate a sense of likes and dislikes pertaining to Victoria Road, Drummoyne, a sense of future opportunities, a visual character aspiration board that outlines the community's vision for the neighbourhood via an image of what Victoria Road, Drummoyne could look and feel like in the future (future character).

The events for community engagement are:

- Community & Stakeholder Workshop
- Community Drop-in Session
- Youth & Children Engagement Event
- Online Engagement - Social Pinpoint
- Intercept Surveys

Community & Stakeholder Workshops & Drop-in Session

A workshop will be held with the general community members and invited stakeholders who have a specific interest in the redevelopment of the area such as Council officers, State Government officers, Chamber of Commerce representatives, business owners, representatives from sporting and educational facilities, police/emergency services representatives and other identified stakeholders. The workshop will follow an enquiry by design format and will seek specific information to assist the masterplanning process. It will be held at the Council Office Building.

The Drop-in session will be held outdoors in the IGA forecourt on a Saturday morning and will comprise informal and informational activities including interactions and conversations about the process, information boards and post-its for comments and ideas. This will enable the participants to share their knowledge, issues and aspirations for the community.

The workshop and drop-in will include the following activities. In addition, a one-on-one focus meeting will be conducted with the Community Reference Group.

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| Activity | Description |
|-------------------------------------|--|
| Placecheck mapping | Placecheck is an engagement technique to assist in the appraisal of places. Participants at the community drop-in sessions and stakeholder workshops will be provided with a base map (an aerial photograph) and colour-coded flags to highlight what needs to be improved and uncover new and/or reinforce known community aspirations and ideas. |
| Ideas Post-it Boards | Encouraging the drop-in session and workshop participants to go beyond likes and dislikes, the post-it boards offer opportunity to provide specific comments. A number of boards addressing different topics will be provided with ample space underneath them to stick post-its with specific comments related to the topics. |
| Visual Character Aspirational Board | Offered as one of the activities in the Community & Stakeholder workshop, it is a visual exercise where participants will be provided with a number of reference photographs relating to different themes. They will be asked to select images of visual character of built forms, public domain, interfaces, etc., to indicate what they would like to see in the study area in the future. |
| Live Graphic Scribing | In addition to the ideas presented on the boards, there will be live graphic scribing on site to collect ideas generated by the participants at the workshop; recording ideas graphically in a way that is easy for all to understand. This technique is used to extract key outcomes and identify issues that may still be unresolved. |

Youth & Children Engagement Event

To ascertain that the needs of young people are not only considered but that they are given a voice by including them in a formal process for community planning and public engagement processes, it is important to devise specific activities that cater to their interests and strengths. The design team will approach Drummoyne Public School and St Mark's Catholic Primary School for participating in the engagement.

The students will be encouraged to draw up their ideas for what they would like to see happen on Victoria Road, things that would make the street safer for them as well as the kind of buildings that they would like to see in their neighbourhood. Their teachers will be equipped with information on the project to share with the students. Council could sponsor a prize for the best drawing (TBD).

Online & Intercept Surveys

An online survey tool, Social Pinpoint, will be used to generate feedback from the community about various aspects of the target area to allow participation from the comfort of their homes. This is especially useful for those who are unable to attend the consultation in person or who would like to provide detailed comments.

In addition to this, Studio GL team will also conduct intercept surveys in key areas at busy times, to allow for quick and informal community input. The project scope allows for four intercept surveys, each an hour long at different times of the day/different days of the week (TBC): In the parking lot outside IGA, in Harris Farm (TBC), at the intersection of Victoria Road and Lyons Road, and on-board the Bay Rider Shuttle (for seniors). Studio GL staff is local to the area and as such will carry out these intercept surveys quickly and efficiently.

Community Engagement Round Two

What do you think?

The second phase of the process involves presenting of the principles, short-term and long-term projects and strategies under consideration for Victoria Road. Discussions will be facilitated to determine public opinion on the design components and options. Engaging with the community at this point builds support for the changes and brings all the parties together to understand the challenges and opportunities and what is required to realise the vision for the centre.

The materials for these presentations will be in the form of A1 sized posters (up to eight), clearly indicating the options being reviewed and the specific issues to be considered along with space for allowing feedback and comments.

The outcomes of this engagement stage will include feedback on how well the design components were received by the community and their comments on the ideas.

Community & Stakeholder Workshops & Drop-in Session

A workshop will be held with the general community and invited stakeholders (as in Stage 1). The workshop will seek feedback on the design ideas and the components in order to determine the preferred direction. It will be held at the Council Office Building.

The Drop-in session will be held outdoors in the IGA forecourt on a Saturday morning and it will comprise informal and informational activities. Information on the design options short-listed for the town will be presented and the community will be asked to provide specific feedback on the different components and options using dotmocracy and ideas post-it boards.

Councillor Workshop

A separate workshop will be held with local Councillors to give them an update on the project process, community engagement, design principles and ideas. This will be conducted in a similar manner to the stakeholder workshops using dotmocracy and ideas post-it boards.

The workshops and drop-in session will include the activities listed below. In addition, a focus meeting will be conducted with the Community Reference Group.

| Activity | Description |
|-----------------------|---|
| Dotomocracy | Using the facilitation technique of dot-voting, community members will be asked to give their opinion on the presented design principles, short-term & long-term projects and strategies under consideration for Victoria Road Drummoyne. They will be provided with green and red dots to determine their preferences. |
| Ideas Post-it Boards | Encouraging the drop-in session and workshop participants to go beyond likes and dislikes, the post-it boards offer opportunity to provide specific comments. A number of boards addressing different design components and ideas will be provided with ample space underneath them to stick post-its with specific comments related to the topics. |
| Live Graphic Scribing | In addition to the ideas presented on the boards, there will be live scribing on site to collect ideas generated by the participants at the workshop and drop-in. These vignettes will inform the design direction for the project. |

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Community Engagement Schedule

This is a draft schedule for the engagement events. The design team will meet with Council to finalise the dates, times and venues for the events, as well as confirm the activities planned within each event.

Round One

| Date | Key engagement actions | Location | Time |
|----------------------------|--------------------------------------|--|-------------|
| Mon, Oct 15 to Mon, Nov 12 | Online Survey Social Pinpoint Active | Online | Four weeks |
| Thu, Oct 25 | Youth & Children Engagement | In Schools | 10am - 12pm |
| Sat, Oct 27 | Community Drop-In Session | IGA Forecourt, near intersection of Victoria Road & Lyons Road | 10am - 12pm |
| Tue, 30 Oct | Community & Stakeholder Workshop | Greg Davis Stand, Drummoyne Oval | 6pm to 8pm |
| Fri, Nov 02 | Intercept Surveys with Seniors | Drummoyne Community Centre | 10am - 12pm |

Round Two

| | | | |
|-------------|----------------------------------|--|-------------|
| Sat, Mar 30 | Community Drop-In Session | IGA Forecourt, Victoria Road & Lyons Road int. | 10am - 12pm |
| Wed, Apr 3 | Community & Stakeholder Workshop | Greg Davis Stand, Drummoyne Oval | 6pm to 8pm |
| August 2019 | Councillor Workshop | Council Offices | TBD |

Community Reference Group

This group, comprising the Mayor, two Councillors, representatives of the Chamber of Commerce and local residents was convened by the Mayor to provide direct input into the Review. The minutes of the meetings are provided in this Appendix.

The information provided by this group has been incorporated into the findings of each Consultation round.

A-2 Engagement round one

Overview

Round one of the engagement process introduced the project to stakeholders and the community. Community members were invited to be actively involved in the consultation, in person and online, and their ideas, comments and aspirations were collected by the design team at Studio GL.

The engagement events included:

- Community Drop-In session
- Community & Stakeholder Workshop
- Youth & Children Engagement at Drummoyne Public School
- Online Engagement - Social Pinpoint
- Intercept Survey Session with seniors at Drummoyne Community Centre

The engagement activities presented at each consultation event were designed to gather community feedback in a variety of forms. The activities for engagement included Placecheck Mapping, Ideas Boards, Visual Character Aspirational Boards, Travel Routes Map and Live Graphic Scribing. Activities like the Placecheck Map and Travel Map provided place-based information where the participants marked areas of concern, their commute routes, places with future potential and other important information on the maps.

Participants submitted hundreds of comments during the engagement. A summary of the most repeated community responses is included in the Chapter 2 Community Engagement of this report.

Studio GL staff also met with the Community Reference Group (CRG) to understand their concerns and aspirations. The comments and ideas from the first meeting with the CRG, held on 20 December, 2018, have been incorporated into the community responses in relevant categories. For minutes from the meeting please refer to Page 116 in the Appendices of this report.

The traffic consultant also conducted preliminary discussions with Roads and Maritime Services about future vision for Victoria Road. Council staff also received a number of emails from community members outlining their concerns and ideas.

A summary of the engagement outcome from these events is included in Chapter 2 Community Engagement of this report. Following is a detailed recording of key comments received from the community during the drop-in session, community workshop, online engagement, workshop with children, workshop with seniors and the CRG meeting. It also includes the comments from the surveys and emails submitted to Council as a part of the process.

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Strengths of the area

| What are the strengths of the area? |
|---|
| Heritage charm, character and a sense of place - heritage buildings in the area including commercial, churches and beautiful houses |
| Plaza in front of IGA is the closest we have to a town square |
| Access to the Bay Run & waterfront |
| Health benefits of public amenities in the area |
| The area is pedestrian friendly and safe |
| Harris Farm is a great asset and has created a community feel in the area |
| Being a key street, transport to CBD or western suburbs is great via Victoria Road |
| Heritage houses on Renwick St are beautiful |
| Buildings along Victoria Road are a good mix of old and new |
| Pedestrian connectors through buildings work well |
| Heritage homes in Drummoyne are some of the best in Sydney |
| A central Post Office is good |
| People are friendly and interested in buying and promoting local businesses |
| Formosa St is often used to access the medical facility along Vic Road |
| Love the planters with poetry on them |



Weaknesses of the area

| What are the weaknesses of the area? | |
|--|--|
| Most repeated comments | |
| Dangerous intersections for pedestrians along Vic Rd: Lyons Rd, Edwin St and Park St | New development is too tall - they are affecting quality of life for adjacent properties (privacy, sun access, visual, overlooking, shadowing, parking and traffic) |
| Carparks are insufficient for the number of users and are in need of improvement | The infrastructure of Vic Road Drummoyne is not capable of handling more development |
| Parking system needs to be re-evaluated; 1hr (near medical centre) or all day parking (at Birkenhead and for business owners on Formosa) is not preferred | Height restrictions on new developments should be strictly adhered to, to minimise the pressures of people & cars on already overloaded streets |
| Edwin St is a dangerous cycle route - especially at the school start and finish times | New development needs to respect and respond to existing adjacent heritage buildings - especially Renwick St as there is no access Road between development & heritage homes, and the topography makes the new buildings even taller |
| Parking is increasingly difficult due to new developments not incorporating parking on premises | Vacant shops are not desirable - shops closing due to lack of access for visitors & landbanking by land owners |
| Lack of vision for Victoria Road has left it ad hoc and it has lost its community feel | New development (Tempo building) does not seem to follow the DCP regulations |
| Traffic flow on Formosa St is a mess - most people don't realise/obey the one ways | Shops along Vic Road have the feeling that all you see is the "back" of shops |
| Traffic flow across Victoria Road is difficult | Seniors mostly visit the Lyons Road Vic Road intersection for all their shopping/bank/services needs. Any new services related development will need to take access for seniors into account |
| Vehicles turning right on Victoria Road have a very long wait | Victoria Road needs trees and more greenery |
| Lack of cycleways | Drummoyne lacks a gathering place/passive recreation space away from busy Victoria Road |
| Cars queue across intersections at Edwin St during peak hours | Pedestrian environment along Victoria Road is not very desirable; footpath is in a bad state |
| IGA frontage to Victoria Road is a blank wall, and two of three pedestrian entry points to Sutton Place are blank-walled tunnels; very uninviting, and don't promote what's within | Footpath in front of the post office is too narrow and feels unsafe |
| Edwin St going from double lane to single lane creates traffic issues | Garden beds and new planting is all weeds and poorly maintained |
| Edwin St has a number of issues for motorists and pedestrians | |
| No more development - do not overdevelop and convert Drummoyne into a dormitory suburb | |

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| What are the weaknesses of the area? | |
|---|---|
| Additional comments | |
| Side streets get the overflow of parking and traffic from Vic Road | Remove the roundabout at the intersection of Day St and Formosa St because people queue in morning peak by driving down Sisters Crescent and up Formosa St, and it is unsafe for pedestrians |
| New development near Church St causing parking issues on College St | Heading south on Edwin St turning right, sun blinds drivers and there have been incidents of pedestrians being hit at the intersection |
| Clearways kill passing trade | Council needs to consider the future impact of Westconnex and the proposed tunnel near Iron Cove Bridge. Victoria Road is already a bottleneck and adding more bodies/cars and carparks is reducing the area's liveability. Perhaps spread the development to other streets |
| On-street parking on the eastern side of Vic Road has become difficult due to Birkenhead overflow | Local residents recognise and value the importance of maintaining Drummoyne's architectural and community heritage qualities. We have seen so many communities destroyed with new thoughtless development; If we don't protect our heritage, we wont leave one |
| Birkenhead Point compromises viability of shopping strip along Victoria Road and creates traffic chaos and overflow parking | The study should focus on the walkability of the public transport catchment as more pedestrians lingering in quality places will be economically essential to improve the quality of retail in the area. Extend the study from bay to bay, north / south |
| Ferry Lane used as parking for Birkenhead Point overflow | The CBA bank moving will cause a lot of inconvenience to seniors who use Bayrider shuttle service to access the Lyons Road Vic Road intersection |
| Island outside Destro's was changed - it worked more efficiently before | Trees that were removed in Edwin St, College St have NOT been replaced. All trees removed by the council were meant to be replaced |
| Concerned about the effects of WestConnex on Drummoyne and Vic Road | Remove the large garbage bin that blocks all sight of cars coming up Marlborough St as you exit Sutton Place - Exit closest to Lyons Road as it is very dangerous and you have to edge out to see around the bin |
| Bottle neck traffic from Gladesville to White Bay | Parking is a key constraint for businesses that rely on Council owned public car parks to service their parking needs |
| Pedestrian footpath access along Formosa St near Brett Park is dangerous due to the traffic from Birkenhead point | |
| The gutter at the Edwin St bus stop is too low and people waiting for buses city-bound get sprayed in wet weather as the water does not drain away properly | |
| The lanes of Edwin St west of Victoria Road need to be marked with very clear arrows as to whether they go right, left and/or straight ahead | |
| Heavy-vehicle use of carpark for deliveries is unsightly, time-wasting and unsafe for shoppers. Separate the two functions | |
| New development on the western side of Victoria Road is an eyesore | |

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Ideas and suggestions for the area

| What are your ideas & suggestions to make this place better? | |
|---|--|
| Most repeated comments | |
| Pedestrian bridges / underpasses across Vic Rd at key intersections - especially Lyons Rd intersection | Heritage facades and facades of buildings in heritage conservation areas should be retained and integrated in new development |
| Need more pedestrian connectors like at IGA & the one between chemist & Radiologist (with coffee shop Copo) could be developed into a nice walkthrough | Limit heights on eastern side of Victoria Rd to 3 to 4 storeys (adjacent to properties on Renwick St) |
| Need more parking and policing of parking times - reconfigure parking in back streets, build parking garages (Formosa St/ IGA) | Encourage night-time activity: restaurants & bars |
| New development should provide parking for its businesses and residents | Side facades of buildings need to be addressed - kookaburra is good, blue panels are ugly |
| Build carparks to accommodate existing and future parking needs - Formosa St | Encourage small local businesses |
| Making better use of the Council carpark (or redeveloping it into a mixed-use building with parking) or redeveloping the strip from 210-240 Victoria Road could provide more vibrancy | Victoria Road is too noisy for cafés and outdoor seating - consider mixed uses on cross streets |
| Install cameras at key traffic lights to discourage cars from intentionally blocking the intersection when the lights change | No more kitchen stores |
| Align Edwin St properly to improve safety and provide wider footpaths for more pedestrian amenity and canopy trees | Need more affordable and healthy food options |
| The car park on the Formosa St side must be changed to 45 degree parking as the Road is not wide enough to support a 90 degree bay causing continual traffic jams | Consider mixed zoning on nearby cross-streets such as Day St, Edwin St and Marlborough St |
| Provide more disabled parking | Need more shade (trees and continuous awnings) in the town centre |
| Retain and upgrade the existing village character of Drummoyne | Need more trees on footpath along Victoria Road & Lyons Road- to soften the aggressive traffic environment, mitigate noise & slow traffic |
| No increase in height limits for new development - it affects not just the properties in immediate vicinity, but there are implications for properties further away | Use public domain to display community history through furniture, displays and public art |
| Do not reduce or change the heritage conservation areas, including the heritage status of Renwick St | Need a small square or plaza in the town centre - perhaps Formosa St (convert a part of the parking to plaza with cafés and outdoor seating) |
| | Consider closing Church St and making it a public park/plaza |
| | Widen footpaths and use planters along Victoria Rd to serve as barriers between pedestrians and traffic |
| | 189 Victoria Rd has always been an empty lot - could become a public plaza |
| | As there is limited ground space, consider developing public open spaces at different levels |
| | Consider closing Formosa St from Lyons Rd to Bowman & convert to a public plaza & outdoor dining |

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| What are your ideas & suggestions to make this place better? |
|---|
| Most repeated comments (contd.) |
| Improve IGA forecourt and make it into a village centre - it used to have a tree in the centre - & make use of the first floor terrace |
| Vic Rd needs more colour and vitality in its public domain design |
| Church grounds & school grounds are a huge asset - develop strategic partnerships for community access |
| Gaps in awnings for new buildings can be used to add some green to the landscape |
| Additional comments |
| Improved cycle connectivity in the area - If Vic Rd is too dangerous for bikes then develop other streets as shared zones |
| Formosa St parking should be 45 degree - 90 degree parking causes traffic jams and unsafe driving against the one way direction |
| Signalised right turn from Lyons Rd to Vic Rd in both directions |
| Explore creative ideas for parking - Balmain lanes are half street half footpath |
| Bring back the ferry stop in Drummoyne for better transport options |
| Use Council Building carpark as a public carpark |
| Make pedestrian crossings longer and show time for pedestrian crossings across Victoria Rd |
| Introduce countdowns at pedestrian crossings |
| New development should be sympathetic to the existing and heritage buildings in its vicinity in terms of materials, scale and character |
| Drummoyne doesn't need an outside brand it's the community that defines the brand |

| |
|--|
| Building owners should be required to have some greenery on the roof |
| More community uses along Victoria Road: theatre, men's shed, community centre for youth |
| Need local service shops along Vic Rd (doctors, dentists, cafés, chemists etc.) and community art and culture uses |
| The site at the southern end of Day St intersection (Melwood) could be redeveloped |
| Why not redevelop the entire Vic Rd, Lyons Rd to Bayswater area (mostly Council owned) |
| Get rid of clearways |
| Council could encourage the use/refurbishing of unused sites such as the one near 187 & 70-72 Vic Rd - Increasing density & using unused space could maintain the heritage appeal of the area while increasing liveability |
| Sutton Place, the Council Car-Park adjacent to the IGA and surrounds can be redeveloped |
| Increase commercial and office space to generate local employment - do not want to be a dormitory suburb |
| Other suburbs that could be an inspiration: Concord, Five Dock, Lane Cove, Majors Bay Rd, Summer Hill, Stanmore, Mosman |
| Marrickville Victoria Rd Precinct - open public spaces, low-rise commercial hubs |
| Concord and Five Dock are good examples |
| Consider a green boulevard along Victoria Rd |
| Encourage use of sandstone as a building material |
| Council should consider adding more amenities in Drummoyne to respond to increased population Especially with reference to family services like childcare |

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| |
|---|
| Achieve an outcome that balances the needs of businesses with the expectations of the community |
| Consider incorporating pop-up shops to help with the revitalisation |
| Carefully considered future development to avoid creating a tunnel-like effect from large continuous built-form |
| Create through-site linkages and new laneways to improve the walkability of the centre |
| Increase the number of retail frontages addressing new lanes or Formosa Street |
| Consider constructing a tunnel along Victoria Road from Rozelle to Gladesville Bridge |
| Locate destination businesses & services near bus stops |
| Incorporate taller buildings to facilitate better design outcomes |
| Consider design competitions for future development to encourage new ideas & outstanding, memorable design |
| An opportunity for sporting facilities within the centre along with areas for children to play |
| The architectural style of future development should be such that it complements, rather than overshadows, the heritage buildings and character of the area |
| Reject any future development on eastern side of Victoria Road that exceeds the current FSR or the 12m height limit |
| Add more family-friendly venues, community amenities and places to congregate |
| Increase the FSR for domestic properties to enable existing residents to build granny flats and second floor extensions |

| |
|--|
| Develop sustainable, attractive mixed-use commercial and residential developments, like One Central Park in Chippendale |
| Make the provision of green space a requirement for future commercial developments |
| Business Chamber needs to offer greater incentives to food related local businesses and restaurants - from Rozelle to Gladesville Bridge |

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Ideas and suggestions outside the study area

| Ideas & suggestions outside the study area |
|--|
| Pedestrian and cycling access to Bay Run is difficult from eastern side of Victoria Rd |
| Bus stop on Marlborough near Lyons Rd is ill-placed & should be moved; when a bus stops here it blocks entry to / exit from the car park |
| Brett Park in need of maintenance: the equipment is old, broken, mouldy and unappealing for young children, and the shaded pergola for picnic use is covered in graffiti |
| Renwick St & Edwin St Intersection can be dangerous; can't see traffic coming down Edwin St if you are at the stop sign in Renwick St |
| Underground parking under Council chambers; it is a huge site under-resourced |
| Need another pool for the new residents - Drummoyne Pool is over its capacity |
| Add tennis courts to Drummoyne Oval |
| Introduce countdowns at pedestrian crossings |
| Therry St is narrow for cars & unsafe for pedestrians |
| Traffic around schools am/pm is chaos |



Visual Character Aspirational Boards

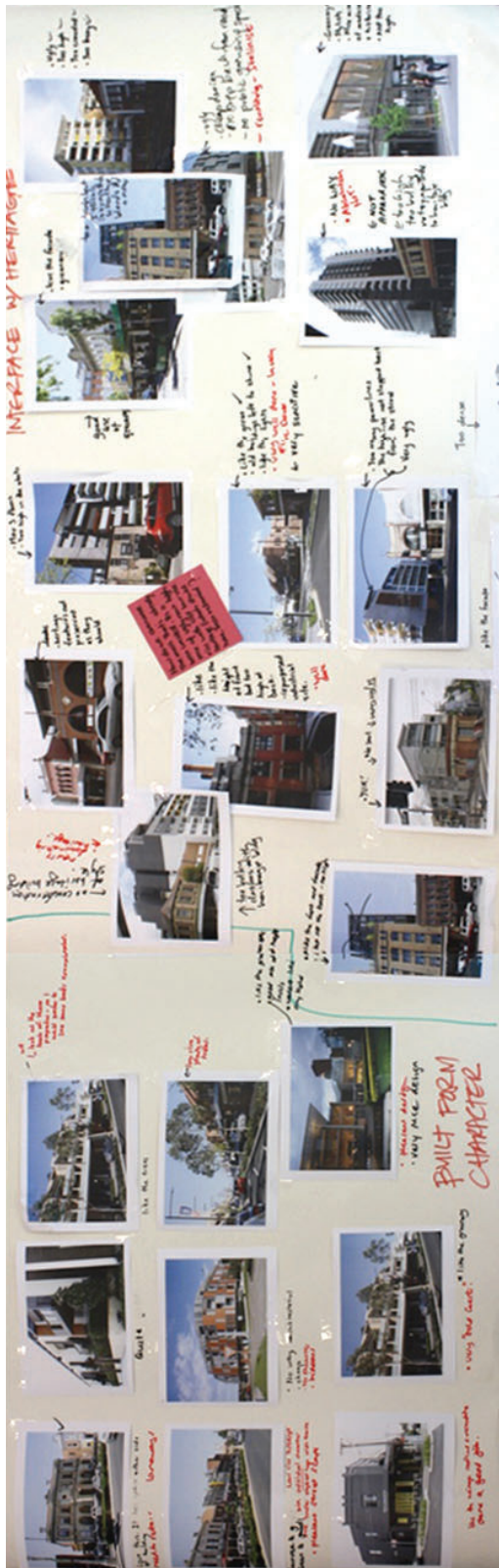
Offered as one of the activities in the Community & Stakeholder workshop, Visual Character Aspirational Boards are a visual exercise where participants were asked to select from a wide range of reference photographs relating to different themes (Built-form Character, Retail / Street Frontages, Steetscape / Public Domain, and Interface with Heritage) to indicate what they would like to see in the study area in the future.

The Visual Character Aspiration collages presented a collection of images and text that indicated what the participants would like to see in the study area in the future, as well as building forms or public domain characteristics they do not think appropriate for their centre.

The collages prepared by the community will be taken into consideration when determining the future character of the study area. Following are a few key comments from the boards. Also attached are photos of the boards at the workshop.

| Comments from Visual Character Aspiration Boards |
|---|
| New development - two to three storeys |
| Well-articulated façades, breaks in the building |
| Back of buildings should be well-designed, as that is what sometimes faces the residences |
| Good landscaping for buildings |
| Buildings with individual character |
| No blank facades; especially when adjacent to heritage |
| Upper floors need to step back from the Rd |
| Sympathetic to adjacent heritage in terms of height, bulk, materials and façade articulation |
| Consider effective use of corners (cafés and outdoor dining) to make Vic Rd more inviting for pedestrians |
| Lots of greenery - Trees and planters along Victoria Rd, greenery around existing awnings |
| IGA plaza could become a lovely community gathering space |
| Shaded walkway for pedestrians |
| Buffer between pedestrians and vehicles |
| Bike parking |
| Property fences could be greened with soft plantings |

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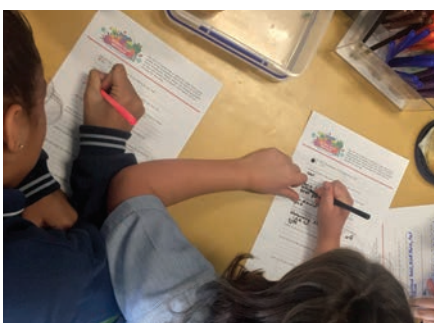
Visual character images assembled by the community

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Community Drop-In session



Youth & Children Engagement at Drummoyne Public School



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Community & Stakeholder Workshop

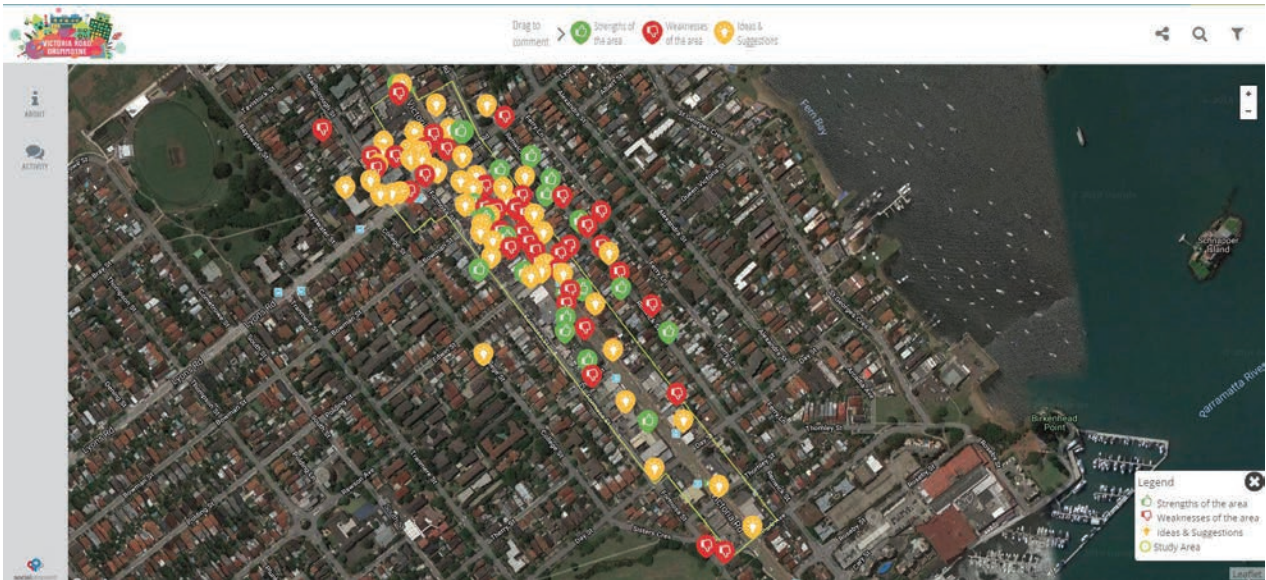


Intercept Survey Session with Seniors at Drummoyne Community Centre

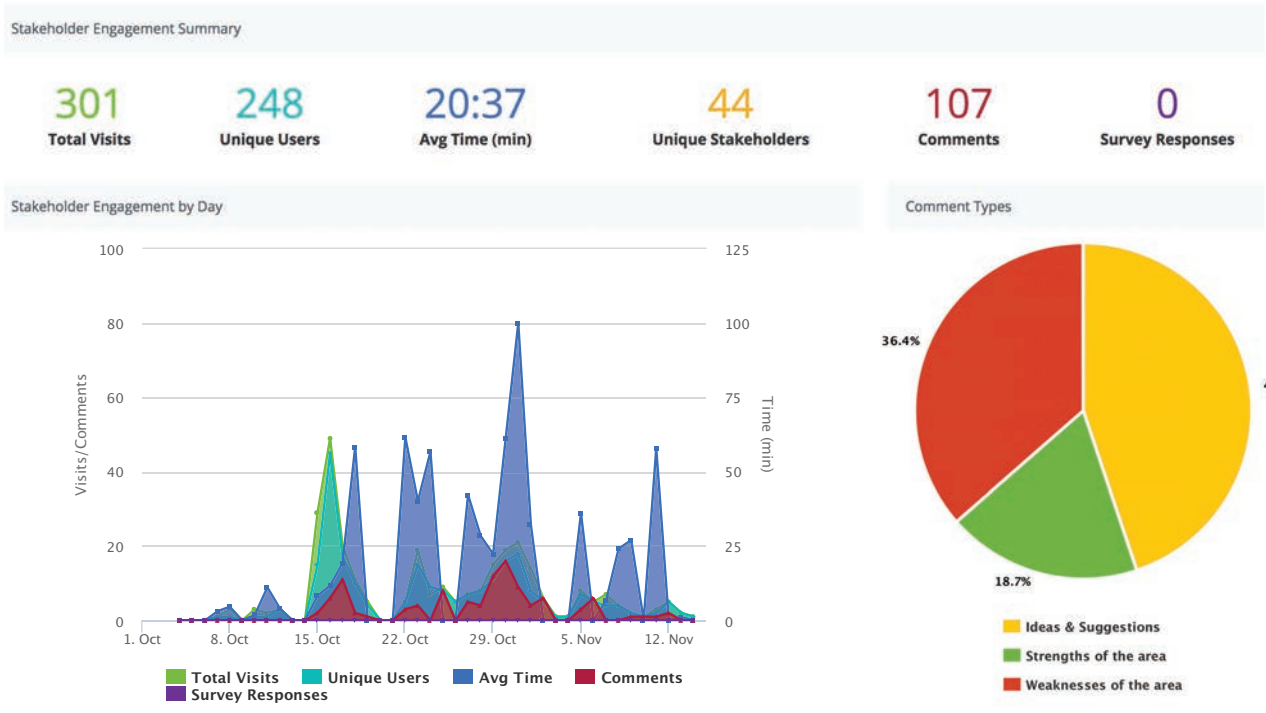


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Online Engagement - Social Pinpoint

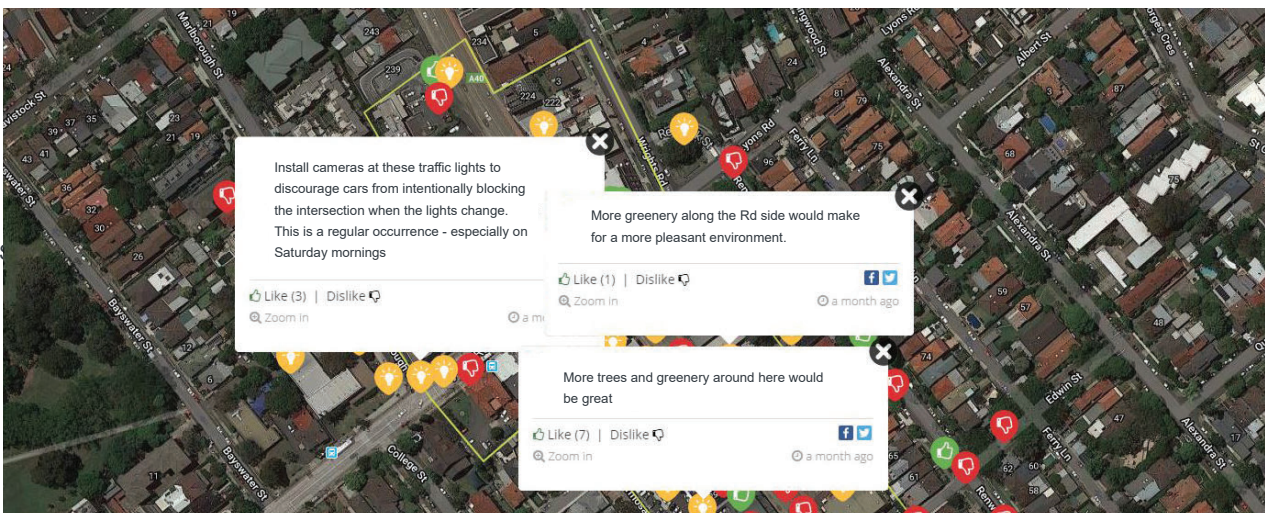
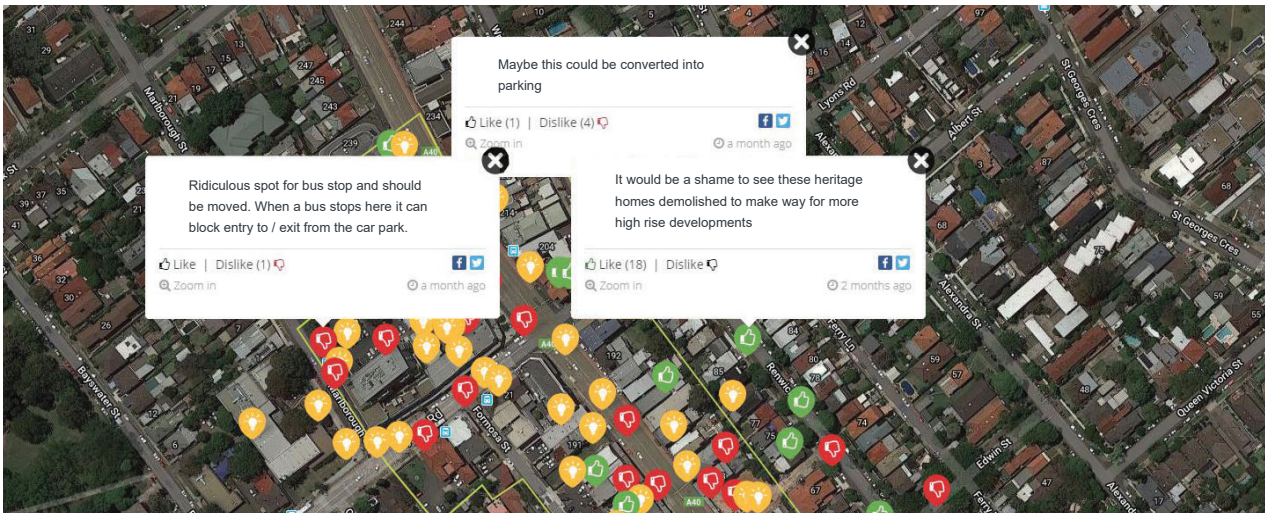
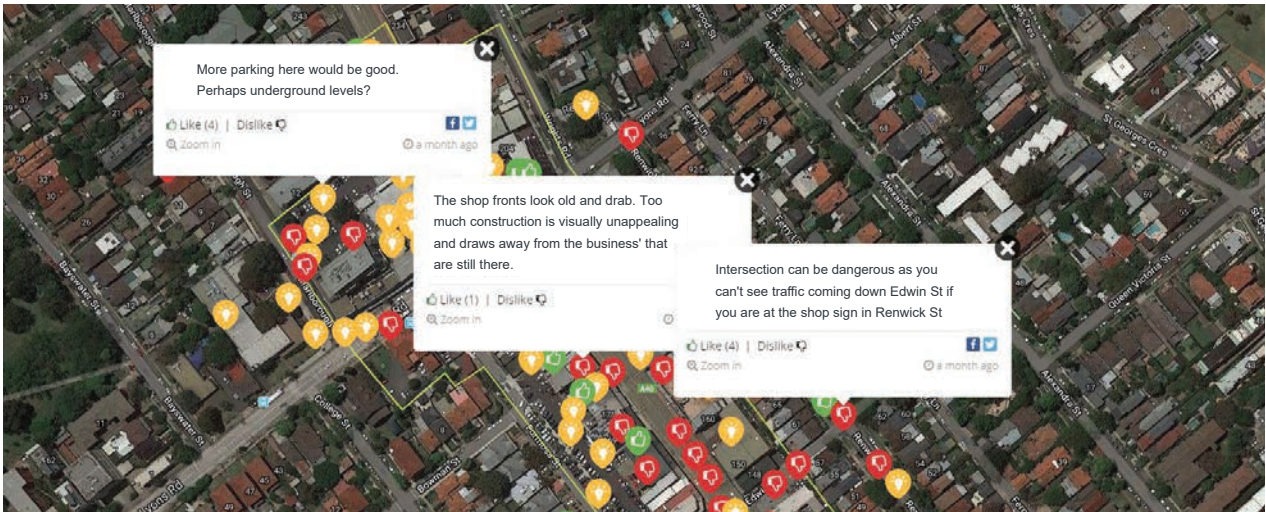


Map page for the online engagement



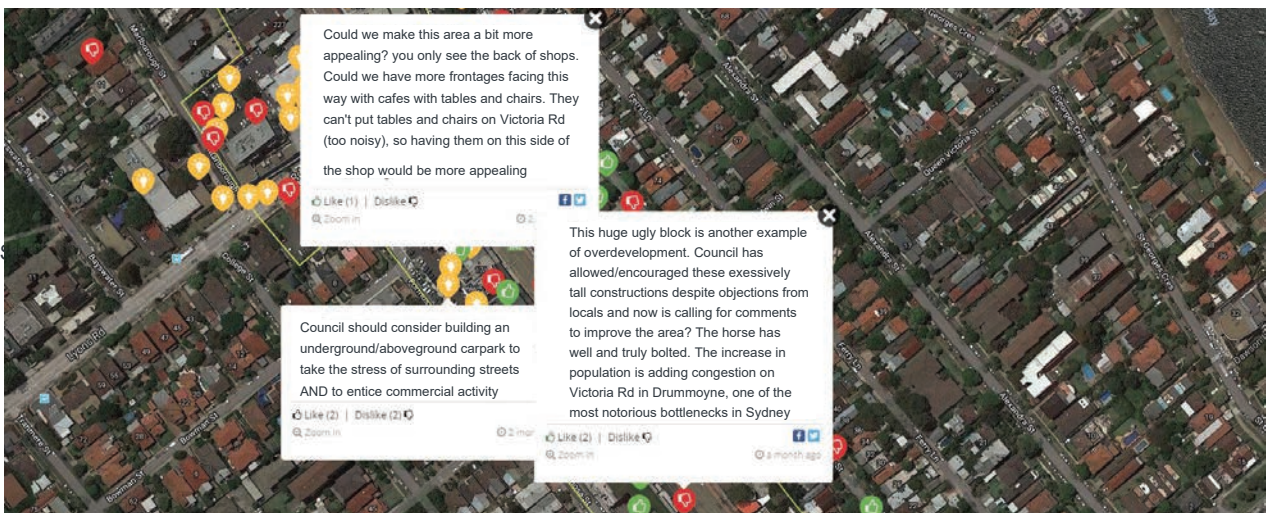
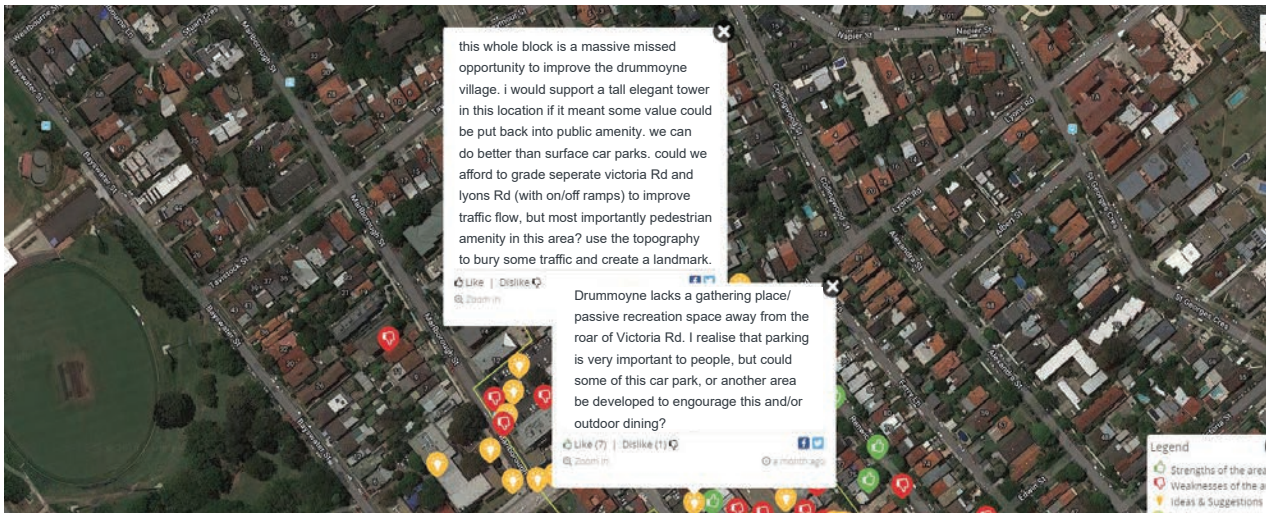
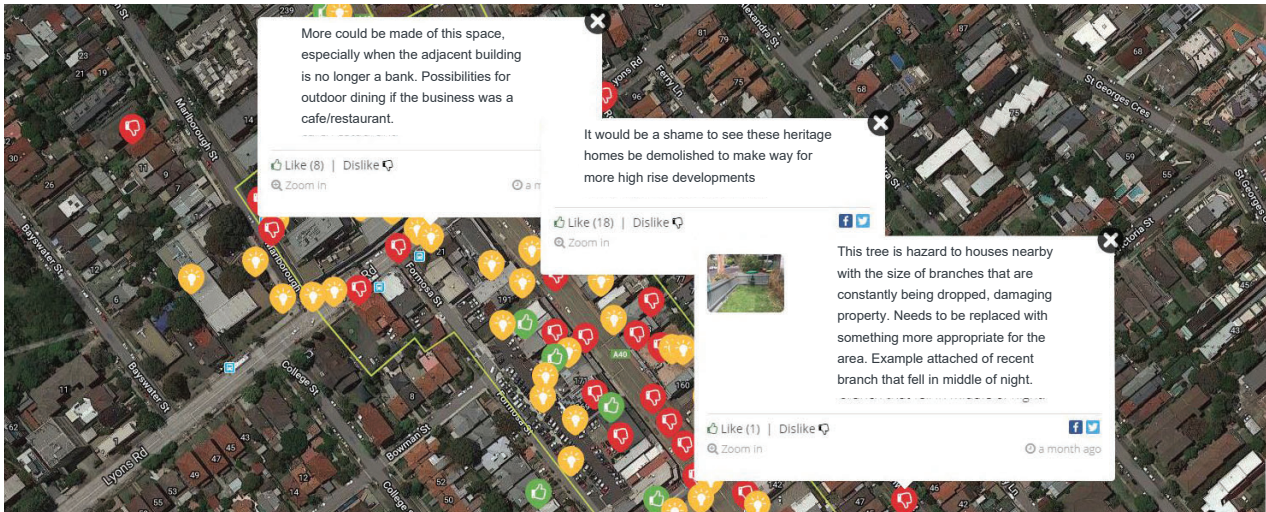
Overview of the online engagement

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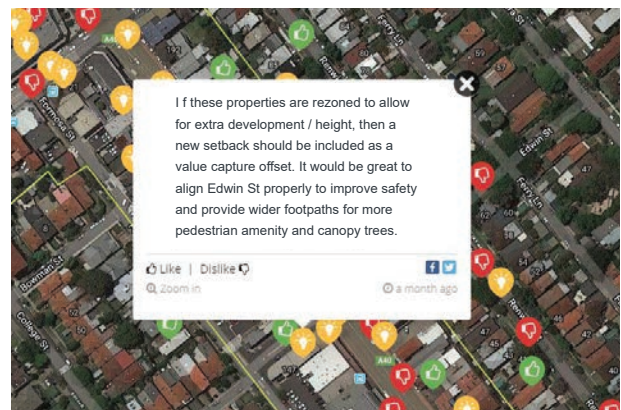
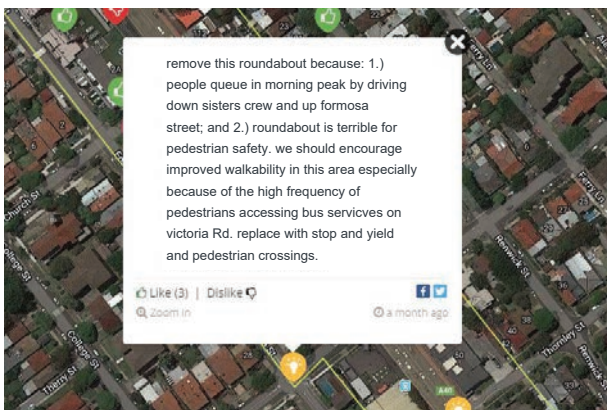
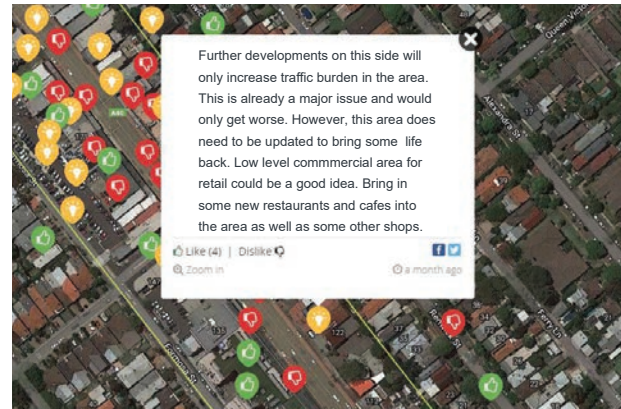
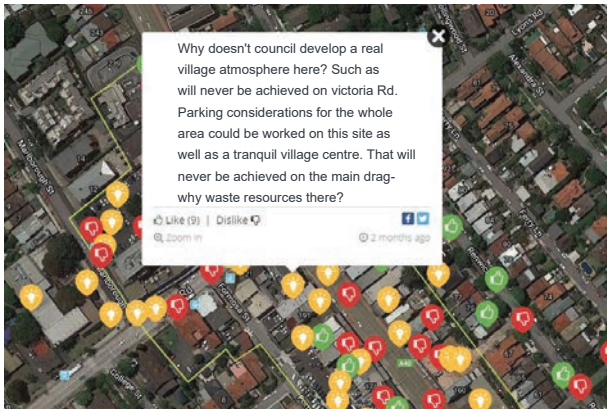
Screenshots of comments from the online engagement

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Screenshots of comments from the online engagement

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Screenshots of comments from the online engagement

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We received 107 comments from 44 community members on Social Pinpoint, the online engagement website. The comments have been incorporated in the key comments listed earlier in this section of the Appendices, and have been taken into consideration for extracting the most repeated comments included in Chapter 2 Community Engagement of this report. Following is a listing of all the 107 comments received under the key categories of strengths, weaknesses and ideas.

| What are the strengths of the area? | Up Votes | Down Votes |
|---|----------|------------|
| There is a lovely cluster of heritage buildings in this area. These buildings add a lot of character to the area and create a great sense of place. | 22 | 0 |
| Maintaining the character of Drummoyne is very important, otherwise we will just become another sole less suburb like Rhodes. Residents do not want the council to change the height restrictions in place for future development on the Northern Side of Drummoyne and there should be no change to the conservation status of any streets including Renwick Street. | 19 | 0 |
| It would be a shame to see these heritage homes be demolished to make way for more high rise developments. | 18 | 0 |
| Renwick St's heritage conservation rules make it one of the loveliest streets in the suburb and enhance the overall appeal of Drummoyne - the removal of such would be detrimental to the suburb's appeal, as would increased height of buildings on Victoria Rd. | 15 | 0 |
| Harris Farm is fantastic. It provides an experiential shopping precinct which draws support from locals and neighbouring communities. Let's think of this retail model and how we can apply similar businesses to our community to drive social and economic value for a bRd number of people. Say no to apartment buildings which do not align with the overall social good described above. Imagine small-med size businesses which are like craft beer - unique, sought after, premium priced, profitable. | 12 | 2 |
| A central Post Office is great. We've lost almost all the banks now, so it's good Australia Post is still here! | 12 | 0 |
| There are some lovely old buildings on Victoria Rd - they should remain and should not be removed for more soulless and unattractive, high-rise apartments. | 12 | 0 |
| As a long time resident of Renwick St I have already seen the negative impact on the safety and amenity of the area of the significant redevelopment undertaken at Birkenhead Point and the haphazard development along this area of Victoria Rd (with the exception of DMC, see comment). Council seems hell bent on destroying the reasons we chose to live here for the benefit of developers. Residents are the losers and developers are the winners. | 11 | 0 |

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| Suggestions of a car park are good, pending how high above ground the car park is (suggest only one level above ground) and also where the carpark is located. Given we already have carparks near the IGA and behind Bowen Island etc. I suggest we build on what we have rather than creating additional car parks. | 8 | 2 |
| The reason why we chose to live here - lovely heritage area. Council seems to not care about Drummoyne and are hell bent on destroying the area. eg allowing heritage homes to be removed, promoting development and adding disgusting lights to Drummoyne oval thus altering the whole feel for the park. | 6 | 0 |
| Renwick St should remain heritage listed as it maintains the character of the area. This side of Drummoyne should not be developed. | 5 | 0 |
| One of the differentiators for Drummoyne is its relatively intact Heritage features but many of the new developments overshadow and diminish this. The Sir Thomas Henley Estate is 48 contiguous semi's built (1906-1910) as a "Working Man's Estate to provide rental housing so that workers could live closer to their work. The Estate is along Formosa St (northern end) and Edwin St. Why not make a feature of it and the other nearby heritage areas to help provide a Village feel. | 4 | 0 |
| There are a few remaining institutions on the strip. We need to keep them and add to the mix of businesses. Both for day and night activation. | 4 | 0 |
| Renwick St has a lovely community and conservation area. It must not be developed | 4 | 0 |
| Harris farm is a fantastic addition to the area. High quality, sustainable business. Great service. Its a real destination,. If adding height to create better retail opportunities on Victoria Rd is required I would support it rather than fighting to retain amenity for a street in the area where I don't own a home. We need to get creative to have a good business centre in Drummoyne. | 3 | 1 |
| DMC is an ideal example of a thriving local business that undertook a sympathetic approach to its re-development of the adjoining building, installing associated medical services. Corner stone tenant, Alfred Imaging, specialists, physio. All services used by locals and not requiring excessive parking. | 3 | 0 |
| Higher-density, mixed-use development with on-site parking is hugely desirable. The recent development on Victoria Rd is one such example, especially with the interesting design of the building. One suggestion is firstly to keep having setbacks for these taller buildings, but also to investigate relocating electricity pillars and installing shrubs/trees to further improve amenity in the area. | 3 | 3 |
| I have lived in Renwick St over 40 years and I don't like what they plan to do with it. If I wanted to live in a decent high rise, no sun suburb, I would move. Is this what we want for Drummoyne. There should be no change to building height. And definitely no change to heritage rules. Leave it alone we don't want change. | 2 | 0 |
| Having a local post office. | 1 | 0 |
| The "massage parlour" is cool. | 0 | 0 |

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| What are the weaknesses of the area? | Up Votes | Down Votes |
|---|----------|------------|
| Let's stop focusing on creating high rise apartments destroying the nature of the community. We've had enough of greedy developers maximizing size and scale of their buildings for self-interest rather let's think to the future with a vision that considers the long term visual and practical sustainability - small scale office accommodation, boutique retail offerings, food providers, quality retail businesses. Let's go visionary to benchmark the area rather than slapping up an architectural mess. | 18 | 1 |
| The heights of recent developments on Victoria Rd in the heritage conservation area bordering Renwick are already over required heights and this has a massive detrimental effect on the residents whose properties border these buildings both in quality of living and property value. Any further increases would display a complete disregard for these families as well as detract further from the heritage street scape in Renwick St. | 15 | 0 |
| Sutton Place, the Council Car-Park adjacent to the IGA and surrounds are in desperate need of a new and invigorating design. | 13 | 0 |
| We already have far too much high density development in Drummoyne - losing its character and compounding traffic congestion. Any new development should be restricted in height to and include sufficient off street (underground) parking to cater for the residents/ guests. | 7 | 0 |
| The parking in Ferry Lane is atrocious. CBC has put parking restrictions in all surrounding streets except Ferry Lane. With apartments going in without parking these lanes are going to be impossible to navigate. You can't keep building without providing parking. Renwick and surrounding streets are becoming more congested with more development. | 6 | 0 |
| Trees that were removed in Edwin St have NOT been replaced. All trees removed by the council were meant to be replaced. | 5 | 0 |
| Parking is increasingly difficult due to developments not incorporating parking on premises. Harris Farm is successful as it holds its own parking lot and serves the community and those visiting well. Heritage conservation rules should be applied to all buildings not just residential homes. | 5 | 0 |
| Intersection can be dangerous as you can't see traffic coming down Edwin St if you are at the stop sign in Renwick St. | 4 | 0 |
| Parking is a struggle along Victoria Rd. | 4 | 1 |
| Between Lyons Rd and Birkenhead is now restricted parking - this has pushed all the commuters to now park between Lyons Rd and Drummoyne Ave. This area is already high density and now parking here is worse. | 4 | 0 |
| Very dangerous crossing Victoria Rd at this intersection - cars often try to rush though the light. Pedestrian need to be very vigilant. | 4 | 0 |

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| This 170 unit block is full of washing on balconies and even more empty shops. We don't want to have the same situation along Victoria Rd/Formosa St. | 4 | 0 |
| Any redevelopment of Victoria Rd that is not based around services required regularly by local residents is doomed to fail. Arterial Rds bounded by residential are not appropriate for retail. This means any developer requires significant height to maximise residences as part of the development. Lack of parking (no desire to lose floor space and/or to increase build cost by providing parking and access from Victoria Rd) means that the new tenants add their vehicles to the already over-crowded adjoining residential streets. | 3 | 0 |
| No parking for this development. Drummoyne already has a problem with no street parking all developments must have parking. | 3 | 0 |
| We already have too much high density development in Drummoyne - losing its character and compounding traffic congestion. We need some more green/open spaces interwoven with retail/café's etc. to offer greater variety of eating/entertainment options. Less apartment development so should restricted the height and think about alternative living options to the big 6 storey box. | 3 | 0 |
| All business seem to face away from Victoria Rd. Mainly due to parking and pedestrian issues. The strip looks dead cause most business are using the rear access to work from. The Rent is driving away businesses leaving plenty of vacancies. | 3 | 2 |
| We are seeing Drummoyne become a "dormitory" suburb with the prevalence of units along Victoria Rd. These do nothing to attract clients to the Drummoyne retail, except those open in the evening. Why not commercial and professional offices around a plaza instead of even more retail and units. This would generate additional clients during regular hours and also provide local employment opportunities. | 3 | 0 |
| Development on Victoria Rd is already having a negative impact on neighbouring streets in many ways: visual, overlooking, shadowing, parking and traffic. Height restrictions on new developments should be strictly adhered to, to minimise the pressures of people and cars on already overloaded streets. | 3 | 0 |
| The streetscape of Victoria Rd is completely built environment and very unappealing. I think there's a need to add greenery (e.g. hedges or shrubs in planter boxes along the length of Victoria Rd on both sides, especially in front of the taller unit complexes/commercial towers. There is a strong need to imbue the sense that Victoria Rd is a destination rather than just a thoroughfare. | 3 | 0 |
|over crowded adjoining residential streets. Further lowering the safety and amenity of these areas. PS this 'consultation' process is a farce. Designed to get 2 second 'sound bite' grabs to justify whatever action is decided. | 2 | 1 |
| Business closes... another is approved & moves in. Oh wait, it's another kitchen store. CBC need to look at the mix of business approved for operation in the Victoria Rd precinct. Rents are too high to sustain a truly community village mix. Do it & they will come :) | 2 | 0 |

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| <p>This huge ugly block is another example of over-development. Council has allowed/ encouraged these excessively tall constructions despite objections from locals and now is calling for comments to improve the area? The horse has well and truly bolted. The increase in population is adding to congestion on Victoria Rd in Drummoyne, one of the most notorious bottlenecks in Sydney.</p> | 2 | 0 |
| <p>There was a beautiful Moreton Bay Fig tree that added a lovely feel to Sutton place, this was removed. I understand there may have been issues with roots etc but NO tree has been planted to replace this tree. Sutton place now looks barren, hot and boring. I thought the council said when it was removed that it would be replaced!!!!</p> | 2 | 0 |
| <p>The lanes of Edwin St west of Victoria Rd need to be marked with very clear arrows as to whether they go right, left and/or straight ahead. Currently those going straight ahead can proceed from either lane creating the potential for an accident as they converge on the one lane on the other side of the Rd.</p> | 2 | 0 |
| <p>Heavy-vehicle use of carpark for deliveries is unsightly, time-wasting and unsafe for shoppers. Separate the two functions.</p> | 1 | 0 |
| <p>This tree is hazard to houses nearby with the size of branches that are constantly being dropped, damaging property. Needs to be replaced with something more appropriate for the area. Example attached of recent branch that fell in middle of night.</p> | 1 | 0 |
| <p>This intersection is a safety nightmare for pedestrians. Cars turning right from Edwin St onto Victoria Rd constantly try to beat pedestrians crossing the Rd. Needs better controls over lights to protect pedestrians from harm.</p> | 1 | 0 |
| <p>The gutter at the Edwin St bus stop is too low and people waiting for buses city bound get sprayed in wet weather as the water does not drain away properly.</p> | 1 | 0 |
| <p>Has council put any thoughts towards the future impact of Westconnex and the proposed tunnel near Iron Cove Bridge. Victoria Rd is already a bottleneck and adding more bodies/ cars, carparks is reducing the area's liveability. Spread the development to other streets. Maybe near where some of your councillors live! If you put in a high-rise you should put in a new accompanying park in the same area!</p> | 1 | 0 |
| <p>Renwick St is already filled with commuters, Birkenhead Points overflow parking, gym users etc. To add additional hight to development is only going to increase less parking for rate payers. This St was saved back in the 70's from being destroyed. Our suburbs does not have to become another Rhodes or Meadowbank. There is no need for higher development or destruction of our heritage homes.</p> | 1 | 0 |
| <p>This process is not designed to elicit informed and detailed responses. Hence the word count limit.</p> | 1 | 1 |
| <p>The shop fronts look old and drab. Too much construction is visually unappealing and draws away from the business' that are still there.</p> | 1 | 0 |
| <p>IGA frontage to Victoria Rd is a blank wall, and two of three pedestrian entry points to Sutton Place are blank-walled tunnels. Very uninviting, and don't promote what's within !</p> | 1 | 0 |

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| Pedestrian footpath access in this vicinity is atrocious and dangerous due to the traffic from Birkenhead point. Needs work. | 0 | 0 |
| Brett Park playground and surrounds are absolutely appalling. Investment is needed in upgrading these facilities, especially for the new hundreds of residents who have come into the area due to tempo development. Where are these children going to play? The equipment is old, broken, mouldy and quite frankly revolting and unappealing for young children. The shaded pergola for picnic use is a disgrace and is covered in graffiti. Not a good look! | 0 | 0 |
| Agree bus stop badly located. | 0 | 1 |
| Ridiculous spot for bus stop and should be moved. When a bus stops here it can block entry to / exit from the car park. | 0 | 1 |
| Insufficient parking, I often leave and shop elsewhere. | 0 | 0 |
| There's opportunity to give this little cluster of shops a feeling of a village by introducing distinctive shrubs/small trees to the pavement so that as soon as you turn from Victoria Rd you feel like you've entered a new mini precinct. | 0 | 0 |
| The high rise that already exists on Victoria Rd negatively impacts the street scape of the suburb due to both the height, the removal of some lovely old buildings and the ugly exterior of the new developments. | 0 | 0 |

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| What are your ideas and suggestions for the area? | Votes | Down Votes |
|--|-------|------------|
| Recent development has been accompanied by tree planting along the street frontage to Victoria Rd. This tree planting should be extended along both side of Victoria Rd so as to soften the aggressive traffic environment. | 22 | 0 |
| Drummoyne needs more tree canopy over footpaths particularly along Victoria Rd, Lyons Rd and the Harris farm to Tempo section of Formosa. Green leafy areas have positive impacts on residents health & well being, property prices and a more attractive streetscape will attract better commercial interests. We need to be aiming for the like of Majors Bay Rd through Concord. Particularly so as we are bombarded with air pollutants from one of the busiest Rds in Sydney plus flight path above. | 11 | 0 |
| If you like at areas like Lane Cove they have a very vibrant area just off their main Rd. Could the little area in front of IGA be enhanced ? It's looking very dated and old. It has the feeling that all you see is the "back" of shops. Could it be reversed and have a more welcoming feel ? Perhaps more cafés with tables, chairs in the area ? | 11 | 0 |
| Drummoyne is a very family-centric suburb but we don't have many family friendly venues - encourage family friendly style businesses into the area | 11 | 0 |
| Victoria Rd Drummoyne offers a major opportunity for council and the community to vision a revitalized street scape - cafés, restaurants, etc. Say no to increasing the number and scale of apartment buildings, they inject a visual eyesore, increase traffic into an already clogged corridor, offering no sustainable community value for the future. Comparisons to Parramatta Rd can easily be drawn and that corridor is an architectural mess. Let's think legacy we leave, not greedy developer gain. | 10 | 0 |
| More small bars in shopping precincts. | 10 | 0 |
| Why doesn't council develop a real village atmosphere here? Such as will never be achieved on Victoria Rd. Parking considerations for the whole area could be worked on this site as well as a tranquil village centre. That will never be achieved on the main drag- why waste resources there? | 9 | 0 |
| Let's plan for more green infrastructure along Victoria Rd to help improve air quality, capture stormwater, provide shade cover and improve the overall outlook. | 9 | 0 |
| More could be made of this space, especially when the adjacent building is no longer a bank. Possibilities for outdoor dining if the business was a cafe/restaurant. | 8 | 0 |
| Why try to create a vibrant hub on what will always be a drive through main Rd (just like Great North Rd) it doesn't really work, look at some of the other areas around, a great food destination - garden, produce maybe in the top section of Bayswater Park or Brett park. If its a destination people will come. Don't destroy what is good about Drummoyne the village atmosphere that sits behind Vic Rd. | 7 | 0 |

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| More trees and greenery around here would be great. | 7 | 0 |
| To reduce stress of parking for commercial, what about council building a underground / 1 story aboveground carpark? If parking for commercial is the issue, this would triple the amount of available parking without encroaching on residents. | 7 | 0 |
| Drummoyne lacks a gathering place/passive recreation space away from the roar of Victoria Rd. I realise that parking is very important to people, but could some of this car park, or another area be developed to encourage this and/or outdoor dining? | 7 | 1 |
| More street trees or shade structures for shade along Lyons Rd, where there are no awnings. Especially at the pedestrian crossing. | 7 | 0 |
| The whole strip along Victoria Rd looks barren and is very hot on summer. Well placed attractive trees would add much more appeal. Pity the reverse is happening in some of the lovely back streets where the tree canopy is gradually disappearing as the council cuts them down. | 4 | 0 |
| There must be a limit on the height of development. No more than 3 stories. | 4 | 0 |
| Further developments on this side will only increase traffic burden in the area. This is already a major issue and would only get worse. However, this area does need to be updated to bring some life back. Low level commercial area for retail could be a good idea. Bring in some new restaurants and cafés into the area as well as some other shops. | 4 | 0 |
| More parking here would be good. Perhaps underground levels ? | 4 | 0 |
| There will always be a significant amount of traffic, pollution and noise on Victoria Rd, so it could be worthwhile considering mixed zoning on nearby cross-streets such as Day, Edwin or Marlborough, especially a redevelopment of Sutton Place and surrounds. This could allow cafés & restaurants with outdoor seating. Greater availability of "third places" such as wine bars, pubs or community centres can only be desirable. Additional parking would also be useful, preferably underground. | 3 | 0 |
| Why not redevelop the entire Vic Rd, Lyons Rd to Bayswater area (mostly Council owned). The land slopes so it is easier to install parking, many streets for exit/entrance to parking and won't need to be as high to get a reasonable return for a developer. Also ensures minimum disruption to existing Drummoyne parking during construction thereby minimising impact on retail. Ensure it contains commercial and professional premises to increase local employment opportunities. | 3 | 2 |
| Install cameras at these traffic lights to discourage cars from intentionally blocking the intersection when the lights change. This is a regular occurrence - especially on Saturday mornings. | 3 | 0 |

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| <p>The unique environment & stringent heritage of the Drummoyne area is what makes Drummoyne special.</p> <p>Sure, development is inevitable. However, would it not make more sense to first spread the developments, rather than dumping them all in one vicinity, causing a major choke point.</p> <p>The electorate is spread far & wide. If the council approve a development in Drummoyne, they should also put the money into upgrading, improving and increasing the number of green spaces in Drummoyne.</p> | 3 | 0 |
| <p>remove this roundabout because: 1.) people queue in morning peak by driving down sisters crew and up Formosa street; and 2.) roundabout is terrible for pedestrian safety. we should encourage improved walkability in this area, especially because of the high frequency of pedestrians accessing bus services on Victoria Rd. replace with stop and yield and pedestrian crossings.</p> | 3 | 0 |
| <p>what is the objective of the urban design review? if it is to improve the public amenity and particular the pedestrian amenity of the area, then I'm not sure that your study boundary makes much sense. the public domain study and suggested improvements should focus on the walkability of the public transport catchment. more pedestrians lingering in quality places will be economically essential to improve the quality of retail in the area. extend the study from bay to bay, north / south.</p> | 3 | 0 |
| <p>This comment applies across the shopping area generally. The footpaths should be repaved with a pleasant uniform appearance and where feasible planting should be installed to soften the hard look. They have been neglected for far too long.</p> | 2 | 0 |
| <p>Increasing density and unlocking unused space could maintain the heritage appeal of the area while increasing liveability. Council could encourage the use/refurbishing of uninhabited/unused sites such as the one near 187 and 70-72 Vic Rd to increase supply of land. More supply allows cheaper rent and allows a greater variety of businesses could thrive in Drummoyne - not just wedding dress and kitchen stores.</p> | 2 | 1 |
| <p>Council should consider building an underground / aboveground carpark to take the stress of surrounding streets AND to entice commercial activity.</p> | 2 | 2 |
| <p>What about a Westfield here it would create more business and make Drummoyne a shopping destination that might encourage and attract more commercial on Victoria Rd with this kind of destination. We can't compete to be a nice village type destination do we have be different in another way.</p> | 2 | 13 |
| <p>Give Sutton Place more seating, shade and greenery. Its carpark has trees, but this public gathering space not a single one.</p> | 2 | 0 |
| <p>More greenery along the Rd side would make for a more pleasant environment.</p> | 1 | 0 |
| <p>We need more affordable and healthy food options. For those working in the area it would be nice to be able to go out for lunch or have a drink after work that is not the pub.</p> | 1 | 0 |

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| Council should not get bogged down trying to "over retail" the ground floors of new developments, whilst developers will promote mixed use, it is typically a residential building with vacant ground floor retail. Ie 162 Victoria Rd. | 1 | 0 |
| Maybe this could be converted into parking. | 1 | 4 |
| Remove the large garbage bin that blocks all sight of cars coming up Marlborough St as you exit Sutton Place. Exit closest to Lyons Rd. This is very dangerous and you have to edge out to see around the bin. | 1 | 0 |
| To help improve Victoria Rd the council should start by maintaining what they have started eg a lovely large tree was removed from behind our bus stop on Victoria Rd near Wolseley St, a tree and large garden bed were installed. The tree is now half dead and the bed is full of weeds. Flowers such as in Five Dock or Majors Bay Rd would help straight away. Why do the work and not maintain it. | 1 | 0 |
| Enhance the amenity of the area in front of IGA and encourage people to want to shop here. The back of the cafe is piled up with bins and milk crates, and there is no shade! What about bring the tree back and introduce some nice bench seating? Where is the investment?! Look to five dock and concord as inspiration. It's not that hard!! | 1 | 0 |
| "The issue of Parking is not valid as the new age of people in the area use Public Transport. The Transport system should be improved and parking should not be the sole issue of the Mayor. Development of anything over 3 stories is unpalatable. There must be limit on high rise as it impacts not just on the immediate area" | 1 | 0 |
| if the northern side of Victoria Rd is to regenerate well, improved pedestrian connection across the Rd will be essential to encourage good shops. walkability and the choice to walk is greatly increased when people given the option. this is a great location for a new pedestrian crossing, which could be triggered by demand like the one outside the medical centre (further west). | 1 | 0 |
| Could we make this area a bit more appealing ? You only see the back of shops. Could we have more frontages facing this way with cafés with tables and chairs. They can't put tables and chairs on Victoria Rd (too noisy), so having them on this side of the shop would be more appealing. | 1 | 0 |
| Having green space along Victoria Rd would be a better outcome. Public transport (green) would be a better solution. | 0 | 0 |
| "Weakness- difficult for parking strengths hub of health care service dentist medical hair nail salon massage,near Birkenhead point Suggestion increased time for parking on Victoria Rd - promote and support Drummoyne as a hub of beauty and health - set up Drummoyne Day event - support local business" | 0 | 0 |

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| The car park on the Formosa St side must be changed to 45 degree parking. The Rd is not wide enough to support a 90 degree bay and there is continual traffic jams associated. It would also fix the issue of people continually and unsafely driving against the one way direction. | 0 | 0 |
| "Local residents (and visitors !) recognise and value the importance of maintaining Drummoyne's architectural and community heritage qualities. We have seen so many communities destroyed in Sydney (and Melbourne, Perth etc) on the pretext of providing essential, affordable/ high density housing...it isn't, it is only about profit. If we don't protect our heritage, we wont leave one." | 0 | 0 |
| Population density is increasing rapidly with more apartments. Where are the associated increases in amenity? I particularly refer to child care. There is no long day care for under 2 years old in Drummoyne. Given there are so many families, the demand for this service is immense. Can council encourage the provision of this service? | 0 | 0 |
| this whole block is a massive missed opportunity to improve the drummoyne village. it would support a tall elegant tower in this location if it meant some of the value could be put back into public amenity. we can do better than surface car parks. Could we afford to grade separate Victoria Rd and Lyons Rd (with on/off ramps) to improve traffic flow, but most importantly pedestrian amenity in this area? use the topography to bury some traffic and create a landmark. | 0 | 0 |
| I f these properties are rezoned to allow for extra development / height, then a new setback should be included as a value capture offset. It would be great to align Edwin St properly to improve safety and provide wider footpaths for more pedestrian amenity and canopy trees. | 0 | 0 |
| Despite interesting shops, Victoria Rd downhill from Sutton Place & the post office lack foot traffic. Making better use of the Council carpark (or redeveloping it into a mixed-use building with parking) or redeveloping the strip from 210-240 Victoria Rd could provide more vibrancy. | 0 | 0 |
| There is so much concrete and hard surface in the Victoria Rd area. the new trees outside the new developments are welcome, but the building line setback could have been greater and the opportunity for canopy trees greater. we need to cool this place for pedestrians. consider a green boulevard from bay to bay. This should be our community's front address, something we can be proud of, not just a vehicle thoroughfare. perhaps new buildings Ok if we get improved shops / walking environment? | 0 | 0 |
| Building owners should be required to have some greenery on the roof, some kind of grass, cactus etc low maintenance to increase greenery around the area and also assist with sustainability goals of absorbing carbon and oxygen production. | 0 | 0 |
| The area in front of IGA is in desperate need of a revamp and could be a delightful, vibrant community hub. It actually already is, even though the surrounds are dismal. It would take very little effort to make it a desirable place to spend time. | 0 | 0 |

Victoria Rd, Drummoyne Urban Design Review

Community Reference Group Meeting No. 1

Date: 20 December 2018

Location: Halliday Room, City of Canada Bay

Time: 6 pm to 8 pm

Attendees

| | |
|---------------------|--------------------------------|
| Angelo Tsirekas | Mayor, City of Canada Bay |
| Michael Megna | Councillor, City of Canada Bay |
| Warwick Williams | Business Representative |
| Joe Cordaro | Business Representative |
| Phyl Blennerhassett | Resident Representative |
| Les Pall | Resident Representative |
| Paul Dewar | City of Canada Bay |
| Anthony Wynen | City of Canada Bay |
| Diana Griffiths | Studio GL |
| Felicity Lewis | Studio GL |

Apologies:

| | |
|-------------------|--------------------------------|
| Margaret Harte | Resident Representative |
| Daniela Ramondino | Councillor, City of Canada Bay |

Minutes

Welcome and introduction

Mayor, Angelo Tsirekas welcomed attendees and provided background to the Study. There are concerns about the general decline in the amenity of the area and an aim of the study is to identify opportunities to reinvigorate the area to create better outcomes for businesses and residents.

The Mayor noted that he had recently put a Mayoral Minute to a Council meeting which recommended that a parking study be undertaken. The study would seek to find opportunities to address concerns raised in relation to the provision of public parking.

Pecuniary, non-pecuniary interests and other conflicts of interest

- *Angelo Tsirekas* No land ownership. Resides on Victoria Road.
- *Michael Megna* No land ownership in Drummoyne.
- *Warwick Williams* Ownership of 192, 193 and 194-200 Victoria Road.
- *Joe Cordaro* Ownership of 169 Victoria Road and 6 strata lots at 159 Victoria Road.
- *Phyl Blennerhassett* Ownership of 81 Formosa Street.
- *Les Paul* Ownership of 11 Renwick Street.
- *Paul Dewar* No land ownership in Drummoyne.

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- *Anthony Wynen* *No land ownership in Drummoyne.*
- *Diana Griffiths* *No land ownership in Drummoyne.*
- *Felicity Lewis* *No land ownership in Drummoyne.*

Overview of the Study was provided by the Urban Design consultant, Studio GL.

Discussion

Vision

- There was general consensus that the public domain and built form outcomes in the study area could be improved.
- It was considered important to achieve an outcome that balances the needs of business with the expectations of the resident community.
- The ideas that are generated through this process will provide a foundation for improvements to the Victoria Road commercial precinct over the short to long term.

Business / Retail

- Need to find a way to make the area more attractive for *local* shopping so that residents don't leave the area to shop elsewhere.
- Business and landowners need to accept responsibility for the maintenance of their buildings/tenancies and keeping rents competitive.
- It was suggested that a greater resident population was needed to improve the viability of existing businesses and the potential for new businesses to be established.
- It would be beneficial if the area could provide places for experiences, perhaps fresh food and services, with popup shops to help with the revitalisation.

Parking / Traffic

- Parking was identified as a key constraint for business, who largely rely on Council owned public car parks to service the parking needs.
- There is more traffic both on Victoria Road and surrounding streets and there is not enough parking available for commercial and residential uses.
- The traffic, including 6 lanes of Victoria Road, creates an environment that is not friendly for pedestrians and is not safe. This issue is exacerbated by residents that choose to drive instead of walk.
- A positive is that there is good access, via public transport, to the city.

Development / Buildings

- Future development needs to be carefully considered to avoid creating a tunnel like effect created by large continuous built form similar to the new Tempo building. This building has a large block length without breaks in the building.
- New development should seek to create more/improved through-site linkages so as to break down the scale of buildings and improve the walkability of the centre.
- Opportunity should be explored to increase the number of retail frontages addressing new lanes or Formosa Street.
- The quality of buildings currently under construction on the eastern side of

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Victoria Road is poor.

- Victoria Road is not an appropriate location for housing given the noise and pollution impacts on residents.

East Side

- The eastern side of Victoria Road is distinctly different from the western side.
- It is not possible to provide vehicular access from certain properties on the eastern side of Victoria as they directly adjoin residential lots. Development of sites on the eastern side of Victoria Road also creates issues in relation to visual impacts and overlooking. It may not be viable to develop this side of Parramatta Road.

West Side

- The western side of Victoria Road has greater scope for redevelopment and change. This side has seen more recent development, there are newer commercial tenancies and more residential than the eastern side.

General

- The Council Administration Centre is outside of the study area but has the greatest potential for change.
- There are insufficient community facilities such as a library, plaza, gathering/meeting places and the distance to services, as well as topography makes access to services difficult, particularly for an aging population.

Ideas

Parking / Traffic

- There were many suggestions to improve the amount of public parking in Drummoyne and to address the existing traffic impacts.
- Suggestions included the construction of a tunnel from Rozelle to the Gladesville Bridge, expansion of Edwin Street, further policing, transfer of development capacity, locating destination businesses/services near bus stops, provision of temporary parking while sites with existing parking are redeveloped, encourage use of public transport with no cars and no parking permits.
- Encouraging pedestrian activity was identified as key to activate the area.
- In addition to traffic/parking issues, sites on the east side of Victoria Road will also have difficulty providing access for construction vehicles and it was suggested that access be provided from Renwick Street.
- Birkenhead Point has become an attractive destination for shopping which has created traffic and parking issues in the vicinity.

Development

- Attendees questioned the likelihood of properties being purchased and demolished on Renwick Street to facilitate access to properties on the eastern side of Victoria Road. It was suggested that there were many constraints to the

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- redevelopment of this side of Victoria Road.
- There would need to be sufficient incentives in place for residents on Renwick Street to sell and redevelop. Not everyone in the community is likely to support the redevelopment of land on Victoria Road where they deem their amenity will be unfairly affected.
 - It is important for new connections to be provided as part of future development (i.e. laneways) and that better architecture/design outcomes are achieved.
 - Consideration should be given to taller buildings within the current floor space ratio so as to facilitate better design outcomes.
 - Car free housing should be considered to reduce traffic on local roads.
 - There is a need for affordable housing and housing for the aged, however not at the expense of poor design.
 - Mosman, Cremorne, Majors Bay Road and Crows Nest were identified as town centres that have managed to retain local character and facilitate appropriate development outcomes.
 - Council owned land throughout the centre presents an opportunity to create change for the better.
 - Consideration should be given to a competitive design competition for future development. Such a process could encourage new ideas and outstanding, memorable, unique design.

Activation

- Pedestrian access must be improved and this could perhaps involve encouraging more pedestrian activity on Formosa Street, as well as opening commercial properties (including cafes/restaurants) to Formosa Street to minimise the impact of Victoria Road. This would need to be carefully managed to minimise any potential impacts to Formosa Street residents.
- There should be investigation into the use of vacant shops and the creation of a pop up community of food trucks with more diverse food options, plazas, gather spaces, nightclubs, and funky unique shops to improve vibrancy.
- The retention and adaptive reuse of heritage buildings was also identified as being important for local identity and activation.
- An opportunity should be identified within the centre for children to play.

Victoria Rd, Drummoyne Urban Design Review

Community Reference Group Meeting No. 2

Date: 7 February 2019

Location: Halliday Room, City of Canada Bay

Time: 6 pm to 8 pm

Attendees

| | |
|---------------------|---------------------------|
| Angelo Tsirekas | Mayor, City of Canada Bay |
| Warwick Williams | Business Representative |
| Joe Cordaro | Business Representative |
| Margaret Harte | Resident Representative |
| Phyl Blennerhassett | Resident Representative |
| Les Pall | Resident Representative |
| Paul Dewar | City of Canada Bay |
| Anthony Wynen | City of Canada Bay |
| Diana Griffiths | Studio GL |
| Felicity Lewis | Studio GL |

Apologies:

| | |
|-------------------|--------------------------------|
| Michael Megna | Councillor, City of Canada Bay |
| Daniela Ramondino | Councillor, City of Canada Bay |

Minutes

Welcome and Introduction

Mayor, Angelo Tsirekas welcomed attendees and provided an indication of the purpose of the meeting, which was to update the Group on the work being undertaken by Studio GL, and to provide an opportunity for the Group to gain an understanding of the ideas being developed and to discuss these ideas.

Presentation

Background

Presentation by Studio GL providing a summary of the project to date, including discussion of the site area and community engagement.

It was particularly important to note that:

- The current LEP allows the tallest building heights on the highest land.
- The fall towards Renwick Street accentuates the height of Victoria Road buildings.
- There is a clustering of activity at the northern end of the study area on the western side.
- There are a number of constraints to development/redevelopment which includes traffic and economics.

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General discussion

- Joe presented the mood of the Chamber of Commerce (comprising business owners and business tenants), which is that the East side have expressed concerns about lack of money being spent upgrading, owners not keen to invest, parking issues, not an attractive area, empty shops
- Warwick indicated land taxes are high, which places a burden on owners

Ideas

Studio GL presented a number of draft future development ideas proposed for presentation to the Community at the upcoming Community Consultation events. The presentation included general discussion by CRG members about the merits of each idea.

Carparking – multideck on Council owned site

- Consider commuter parking on roof level
- Consider long-stay options
- Traffic implications
- Impact on other tenancies

Intersection upgrade at Lyons Rd

- Consider an overpass, SGL raised issue of Heritage Conservation Area (HCA)
- Question asked whether HCA should yield to improved access, better outcome for community
- HCA can provide authenticity and uniqueness
- Concern expressed about potential 24 hr clearways (RMS)

Overpass at Edwin St

- SGL indicated that this ideas was considered but not progressing
- Noted that if overpass provided, on-grade crossing would be removed
- Cost prohibitive
- Requires consent of a number of land owners or acquisition of land.

Widening of Edwin St

- SGL indicated that this ideas was considered but not progressing
- CRG raised option for closure of Edwin St, potentially both sides
- Loss of buildings within Heritage Conservation Area
- Cost prohibitive

Public Space - Church St closure

- Expressed concern that this was removed from the 'action'
- Issues of access to taxis etc, that currently serve the adjacent buildings
- General traffic implications

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Public Space – Formosa St closure

- Would support more pedestrian friendly feel
- Locations of pedestrian crossings etc would need to be reviewed
- Traffic implications/management

Streetscape upgrade – Formosa St

- Power lines can be bundled
- Landscape blisters to provide 'greening'

Street Trees

- Supported
- Incidental benefit – will improve screening of taller buildings
- At present pedestrians generally prefer walking along Formosa or Renwick Streets

Bus Shelter

- May be difficult with narrow footpath width

Awning Controls

- Trees are considered to be visually more appealing
- Issues of who is using footpath, the elderly may require more protection
- Need to consider how to address heritage buildings
- Issue of trees obstructing views as they grow
- Vandalism / damage to trees has been an issue previously
- Generally considered that a continuous awning is not required

Signage

- Agreement that signage creates visual clutter
- Consider an audit of signage
- Controls would produce more consistency

Heritage

- Promotion of heritage

Lighting

- Improvement of public lighting
- Increased perception of safety
- Ability to put interesting architecture/heritage on show

Night Time Economy

- Use of blank walls to create identity

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- Especially appropriate for short to medium term

Public Art – on building facades

- Needs to be high quality
- Owners may have concerns about loss of building value
- Provides a decorative upgrade
- Concerns regarding how this can be achieved without costing owners corporations

Privately Owned Public Space (POPS)

- Potential existing locations which include Sutton Place and CBA building.

Building heights – East side

- Raise height from 12m to 13.5m, supports better retail ground floor outcome
- Issues of obsolete commercial space
- Consider reduced parking requirement
- Is retail sustainable / likely where there is limited or no parking
- Focused on Medical and Home Improvement businesses
- Reliant on public parking
- There used to be a system of 'buying' parking spaces
- Additional impacts of plant and equipment on top of additional height
- East side unlikely to be redeveloped
- Change to all residential and turn to combine with Renwick Street

Building heights – West side

- Changes to bring section between Lyons Rd and Edwin St in line with heights currently available on the ridges – up to 20m
- There is a Board sewer line (large) between the Council car parking (Formosa St) and the properties fronting Vic Rd
- More relaxed parking requirements would support this side of Victoria Rd
- Development can still be seen from and impact upon Renwick Street

Privacy screens

- Introduce controls for privacy screens and solid balustrades to east facing properties on the eastern side of Victoria Road that overlook Renwick Street.

Additional ideas / discussion

- Provide turn out bays on Vic Rd, similar to McDonald's for taxi's and car sharing
- Drummoine is mainly families, facilities need to support families rather than young professionals (Boarding houses)
- Go-get and other car sharing may be appropriate if parking requirements are lessened
- Micro units (modern Boarding houses) have benefits for different types of tenants
- Consider how Mosman has dealt with Military Rd, also an arterial Rd, great vision

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- Look at Paramatta Rd, Leichhardt
- The existing situation has constraints that are difficult to overcome
- Proposal to widen Victoria Rd by demolishing commercial property on eastern side
- Rezone Renwick St, to enable development
- RMS is likely to be considering 24hr clearways
- RMS vision for Victoria Rd is a s a movement zone
- Propose tunnel, extension of WestConnex
- Allow taller buildings with a smaller footprint to enable ground level public space
- Provide amenity – wider footpaths, open spaces, issues of access, steep hill is an impediment
- Consider Residential on ground floor (eastern side), issue of proximity to Victoria Rd, may reduce the number of dilapidated retail premises

Parking Study

A Brief is currently being prepared by Council's Traffic and Transport Department which will ask for expressions of interest to conduct a parking study.

A-3 Engagement round two

Overview

The second phase of the process involved presenting of the short-term and long-term ideas and strategies under consideration for Victoria Road to the community and stakeholders.

The materials for these presentations were in the form of A1 sized posters, clearly indicating the options being reviewed, some built examples of the ideas and the specific issues to be considered, along with space for allowing feedback & comments. A brief summary of the outcomes from Engagement Round One was also included.

Using the facilitation technique of dot-voting, the participants were asked to give their opinion on the presented ideas using green and red dots to determine preferences; green being for projects they liked and would like to see move forward, and red being for ideas they did not like. They were also provided an opportunity to present any new ideas they might have for the place. Discussions were facilitated to determine public opinion on the design components and options.

Participants voted on all the ideas and strategies and submitted a number of comments to support their votes during the engagement.

For the drop-in session, community members stopped by on their way to or from grocery, coffee shop visits and other errands. As such, the duration of the engagement of participants varied from a few minutes for those in a rush, to over an hour and half for those who came specially for the drop-in session. This engagement event was attended by an estimated 100 people.

For the stakeholder workshop, the ideas were presented in a more detailed format where Studio GL staff went through each concept and discussed it briefly. This allowed for a better understanding of what each concept entailed, thus allowing the participants more clarity in their voting using the green and red dots. The workshop was attended by 16 people.



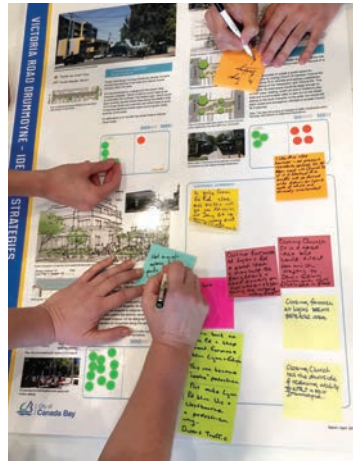
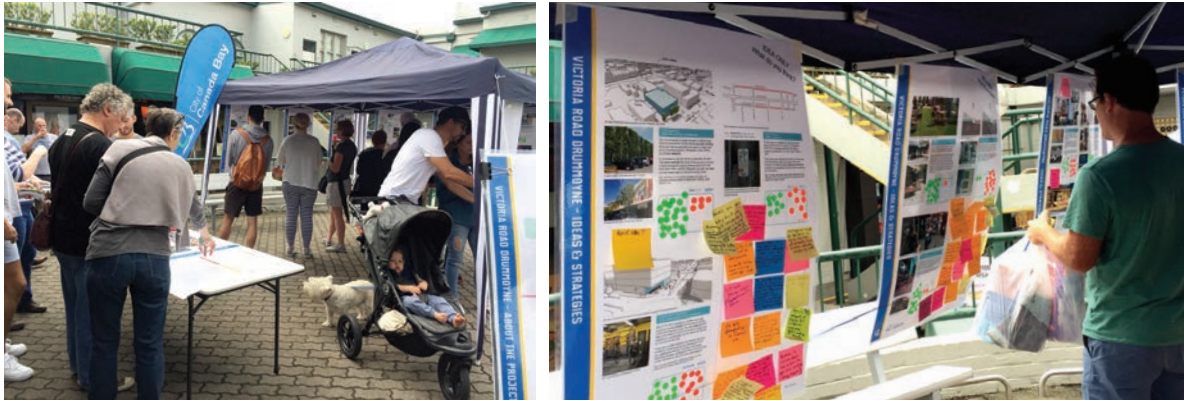
Promotion material

Studio GL staff also met with the Mayor and the Community Reference Group (CRG) to share community's feedback on the ideas and strategies, and gather their thoughts.

A summary of the outcome from the engagement events is included in Chapter 2 Community Engagement of this report. Following is a detailed recording of votes and comments received from the community during the drop-in session and the stakeholder and community workshop. It also includes comments from emails submitted to Council as a part of the process.

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Community Drop-In session and Community & Stakeholder Workshop



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Feedback and Detailed Comments from Community Drop-In session

| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|----------------------------|---|------------|----------|---|
| <i>Access and Movement</i> | | | | |
| 1 | Car parking- Multi-deck parking on Council site | 21 | 6 | <p>Good idea!</p> <p>Put multi-deck carparking on Council site</p> <p>Need to ensure height restrictions on the car park - suggest it goes underground as well as 1-2 storeys above ground</p> <p>It will block views from apartment block</p> <p>Future development is too high</p> <p>Building car parks create more traffic - bad idea</p> <p>More developments need to have parking but only for residents but also to service businesses they have at ground level. There is no parking at Tempo building for the shops on that site</p> |
| 2 | Car Parking- Traffic Management | 8 | 9 | <p>Why don't you let residents and developers build enough carparking when they submit a DA? They want to, but Council says no</p> <p>Residents should be allowed to get at least two visitor parking permits - this will allow residents to have dinner parties, family get together, etc.</p> <p>6pm is adequate at present - if it is 10pm, then need more guest parking permits</p> <p>For above comment - I agree with this one</p> <p>Current parking limit of 1hour in Sutton Place discourages shopping@ Sutton Court; increase it to 2 hours</p> <p>Perhaps have inspectors more visible, more often, giving out fines</p> <p>Council needs to stop approving apartment developments with no parking - existing parking restrictions will not manage this issue</p> <p>For above comment - Very true. Happening continually</p> <p>Don't like this idea because I'm not a resident - like to leave car here overnight when going into the city</p> |

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| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|-----|---|------------|----------|---|
| 3 | Intersection upgrade- Between Victoria Rd and Lyons Rd | 14 | 0 | Need right turn arrows for traffic going north out of Lyons Road as very dangerous at the moment |
| | | | | Need two lanes turning right from Lyons Rd to Victoria Rd |
| | | | | Pedestrian overpass at Edwin Street - safe for children and elderly |
| | | | | Slip lane from Lyons Rd to Victoria Rd was good; pocket park doesn't make sense |
| 4 | Cycle EOT facilities- At corner of Edwin St and Victoria Rd | 15 | 9 | A safe, wide and pleasant cycleway to the city would negate need for EOT in Drummoyne |
| | | | | Due diligence needs to be done on cycle facilities we already have before starting anything else |
| | | | | This is a nice idea, but not sure would be well enough used to justify the use of already limited public space |
| | | | | EOT is at the wrong end of CBD commute. Carparking away from businesses won't get used |
| | | | | The bus stop on the corner of Victoria Rd and Edwin St already does not cope with the number of commuters - EOT facilities would need to be co-ordinated with Sydney Transport |
| 5 | Pedestrian links- Upgrade existing links and add new links | 25 | 0 | Good idea to create more fine grain spaces in contrast to Victoria Rd |
| | | | | Good idea - more green more pedestrian spaces - less high rise |
| | | | | The link to Victoria Rd from IGA needs an upgrade, but more than proposed; it still looks dirty after upgrade |
| | | | | Owner of Sutton Place has neglected and caused our area to be downgraded |
| | | | | Morally wrong that Sutton place owner is increasing rent without any public benefits; from vegetables to wine, why was approval given |
| 6 | Safer crossings- Overpass at Edwin St intersection | | | Considered but not progressed |
| 7 | Safer crossings- Widening Edwin St | | | Considered but not progressed |
| 8 | Tunnel under Vic Road Drummoyne | 34 | 5 | This is RIDICULOUS. Why do residents have to be forced underground to a dead tunnel for the sake of cars. We should be making Victoria Rd more friendly for local pedestrians and forcing cars to respect the fact that they are traveling through a residential area |
| | | | | Public transport / metro not cars! |

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| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|----------------------|--|------------|----------|--|
| <i>Public Domain</i> | | | | |
| 9 | Public Space- Closing Church St | 33 | 2 | <p>More green space</p> <p>This is good as can create some public space. Its a pity that the monstrous development next door wasn't required to make more usable milling space for its residents and that now the rest of the residents have to enhance the private development by creating public space - 2 likes</p> |
| 10 | Public Space-Closing Formosa St | 29 | 5 | <p>Would help fix problems of cars driving the wrong way in one way sections of Formosa St</p> <p>Closing Formosa Street will cause even more disruptions to traffic flow</p> <p>College St is a no entry from Lyons Rd so this would be a further no access from Lyons Rd</p> |
| 11 | Streetscape upgrade- Along Formosa St | 27 | 0 | <p>Angle the parking along Formosa, Bowman, Edwin St</p> <p>Cycle lanes and bike racks please</p> <p>Underground power should have been condition of consent</p> <p>For above comment - Completely agree; underground power</p> <p>Underground power lines more cost effective at time of development - make it a condition of consent</p> |
| 12 | Streetscape upgrade- Along Victoria Rd | 29 | 0 | <p>Great idea! Reduce rates - incentive to encourage shops to maintain</p> <p>Looks good but the last street planting depended on shopkeepers watering etc. - a failure, cost to maintain</p> |
| 13 | Street trees- Along Formosa St, Renwick St and Wright St | 18 | 0 | <p>More green is better</p> <p>Trees near Taplin Park - no trees, nowhere to sit</p> |
| 14 | Bus Shelter | 21 | 2 | <p>Totally need commuter friendly bus shelters that 'shelter' you from rain and sun, and glass that allows you to see buses when seated</p> |
| 15 | Awning controls- for the entire study area | 8 | 9 | <p>Narrowed awnings can co-exist with trees</p> <p>Awnings give good rain cover as parts of Victoria Rd are No Stopping, and shoppers have to walk in all weather</p> <p>Awnings needed for aggressive morning sun. Continuous awning prevents stock damage - it is tough enough for retailers</p> |

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| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|-------------------|---|------------|----------|--|
| <i>Activation</i> | | | | |
| 16 | Showcasing heritage- Along Victoria Rd | 22 | 0 | <p>Preserve the heritage</p> <p>Refurbish heritage and old buildings rather than demolish and build new</p> |
| 17 | Showcase lighting-for the entire study area | 12 | 1 | <p>New power and light poles, please</p> <p>Can we improve the current street lighting hidden in trees</p> <p>What heritage features will be left on Victoria Rd to light?</p> <p>Waste of dollars. Better spent on what residents want - whoever came up with this should be sacked</p> |
| 18 | Night time economy- Along Victoria Rd | 28 | 1 | <p>Need to increase residential to stimulate night time economy</p> <p>Use solar lighting here?</p> <p>The night time economy is better located at Birkenhead Point (waterfront dining) not the residential street Formosa Street</p> <p>Formosa Street needs to become the hub of Drummoynes- cafés, restaurants, etc.</p> |
| 19 | Public Art- On building façades and public spaces | 8 | 13 | <p>Distracts drivers and slows traffic</p> <p>Every local hates the Kookaburra</p> <p>For above comment - No they don't!</p> <p>Public art is a waste of dollars. Spend on upgrading public facilities</p> <p>Yes - within heritage conservation guidelines</p> <p>Not a fan of the kookaburra</p> <p>The kookaburra is not a good example</p> <p>More pedestrian scale artworks -statues etc. - 1 like</p> <p>Needs to not clash with heritage features - shouldn't be used to justify reduced open space for milling about in new developments</p> <p>Sorry but this doesn't even look like a kookaburra - looks more like a Tawny Frogmouth</p> |
| 20 | POPS- Privately Owned Public Spaces | 19 | 1 | <p>Provided the POPS is not associated with additional height on existing buildings I'm supportive of the idea</p> |
| 21 | Sutton Place Community Hub | 15 | 1 | <p>Good idea - increase height to 8 storeys</p> <p>Like this idea</p> <p>Maximum 15m height - not fair to block city views for adjoining apartments</p> |
| 22 | Smart Poles - Along Victoria Rd | 15 | 2 | |

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| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|--------------------|--|------------|----------|--|
| 23 | Signage & Advertising - Along Victoria Rd | 8 | 2 | Reduce visual clutter |
| <i>Development</i> | | | | |
| 24 | Building heights- Update LEP controls for east side of Victoria Rd | 7 | 52 | <p>The sunken floor is greed, not an issue with planning heights - 1 like</p> <p>Council have to approve within their own rules - currently this is not happening</p> <p>We were assured that the development of the old RSL which exceeded planning was a one off</p> <p>Heritage houses and community feel shouldn't be sacrificed for developer profits</p> <p>I get progress...however, heritage conservation - without growth we are losing our unique environment</p> <p>Like the idea of creating consistency along Victoria Rd than more domestic heights on rear streets but suggest greater lot setbacks along Victoria Rd</p> <p>The heritage look and feel is valued - why compromise this by increasing height</p> |
| 25 | Building heights- Additional heights along west of Victoria Rd | 6 | 42 | <p>Keep height limits as they are</p> <p>How many times do we need to tell you we don't want to raise heights before you take tat off the table</p> <p>Development currently go above planning - an increase to planning will lead to an increased height in waiver requests</p> <p>Purchased in Tempo building for the view. Anything too high (over 13.5m) will be an obstruction and an eyesore</p> <p>High rise is revolting. The draw-card of Drummoyne is community vibe and historic charm - tall, impersonal buildings take this away</p> <p>How was an apartment building without parking approved? Corruption, abuse of power?</p> <p>No increase in heights on west side. If the developer has a big enough site, they can already go to 20m</p> <p>Victoria Road is one of the busiest roads; why do you want to add residential here at all - there are beautiful places in Drummoyne that are within walking distance to Victoria Rd</p> <p>Prefer lower heights - like the staggered setbacks</p> <p>Is this to justify the development at the ambulance site on Victoria Rd?</p> |

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| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|-----|--|------------|----------|--|
| 26 | Privacy screening- Update DCP controls | 2 | 15 | <p>Good - but should not be used to try to justify further height to new developments</p> <p>No increase in height restrictions</p> <p>Limit east side development to 3 storeys per DCP - 1 like</p> <p>The issue is they are approving set backs - blocks of 4 storeys +</p> <p>Just step building east of Victoria Rd with multi-storey (3+) and no car parking</p> <p>Great idea. Again, if its design is kept within heritage guidelines - stop approving high rise which impedes on privacy of heritage as per residents and owners</p> <p>View into rear gardens and homes - invasive and against heritage guidelines</p> <p>Drummoyne Avenue to Lyons Road - cant get out of the carpark</p> <p>Horrific traffic - cant get out of the carpark</p> <p>Please limit height of apartments. We do NOT want beautiful Drummoyne to turn into high-rise ugliness</p> <p>Stop approving high-rises along Victoria Rd - that will prevent overlooking issues</p> <p>Why can new development build right to boundary as pictured - shadowing not addressed?</p> <p>Increase the height limits to look over not into Renwick St properties</p> <p>How is the Council so adamant in not allowing changes to heritage houses, but allows for demolition of such houses to make way for driveways to apartment blocks on Victoria Rd? e.g. Renwick St. Smells of corruption and inequality</p> <p>City of Canada Bay is not focusing on Drummoyne, instead on Concord. They don't even clean our streets</p> <p>Any open space that becomes public on top of buildings on the eastern side increases the loss of privacy and overnight noise in family backyards</p> <p>Development still getting built without considering the angle</p> |

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Feedback and Detailed Comments from Stakeholder & Community Workshop

| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|----------------------------|---|------------|----------|---|
| <i>Access and Movement</i> | | | | |
| 1 | Car parking- Multi-deck parking on Council site | 7 | 0 | At least two underground parking levels to reduce the bulk Multi-deck car parking at Sutton Place or Council chambers is a good idea If you improve the carpark, you could include a small bike storage shed |
| 2 | Car Parking- Traffic Management | 1 | 6 | It is already a problem for visitors to park; don't make it worse as it discourages community interaction |
| 3 | Intersection upgrade- Between Victoria Rd and Lyons Rd | 1 | 2 | Countdown good idea Good idea of timer - need to use green right hand turn arrows |
| 4 | Cycle EOT facilities- At corner of Edwin St and Victoria Rd | 0 | 3 | Bike paths more important for locals Cyclists would not be coming to Edwin St in numbers - to justify this, maybe Day St on east side |
| 5 | Pedestrian links- Upgrade existing links and add new links | 2 | 0 | Further improvement to crossing the D mart |
| 6 | Safer crossings- Overpass at Edwin St intersection | | | Considered but not progressed |
| 7 | Safer crossings- Widening Edwin St | | | Considered but not progressed |
| 8 | Tunnel under Vic Road Drummoyne | 2 | 6 | Tunnelling is pie in the sky - devastate local area for the sake of the car. Better to focus on improving the behaviours of drivers to respect the residential area Why not a tunnel on Victoria Rd from bridge to bridge - that must be in the RMS plans Will never happen! |
| <i>Public Domain</i> | | | | |
| 9 | Public Space- Closing Church St | 3 | 4 | Closing Church St has the downside of reducing ability to enter and exit Drummoyne I like this idea - however, at present residents access Victoria Rd from west via Church St. If it is blocked this traffic will be forced onto Edwin or Lyons, both of which are already overloaded In only from Victoria Rd else all traffic will go via Edwin St as Day St is one way out to Victoria Rd Closing Church St is a good idea but would direct too much traffic to Day St or Edwin St, which are now gridlocked in peak hours |

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| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|-------------------|--|------------|----------|--|
| 10 | Public Space-Closing Formosa St | 11 | 0 | Closing Formosa St at Lyons Rd seems like a sensible idea |
| | | | | Closing Formosa St at Lyons Rd is a good idea and may help the dangerous fast drivers on Formosa - often going fast the wrong way |
| 11 | Streetscape upgrade- Along Formosa St | 3 | 1 | Turn back on Victoria Rd and shops to front Formosa St between Lyons St and Edwin St. This can become 'centre' pedestrian. Plus make Lyons Rd between Victoria Rd and Westbourne a pedestrian way - divert traffic |
| | | | | For the above comment - good idea |
| 12 | Streetscape upgrade- Along Victoria Rd | 9 | 0 | Victoria Rd footpaths are fairly narrow; who will look after the planters - not the commercial places |
| 13 | Street trees- Along Formosa St, Renwick St and Wright St | 7 | 0 | If street trees are planted, Council needs to water and care for them; this has not happened with the new plantings nearby |
| 14 | Bus Shelter | 2 | 0 | Need a bus shelter at the post office |
| 15 | Awning controls - for the entire study area | 2 | 0 | Good idea to plant trees on Victoria Rd - need allocation of responsibility for watering |
| | | | | A combination of the two awnings could work |
| | | | | Broken awnings with trees if well looked after would have a big impact on improving this as a walkable area |
| <i>Activation</i> | | | | |
| 16 | Showcasing heritage- Along Victoria Rd | 0 | 2 | The old will be highlighted but look wrong with the modern units on top |
| 17 | Showcase lighting-for the entire study area | 0 | 0 | |
| 18 | Night time economy- Along Victoria Rd | 2 | 0 | This comes when community hub/spaces happen |
| 19 | Public Art- On building façades and public spaces | 1 | 5 | Yes to public art, however not a fan of the kookaburra picture ; interactive art murals |
| 20 | POPS- Privately Owned Public Spaces | 6 | 0 | Tree masterplan is better than ad-hoc planters |

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| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|--------------------|--|------------|----------|--|
| 21 | Sutton Place Community Hub | 6 | 0 | YES - Sutton Place should be the focus, especially with a green village square |
| | | | | In the meantime, upgrade the existing Sutton Place including replacing the tree that was cut down a couple of years ago |
| | | | | It should be focussed on the community hub aspect and not a commercial centre/retail shopping: Plaza, library, local markets, activity hall, local focus - connecting activities in the community centre |
| | | | | Sutton location is a good spot for a potential metro station |
| | | | | Re-zone carpark IGA Victoria Rd shops and land to business and community to isolate them. Proposal too bulky, raise height limit to 25m. Plan for a metro station here |
| 22 | Smart Poles - Along Victoria Rd | 0 | 5 | A waste of money - change the poles; this money could be spent on other facilities |
| | | | | Save money on power poles and plant more trees |
| 23 | Signage & Advertising - Along Victoria Rd | 2 | 1 | I like - helps stress that this is a local area. Shows pride that despite the traffic, shops and locals take pride in the area - it needs to be respected |
| <i>Development</i> | | | | |
| 24 | Building heights- Update LEP controls for east side of Victoria Rd | 0 | 13 | Building heights won't solve any issues |
| | | | | This increase in height limit needs to be modelled better - to illustrate the trade off between increased height and reduced bulk |
| | | | | What about the effect of height on streetscape from Renwick St side? This just gives developers the opportunity for an extra level on Renwick |
| | | | | Don't think giving improved frontage to east of Victoria Rd would make the retail area more attractive. Access crossing Victoria Rd plus no parking around are the major issues |
| | | | | If there is an increased height limit, developers will take 13.5m as the minimum - so we now have 15 to 16m! |
| | | | | Notwithstanding height limits, planters etc. should be used to raise sight lines |
| | | | | Developers will always take advantage - they will still work to absolute boundaries with an extra storey |
| | | | | What about pop-up shops in empty premises - need Council underwriting this |

APPENDIX A - COMMUNITY CONSULTATION

| No. | Ideas & Strategies | Green dots | Red dots | Detailed Comments |
|-----|---|------------|----------|---|
| 25 | Building heights- Additional heights along west of Victoria Rd | 0 | 15 | Increase in height, especially up the hill, closes Victoria Rd - makes it a tunnel |
| | | | | An increase in height with setbacks in both directions - potential setback on Formosa? |
| | | | | No increase in heights - Council has DCPs - developers should be held to the existing rules |
| 26 | Privacy screening- Update DCP controls | 7 | 0 | Agree but Council rules need to be followed. For e.g. Victoria Rd developments currently have no setback. They are the same height for whole site |
| | | | | Would be good if DCP controls were actually adhered to |

APPENDIX A - COMMUNITY CONSULTATION

Additional Ideas received at the Drop-in session

| Additional Ideas & Comments |
|--|
| Libraries needed - 4 likes |
| More money for community run facilities like Community Day Care |
| Even as a student there is no place to go to study - no library! - go to Balmain |
| More pedestrian scaled public art |
| Planter boxes, gardens along cafés/shops on Lyons Rd between Marlborough and Victoria Rd |
| More restaurants and vibe similar to Balmain |
| Encourage more shops/cafés/retail rather than industrial tile shop; lots of empty run-down shops |
| Need a music venue - place for music events and festivals |
| Solar powered developments - at least public spaces - community coming together to put solar |
| Existing shopping centre is tired - retain courtyard. Any carpark should be underground. Reserve corridor for metro and station |
| Better plaza feel - Victoria Rd means can't cross and feels like living on a motorway |
| Need pedestrian overpass at Edwin St - safe for children and elderly |
| Two lanes turning right from Lyons Rd to Victoria Rd |
| Make the signage for the turns onto Victoria Rd from the south much further back; so the drivers know which lane to use |
| Closed slip lane from Lyons Rd to Victoria Rd - not good |
| Issue with no left into Thompson St |
| Weekend clearways to reduce traffic gridlock |
| Remove Victoria Rd middle concrete barriers - more crossings, more open, more green |
| The roundabout at Renwick and Roseby needs an upgrade. This is dangerous for pedestrian walking from bus to Birkenhead Point |
| Make developers build extra levels of carparking to reduce congestion on the road |
| Parking along Formosa St carpark |
| Changes to Formosa for parking |
| Provide setbacks along Victoria Rd above 3 storeys |
| Don't increase FSR |
| Don't increase height limits |
| No more high-rises, no more relaxing of the DCP plan, and replace old power poles or go underground |
| It seems you are proposing other ideas (some of which are good) to overshadow the main issue - locals want the high rise developments stopped! |
| Height and increased FSR along west side of Victoria Rd |
| Increase height limits to 8 storeys between Formosa/ Marlborough and Victoria Rd; FSR = thinner buildings |
| The whole tempo building external decor is hideous, including the Kookaburra |
| The kookaburra is an eyesore; horrible green walls |

APPENDIX A - COMMUNITY CONSULTATION

| Additional Ideas & Comments |
|--|
| Make provision for a Metro north station |
| Provision for two metro stations along Victoria Rd - Birkenhead and Sutton Place |
| Why was an apartment block approved in the old Commonwealth site - metres away from busy Victoria Rd and Lyons Rd intersection - that will create further traffic havoc |
| Traffic is a major issue in this area, and adding high rises will only make the matters worse |
| No more cars please - let us encourage transport other than cars |
| Would like to see us value our heritage and aim for destination experience as seen in other parts of Sydney |
| The idea of planting trees along Formosa St is not very relevant - there are already trees along most of Formosa except where the footpaths are narrow or where a large number of garbage cans need to be picked up, e.g.. Harris Farm |
| Pedestrian overpasses - major toll for single cars that use roads going in or near CBD |

Victoria Rd, Drummoyne Urban Design Review

Community Reference Group Meeting No. 3

Date: 10 April 2019

Location: Halliday Room, City of Canada Bay

Time: 6 pm to 8 pm

Attendees

| | |
|---------------------|--------------------------------|
| Angelo Tsirekas | Mayor, City of Canada Bay |
| Michael Megna | Councillor, City of Canada Bay |
| Daniela Ramondino | Councillor, City of Canada Bay |
| Joe Cordaro | Business Representative |
| Margaret Harte | Resident Representative |
| Phyl Blennerhassett | Resident Representative |
| Les Pall | Resident Representative |
| Paul Dewar | City of Canada Bay |
| Diana Griffiths | Studio GL |
| Felicity Lewis | Studio GL |

Apologies:

| | |
|------------------|-------------------------|
| Warwick Williams | Business Representative |
| Anthony Wynen | City of Canada Bay |

Disclosure

| | |
|-------------------|--|
| Daniela Ramondino | Owner of property at 147 Victoria Road, Drummoyne. |
|-------------------|--|

Minutes

Welcome and introduction

Mayor, Angelo Tsirekas welcomed attendees and provided an indication of the purpose of the meeting, which was to update the Group on the work being undertaken by Studio GL, and to present the input received during the recent Round Two of the Community Consultation.

Presentation and Discussion

Presentation by Studio GL of the ideas presented to the Community and the feedback received during the Consultation Round Two.

Each idea was discussed by the CRG, and the outcome of these discussions is indicated below.

Ideas

Carparking – multi deck on Council owned site adjacent to Sutton Place

- To be considered as part of the Parking Study that is currently being prepared
- Could operate in a multitude of ways, i.e. residents receive free 2 hr parking,

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- Possible use by commuters at a day rate
- Might unlock opportunity for upgrade of Sutton Place retail, encourage owners to upgrade
- Option for green space on top of car park, as per Salesforce Transbay Transit Center building in San Francisco
- Risk of overshadowing from adjacent buildings
- CRG generally in favour

Carparking – extension of timed parking limits

- Would require consideration as part of the Parking study
- CRG generally in favour, with parking restrictions in place only until 8pm

Intersection upgrade – timed crossing at Victoria Rd and Lyons Rd

- CRG generally in favour

Cyclist End of Trip facilities

- Consider alternate locations, possibly Ambulance site
- Trade off for installation on a private site would most likely be required
- Should be located close to a bus stop, also close to a crossing point to enable access to bus stops on either side of Victoria Rd
- CRG generally in favour

Upgraded pedestrian links

- Would be a 'quick win'
- CRG generally in favour

Tunnel under Victoria Road

- There was a previous State Government proposal to consider a tunnel, however funding for the study was never forthcoming
- It is acknowledged that a tunnel would be expensive and land acquisition may be needed
- This is an idea that has not progressed previously, but the situation may change and there is support
- Roads will not disappear, beneficial to divert non-local traffic
- CRG generally in favour

Public space – Church St closure

- Concern that it was located away from the centre of Drummoyne
- Option for partial closure, especially that allows for access in from Victoria Rd in the morning, and access out onto Victoria Rd of an afternoon
- Option for slow traffic zone, maybe not complete closure

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- CRG generally in favour

Public space – Formosa St closure

- Some indicated support for one lane, partial closure, with no parking as street is currently three lanes wide
- Others indicated support for full closure
- CRG generally in favour, but this is not about closing roads, especially if there would be detrimental effect on traffic, it is in support of improving amenity in the area
- Option to open up College St raised, but this was closed to prevent rat running, so no support for re-opening

Formosa St – streetscape upgrade

- Concern about prohibitive cost, especially for undergrounding of power lines
- CRG generally in favour

Victoria Rd – streetscape upgrade

- The footpath is already very narrow, would need to ensure enough width remained to allow passing wheelchairs / walking frames, prams etc
- Temporary planters have been tried before, generally not maintained, have been used to dump cigarette butts, rubbish etc
- Idea could be focused on corners and intersections
- Could be a community Outreach option
- Shop owners should be included, encouraged to have pride in their street
- CRG generally in favour, however subject to appropriate maintenance

Street trees

- Canada Bay LGA can do more to meet urban tree canopy targets under the Eastern City District Plan
- Need exists to improve community support for trees, both public and private
- Recognition that street trees could 'block' some of the visual impact of the newer developments
- Issues raised relating to maintenance, who is responsible
- Previous street tree planting has been less than successful
- CRG generally in favour

Bus Shelter – at Post Office bus stop

- Concerns about visibility
- Very narrow footpath
- Risks of this becoming an advertising billboard
- Options to 'hang' structure off Post Office to provide shelter and seating
- CRG generally in favour

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Awning controls

- Balancing act, desire to increase greening where possible
- Newly planted trees in front of Tempo building are dying
- CRG generally in favour

Showcasing heritage

- Concerns expressed about cost of maintenance
- Costs of development are increased by need to retain / incorporate heritage elements
- Discussion about the impact of heritage or conservation area listing on ability for property to be developed
- Generally, desire to maintain as much as possible, but acknowledgement of heritage being a constraint to the redevelopment of some sites.
- No consensus from CRG.

Showcase lighting

- Lighting study may be required
- Current street lighting is to a standard, provision of additional street lighting (double poles) is unlikely
- CRG supportive of under awning lighting, lighting at pedestrian level for safety and amenity

Night-time economy

- Focus may need to be on 'evening-economy', rather than night-time economy
- Opportunity for some spill out and activation of parts of the centre
- CRG generally in favour

Public Art – on building facades

- Given community concern about this idea, will be modified to focus on public art at a pedestrian scale
- Raised issue of owner's consent to installation of any public art
- CRG generally in favour

Privately owned public space

- Would need to be at 'no cost'
- 'there is no such thing as a 'free' lunch'
- Development of former Comm Bank site has a POP to Lyons Rd (similar to existing), this is expected to be a café, so the area will be 'controlled' public access
- CRG generally in favour

Sutton Place Community Hub

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- Amazing if this can be funded
- Multi-deck carpark may unlock value of this prominent site
- This area is of particular value to older community members
- CRG generally in favour

Smart Poles

- Removes power lines
- Improves opportunities for trees
- CRG generally in favour

Signage and Advertising

- This would provide clear direction about what is acceptable
- CRG generally in favour

Building heights – update of LEP controls for east side of Victoria Rd

- Various views expressed in relation to this idea
- Additional height, with the existing 30 degree rear setback provision, would have little impact as height would be to Victoria Rd - it is possible that the community has false perception of what the impacts would be
- Issues exist relating to enforcement of existing controls
- Lift overruns, etc result in additional height
- Height along Victoria Rd operates as a sound barrier
- Developers will continue to want to build 4 storeys
- There are issues of dysfunctional ground floor retail
- Raising ground floor retail to be level at street may not fix the issue of vacancies etc
- Slope of the street is also an issue, especially with long frontages
- Discussion about LEP versus DCP controls.
- No consensus from CRG.

Building heights – additional height along west side of Victoria Rd

- Various views expressed in relation to this idea.
- 20 m height limit is simplistic, maybe need to go to 25 m with requirement for breaks in continuous walls.
- There is an issues with increasing height, when the community perception is that current controls are not being adhered to.
- Impact of additional height is exacerbated due to slope of street.
- Concerns raised about solar access, acknowledgement that there are standards, but overshadowing is an issue that would need to be addressed.
- Residential properties would have been purchased with an expectation / understanding that adjacent commercial properties may develop
- Need to consider what Drummoyne will look like in 30+ years
- Risk of encouraging poor development, developers maximising return, is this what we want?

APPENDIX A - COMMUNITY CONSULTATION

- No consensus from CRG

Privacy screening – update DCP controls

- There may have been confusion about diagram
- CRG generally in favour

General Discussion

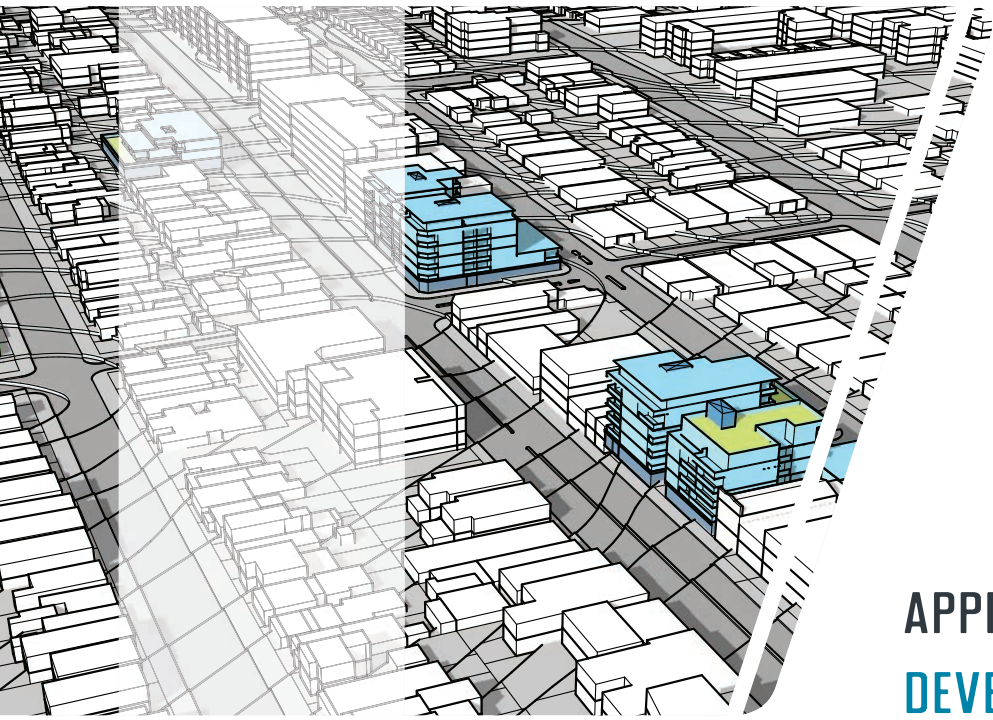
Use of LEP versus DCP control

- LEP controls are more enforceable, DCP controls are for guidance only
- Consideration to be given to what controls should be in LEP, and what can stay in DCP
- Moving some controls, that are considered essential, into LEP will tighten the controls
- Consideration also to be given to more explicit controls, relating to sightlines etc
- Developers have more 'leeway' with DCP controls, especially if they take the project to Court
- Clearer requirements, clearer articulation of desired outcomes may result in better outcomes

Vision

- What will Drummoyne be like into the future?
- Not going to be retail or commercial due to lack of parking.
- Could leave as is, but then nothing will happen, how would a poor urban environment impact the area.
- Sensitive development could be a catalyst for positive change.
- Will revitalisation happen without development?
- Consideration may need to be given to reducing car parking in the right circumstances, particularly where access or traffic issues preclude parking provision for particular sites.
- Regarding vacant retail premises, consideration for 'Renew Newcastle' style program
- Overall, the vision should aim to improve the area, not make it worse.

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APPENDIX B

DEVELOPMENT SCENARIOS

B-1 Potential catalyst sites



Figure 59 Potential Catalyst Sites

As part of the Victoria Road Urban Design Review, six possible catalyst development sites were identified through an analysis of the City of Canada Bay Local Environmental Plan 2013, City of Canada Bay Development Control Plan, Drummoyne Village Control Plan and information provided by council such as development applications, planning proposals, strata developments, lots with heritage item, conservation areas and ownership.

Out of a possible six sites, three, identified as 2, 4 and 5 were selected to test development scenarios. Sites 2 and 4 were selected as they are some of the largest remaining sites on the western side of Victoria Road, and for each council is a significant landowner. Site 5 was selected as it is one of the largest undeveloped consolidated sites under single ownership on the eastern side of Victoria Road.

B-2 Development scenario testing

Two development options were tested for each of the three identified catalyst sites. Firstly, a scenario which complied with the current planning controls was developed to establish a baseline and test the current controls. Secondly, an alternative scheme was produced which considered options beyond these planning controls where additional development capacity would be traded for community benefit.

Site 2 - Initial modelling of a compliant scheme demonstrated that the maximum FSR on the site was hard to achieve with a predominantly residential development. An alternative development scenario was tested that allowed for increased height from 15m to 20m (4 to 6 storeys), and included the creation of a new pedestrian link to Victoria Road.

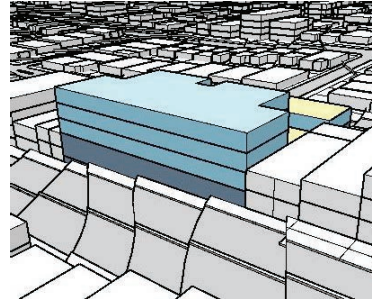
The alternative scenario was selected for further exploration as it could deliver a pedestrian link, and it may also be possible to provide additional public parking spaces.

Site 4 - Initial modelling of a compliant scheme demonstrated that the maximum FSR on the site was hard to achieve with a predominantly residential development. An alternative development scenario allowed for increased height from 15m to 20m (4 to 6 storeys).

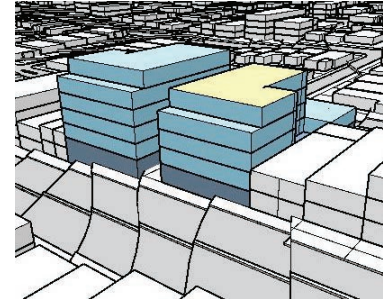
The alternative scenario was selected for further exploration, as the site sits in a low point on Victoria Road reducing the visual impact of additional height. It may also be possible to provide additional public parking spaces.

Site 5 - Initial modelling of a compliant scheme demonstrated that the maximum FSR on the site was hard to achieve with a predominantly residential development. An alternative development scenario allowed for increased height from 12m to 14m (3 to 4 storeys) and an increased ground floor setback from the rear boundary.

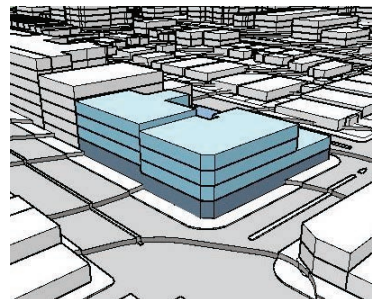
Due to vehicle access constraints from Victoria Road, and the sensitive boundary with the adjacent conservation area the compliant scheme was selected for further investigation.



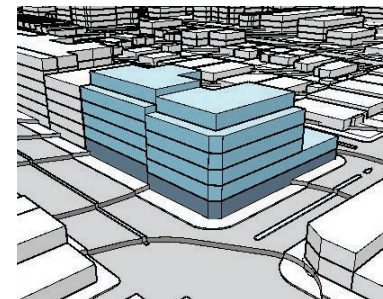
Site 2 - compliant scenario



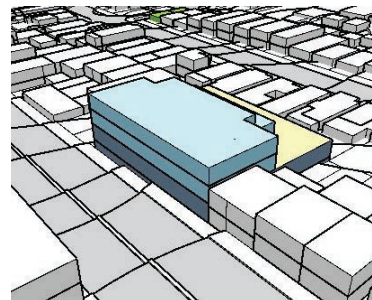
Site 2 - alternative scenario



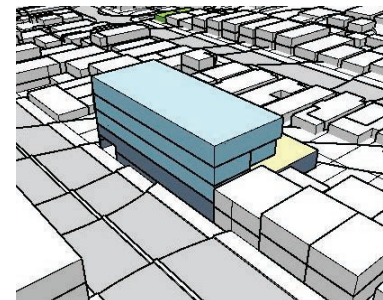
Site 4 - compliant scenario



Site 4 - alternative scenario



Site 5 - compliant scenario



Site 5 - alternative scenario

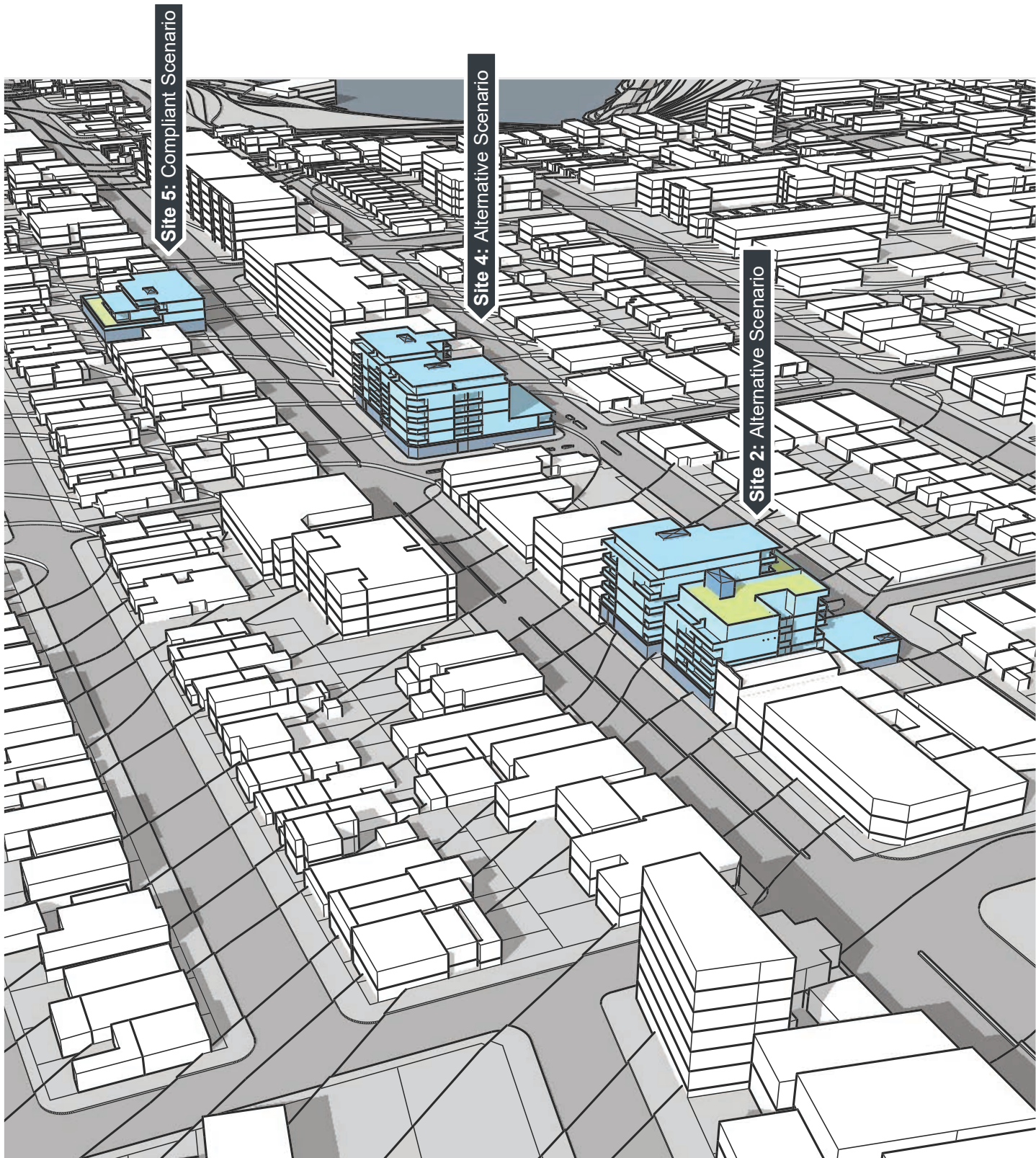


Figure 60 Aerial view of selected development scenarios, looking south.

APPENDIX B - DEVELOPMENT SCENARIOS

Site 2 Alternative Scenario

177-189 Victoria Road, 70 Formosa Street, Drummoyne

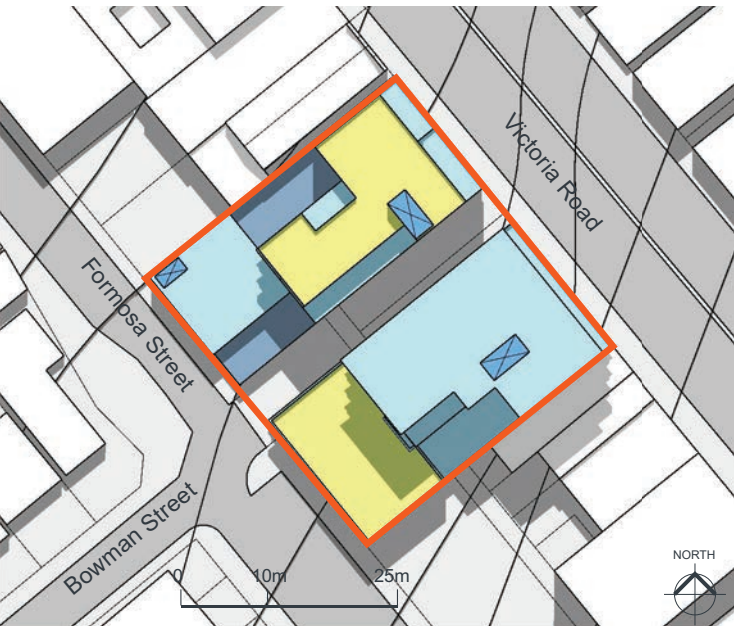


Figure 61 Plan view

Site information

| | |
|---------------|------------------------------|
| Site area | 1925m ² |
| Site frontage | 45m + 45m |
| Access | Dual frontage with rear lane |
| Amalgamation | Yes - 8 lots |

Key planning controls

| | | |
|---------------------|---|---|
| Applicable controls | <input checked="" type="checkbox"/> LEP | <input checked="" type="checkbox"/> DCP |
| | <input checked="" type="checkbox"/> SEPP 65 | |
| Zone | B4 Mixed Use | |
| Density | 2.75:1 FSR | |
| Height limit | 15m (Proposed 20m) | |

Area calculation

| | GBA total | GBA car parking (+loading) | GBA remaining | Residential GFA (75% of GBA) | Commercial GFA (80% of GBA) | Apartment area (90% of GFA) | No. of dwellings | No. of car spaces |
|------------------|--------------|----------------------------|---------------|------------------------------|-----------------------------|-----------------------------|------------------|-------------------|
| Basement Level 3 | 775 | 775 | 0 | | | | | 26 |
| Basement Level 2 | 1645 | 1645 | 0 | | | | | 47 |
| Basement Level 1 | 1885 | 1885 | 0 | | | | | 44 |
| Level 1 (GF) | 1640 | 260 | 1380 | 83 | 1016 | 0 | 0 | |
| Level 2 | 1385 | 0 | 1385 | 1039 | 0 | 935 | 12 | |
| Level 3 | 960 | 0 | 960 | 720 | 0 | 648 | 9 | |
| Level 4 | 960 | 0 | 960 | 720 | 0 | 648 | 9 | |
| Level 5 | 820 | 0 | 820 | 615 | 0 | 554 | 7 | |
| Level 6 | 435 | 0 | 435 | 326 | 0 | 294 | 4 | |
| Total | 10505 | 4565 | 5940 | 3503 | 1016 | 3078 | 41 | 117 |

| | |
|--------------------------------|------|
| FSR (approx) = GFA / site area | 2.3 |
| Site area m ² | 1925 |

Dwelling mix

| | Average dwelling size (m ²) | No. of dwellings. | Dwelling mix | Average GFA per dwelling in m ² |
|-------------------|---|-------------------|--------------|--|
| 1 - bed apartment | 55 | 10 | 25% | 75 |
| 2 - bed apartment | 77 | 25 | 60% | |
| 3 - bed apartment | 99 | 6 | 15% | |

manual input fields (measured and/or counted)

APPENDIX B - DEVELOPMENT SCENARIOS

Development scenario overview

| | | |
|---------------------------|--------------------|----|
| No. of storeys | 6 | |
| No. of basement levels | 2.5 | |
| Building height (approx.) | 20m | |
| No. of dwellings | 41 | |
| Commercial GFA (approx.) | 1015m ² | |
| No. of car spaces (max.) | 117 (basement) | |
| Communal Open Space | 490m ² | |
| Circulation cores | Residential | 2* |
| | Commercial | 2 |
| Lifts | Residential | 2 |
| | Commercial | 2 |
| Maximum FSR achieved | No | |

*Additional single stair to level 1 apartment, accessed from Formosa Street.

Commentary

The alternative development scenario for catalyst site 2 allowed for increased height of up to 20m (6 storeys), additional public parking and the creation of a new pedestrian link from the end of Bowman Street to Victoria Road. The scenario accommodates 45 residential parking spaces as per the max. residential parking rate within the zone and 25 business/office/retail spaces (minimum required). This scenario also proposes 46 public parking spaces to replace those on the existing site lost due to reconfiguration of street parking to enable two way traffic on Formosa Street, between Edwin and Bowman Streets. The feasibility of this scenario is impacted by the number of parking spaces provided for residential, non residential and public use. If the number of parking spaces could be reduced or relocated to a shared facility, this would reduce the size and number of basement levels.

A mixed use development can create 41 one, two and three-bedroom apartments with an approximate average internal apartment area of 75m² on the upper storeys and approximately 1,015m² of commercial space at the ground floor.

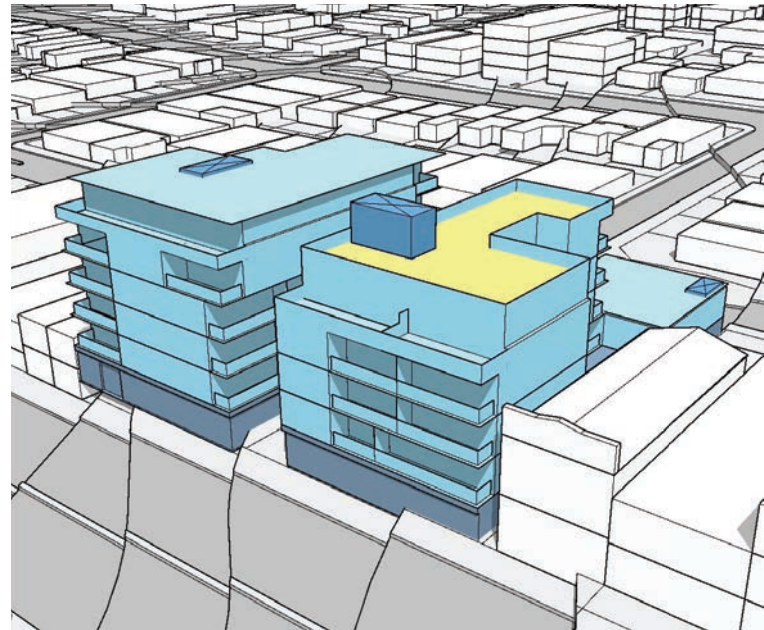


Figure 62 3D model birdseye view

While an estimated FSR of 2.3:1 was achieved, the maximum permissible GFA was not achieved in this scenario as an additional 'bonus' FSR is currently permitted for sites with a site area between 1500-2,000m² increasing the permitted floor space ratio for the site from 2:1 to 2.75:1.

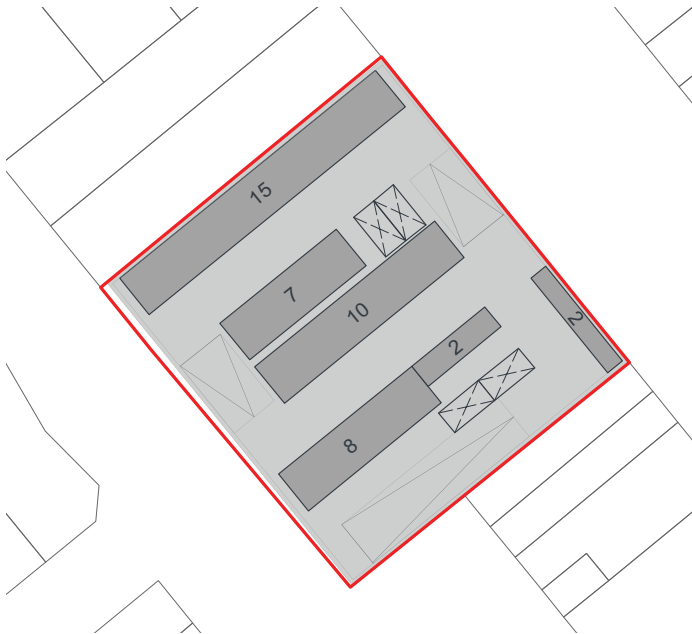
The Gross Floor Area (GFA) is restricted by the depth of residential floor plates on the upper levels, which are not as deep as would be optimal for commercial use. Additionally DCP controls require the rear 1/3rd of the site adjacent to Formosa Street to be a maximum of two storeys. The design also assumes a reduced height for the northern part of the development to better relate to the heritage building at 191 Victoria Road.

Providing a pedestrian through site link also reduces the quantity of commercial floor space which can be provided at ground floor. However, the link increases the length of commercial frontage, and creates a diversity of spaces with frontages to a more pedestrian friendly environment.

Site 2 Alternative Scenario

177-189 Victoria Road, 70 Formosa Street, Drummoyne

Floorplans



Typical basement plan








Ground floor plan



Level 2 plan

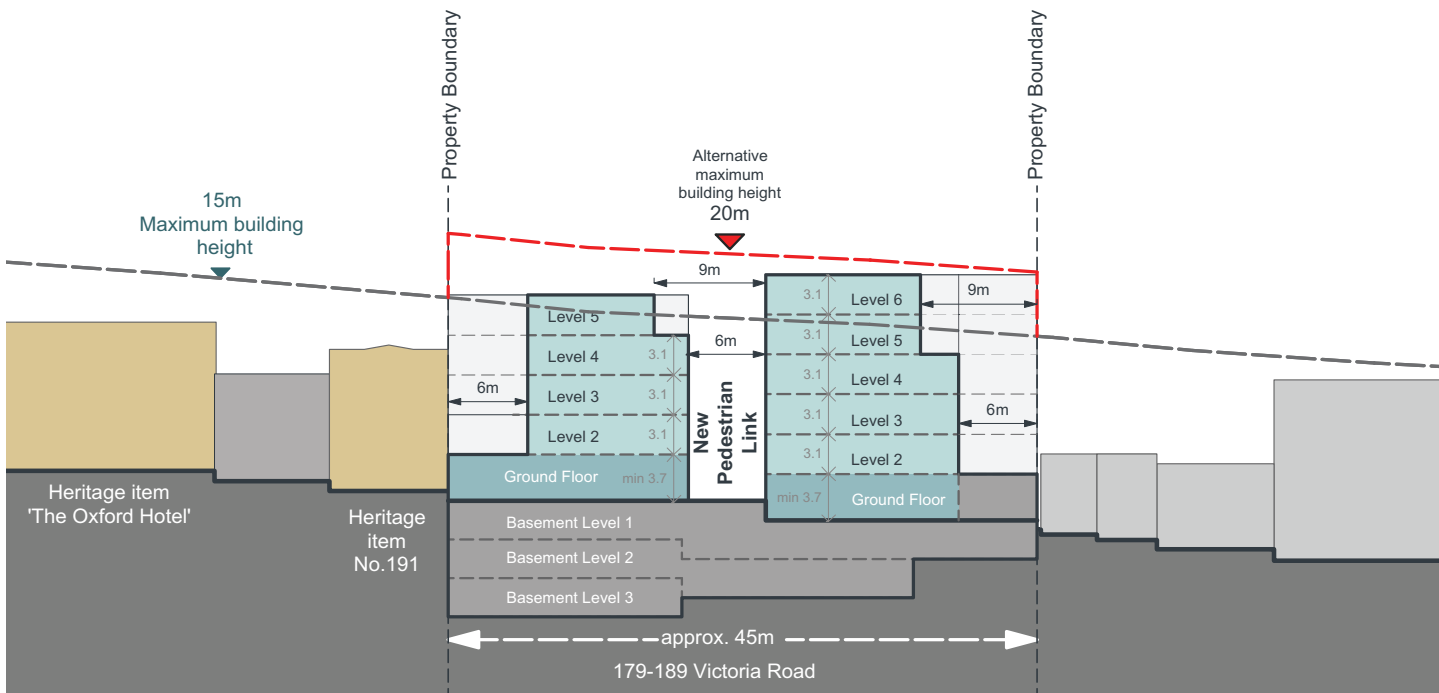


Level 6 (top floor) plan

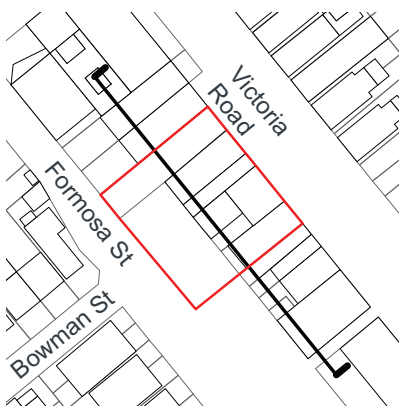
-  Vertical circulation core
-  Balconies
-  Communal open space
-  Pedestrian access
-  Vehicle access



APPENDIX B - DEVELOPMENT SCENARIOS



Cross Section



Key Plan (NTS)

Site 2 Alternative Scenario

177-189 Victoria Road, 70 Formosa Street, Drummoyne

Overshadowing Impact

21 June (Mid Winter)

Solar access is a key consideration when testing future built form and scale, with the aim being to minimise the impact on the adjacent properties. Overshadowing in winter months is greatest due to the low solar angles, while in summer days are longest and the sun reaches its highest altitude.

The adjacent plans show the overshadowing impact in mid-winter (21 June) of the alternative development scenario on the surrounding area, including public domain and private properties.

The plans show very limited overshadowing of houses on the opposite side of Formosa Street at 9am. Commercial buildings to the south receive some overshadowing from the proposal. As the ground slopes down to the south, development of this site casts longer shadows than development of comparable height on the other identified catalyst sites.



APPENDIX B - DEVELOPMENT SCENARIOS



Site 2 Alternative Scenario

177-189 Victoria Road, 70 Formosa Street, Drummoyne

Modeling Assumptions and Principles

| <p>Key LEP controls</p> | <ul style="list-style-type: none"> • B4 Mixed Use Zone (shop top housing & commercial premises permitted) • Maximum height 15m (Increased to 20m in alternative scenario) • Floor space ratio for sites between 1500-2000m²: 2.75:1 | | | | | | | | | | | | |
|--|--|----------------------|--------------|----------------------|-----------------|------------------|-----------------|-----------------------------------|------------------|------------------|-----------------|------------------|------------------|
| <p>Key DCP controls (Mixed Use Areas and Neighbourhood Centres, Victoria Road Drummoyne, Area C)</p> | <ul style="list-style-type: none"> • Maximum building height is four storeys at Victoria Road reducing to two storeys at Formosa Street. (Maximum building height increased to 6 storeys in alternative scenario) • The two storey development should extend for a minimum depth of 1/3 of the site east of Formosa Street. • Maximum parking rates within B4 Mixed Use zone: <table border="1" data-bbox="555 969 1382 1126"> <tr> <td>1 bed apartment</td> <td>0.6 spaces</td> </tr> <tr> <td>2 bed apartment</td> <td>0.9 spaces</td> </tr> <tr> <td>3 bed apartment</td> <td>1.4 spaces</td> </tr> <tr> <td colspan="2">1 visitor space per 5 apartments.</td> </tr> </table> | 1 bed apartment | 0.6 spaces | 2 bed apartment | 0.9 spaces | 3 bed apartment | 1.4 spaces | 1 visitor space per 5 apartments. | | | | | |
| 1 bed apartment | 0.6 spaces | | | | | | | | | | | | |
| 2 bed apartment | 0.9 spaces | | | | | | | | | | | | |
| 3 bed apartment | 1.4 spaces | | | | | | | | | | | | |
| 1 visitor space per 5 apartments. | | | | | | | | | | | | | |
| <p>Key Apartment Design Guide Criteria (SEPP65)</p> | <ul style="list-style-type: none"> • Use a range of maximum apartment depths of 12-18m from glass line to glass line. • Building separation distances between habitable rooms/balconies are 12m up to four storeys and 18m between five and eight storeys. • Communal open space has a minimum area equal to 25% of the site. • Achieving deep soil criteria may not be possible on some sites including in high density areas, centres, and where there is 100% site coverage of non-residential uses at ground floor level. Acceptable stormwater management should be achieved, and alternative forms of planting provided such as on structure. • Habitable rooms have a minimum ceiling height of 2.7m. • The maximum number of apartments off a circulation core is 8. • Minimum area and balcony size per dwelling type: <table border="1" data-bbox="555 1697 1382 1845"> <thead> <tr> <th></th> <th>Minimum area</th> <th>Minimum balcony size</th> </tr> </thead> <tbody> <tr> <td>1 bed apartment</td> <td>50m²</td> <td>8m²</td> </tr> <tr> <td>2 bed apartment</td> <td>70m²</td> <td>10m²</td> </tr> <tr> <td>3 bed apartment</td> <td>90m²</td> <td>12m²</td> </tr> </tbody> </table> | | Minimum area | Minimum balcony size | 1 bed apartment | 50m ² | 8m ² | 2 bed apartment | 70m ² | 10m ² | 3 bed apartment | 90m ² | 12m ² |
| | Minimum area | Minimum balcony size | | | | | | | | | | | |
| 1 bed apartment | 50m ² | 8m ² | | | | | | | | | | | |
| 2 bed apartment | 70m ² | 10m ² | | | | | | | | | | | |
| 3 bed apartment | 90m ² | 12m ² | | | | | | | | | | | |

Modeling Assumptions and Principles cont.

| | |
|--------------------------------|--|
| <p>Design Guidelines</p> | <ul style="list-style-type: none"> • Assumed ground floor retail use, and upper storey residential use. • Residential floor to floor height of 3.1m. • Minimum commercial floor to floor height of 3.7m. • Apartment mix of 1 and 2 and 3 bed apartments. • 45 residential parking spaces provided as per the maximum parking rates for residential flat buildings within the B4 Mixed Use zone. • 25 commercial/retail parking spaces provided as per the minimum parking rates for office and retail premises within the B4 Mixed Use zone. • Existing 18 council car parking spaces re-provided on site. • An additional 26 public car parking spaces provided on site to allow reconfiguration of street parking to enable two way traffic on Formosa Street, between Edwin and Bowman Streets. • New pedestrian public connection between Victoria Road and Formosa Street provided. |
| <p>Measurement Methodology</p> | <ul style="list-style-type: none"> • GFA calculated at 75% of GBA, measured from building model. Internal apartment area calculated at 90% of GFA. |

APPENDIX B - DEVELOPMENT SCENARIOS

Site 4 Alternative Scenario

137-147 Victoria Road, 64-66 Formosa Street, Drummoyne



Figure 63 Plan view

Site information

| | |
|---------------|--------------------|
| Site area | 1885m ² |
| Site frontage | 45m + 41m + 45m |
| Access | Triple frontage |
| Amalgamation | Yes - 5 lots |

Key planning controls

| | | |
|---------------------|---|---|
| Applicable controls | <input checked="" type="checkbox"/> LEP | <input checked="" type="checkbox"/> DCP |
| | <input checked="" type="checkbox"/> SEPP 65 | |
| Zone | B4 Mixed Use | |
| Density | 2.75:1 FSR | |
| Height limit | 15m (proposed 20m) | |

Area calculation

| | GBA total | GBA car parking (+loading) | GBA remaining | Residential GFA (75% of GBA) | Commercial GFA (80% of GBA) | Apartment area (90% of GFA) | No. of dwellings | No. of car spaces |
|------------------|--------------|----------------------------|---------------|------------------------------|-----------------------------|-----------------------------|------------------|-------------------|
| Basement Level 3 | 1595 | 1595 | 0 | | | | | 44 |
| Basement Level 2 | 1595 | 1595 | 0 | | | | | 44 |
| Basement Level 1 | 1870 | 1870 | 0 | | | | | 45 |
| Level 1 (GF) | 1870 | 250 | 1620 | 101 | 1188 | 0 | 0 | |
| Level 2 | 1310 | 0 | 1310 | 983 | 0 | 884 | 12 | |
| Level 3 | 995 | 0 | 995 | 746 | 0 | 672 | 9 | |
| Level 4 | 995 | 0 | 995 | 746 | 0 | 672 | 9 | |
| Level 5 | 995 | 0 | 995 | 746 | 0 | 672 | 9 | |
| Level 6 | 785 | 0 | 785 | 589 | 0 | 530 | 7 | |
| Total | 12010 | 5310 | 6700 | 3911 | 1188 | 3429 | 46 | 133 |

| | |
|--------------------------------|------|
| FSR (approx) = GFA / site area | 2.71 |
| Site area m ² | 1885 |

Dwelling mix

| | Average dwelling size (m ²) | No. of dwellings. | Dwelling mix | Average GFA per dwelling in m ² |
|-------------------|---|-------------------|--------------|--|
| 1 - bed apartment | 55 | 11 | 25% | 75 |
| 2 - bed apartment | 77 | 28 | 60% | |
| 3 - bed apartment | 99 | 7 | 15% | |

manual input fields (measured and/or counted)

APPENDIX B - DEVELOPMENT SCENARIOS

Development option overview

| | | |
|---------------------------|--------------------|---|
| No. of storeys | 6 | |
| No. of basement levels | 3 | |
| Building height (approx.) | 20m | |
| No. of dwellings | 46 | |
| Commercial GFA (approx.) | 1185m ² | |
| No. of car spaces (max.) | 133 (basement) | |
| Communal Open Space | 475m ² | |
| Circulation cores | Residential | 2 |
| | Commercial | 1 |
| Lifts | Residential | 2 |
| | Commercial | 1 |
| Maximum FSR achieved | No | |

Commentary

The alternative development scenario for catalyst site 4 allows for increased height of up to 20m (6 storeys). The scenario accommodates 50 residential parking spaces as per the max. residential parking rate within the zone and 30 business/office/retail spaces (minimum required). This scenario also proposes 50 public parking spaces to replace and augment the spaces currently provided by Council. The feasibility of this scenario is impacted by the number of parking spaces provided for residential, non residential and public use. If the number of parking spaces could be reduced or relocated to a shared public carpark facility, the size and number of basement levels could be reduced.

This mixed use development can provide 46 one, two and three-bedroom apartments, with an approximate average internal apartment area of 75m² on the upper storeys and approximately 1,185m² of commercial space at the ground floor.

The maximum permissible GFA was not achieved in this scenario, an additional 'bonus' FSR is currently permitted for sites with an area between

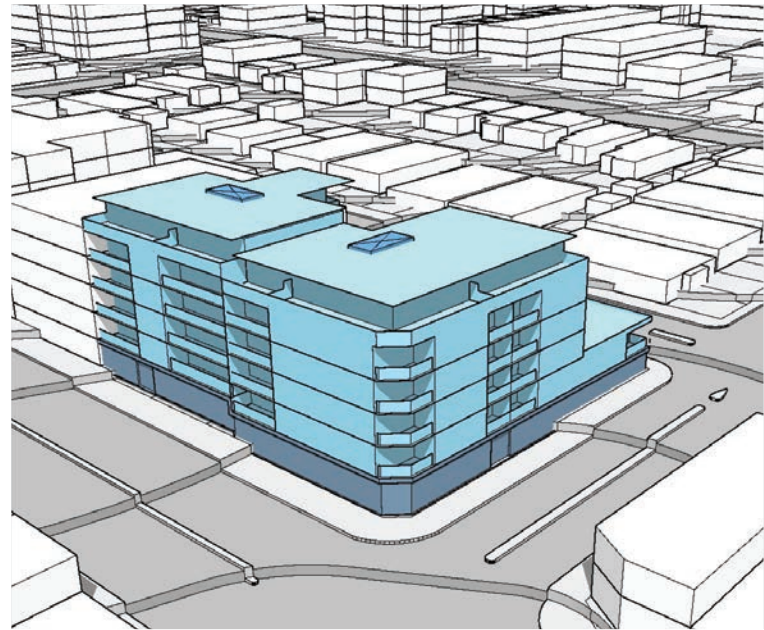


Figure 64 3D model birdseye view

1500-2,000m², increasing the permitted floor space ratio for the site from 2:1 to 2.75:1.

Provision of the GFA is restricted by the depth of residential floor plates on the upper levels, which are not as deep as those suitable for commercial uses. Additionally DCP controls require the rear 1/3rd of the site adjacent to Formosa Street to be a maximum of two storeys.

If additional height were permitted adjacent to Formosa Street the maximum permissible GFA for the site may be achievable.

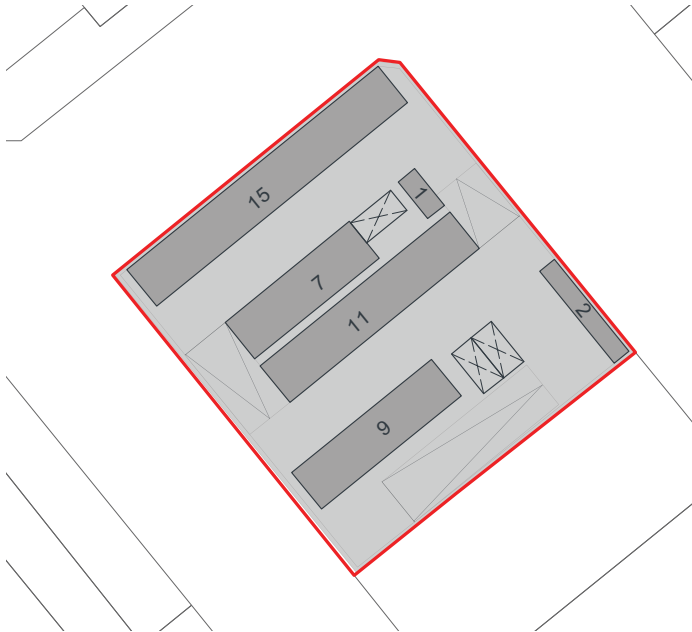
In order to maximise the GFA, whilst still providing residential uses on the upper storeys, a deep commercial floor plate covering almost 100% of the site would be required on the ground floor.

A key challenge for larger sloping sites along Victoria Road is stepping the floor plates at regular intervals to ensure the ground floor is close to street level and can support high quality active frontages to ground floor retail/commercial uses.

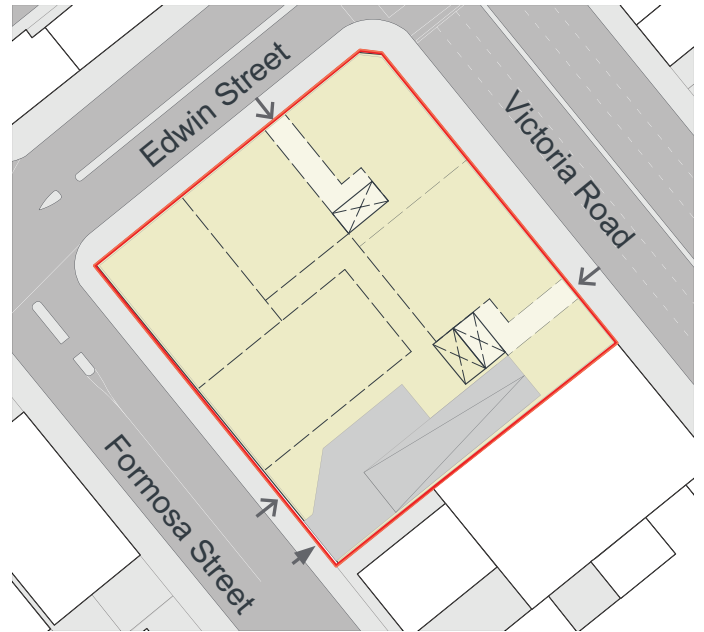
Site 4 Alternative Scenario

137-147 Victoria Road, 64-66 Formosa Street, Drummoyne

Floorplans



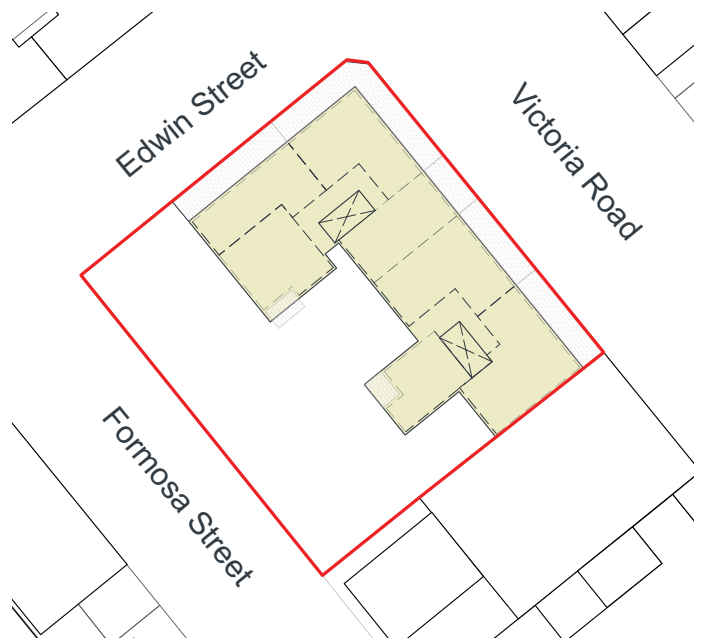
Typical basement plan








Ground floor plan



Level 2 plan

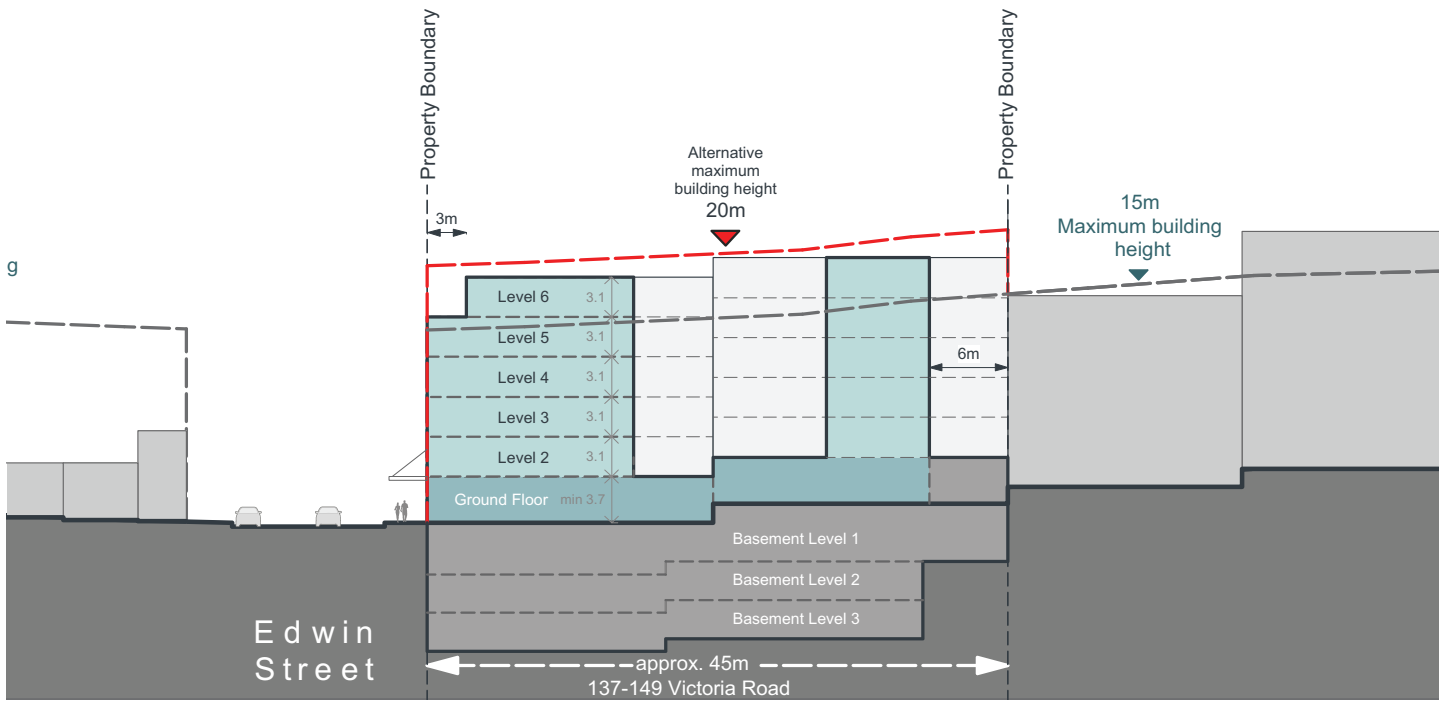


Level 6 (top floor) plan

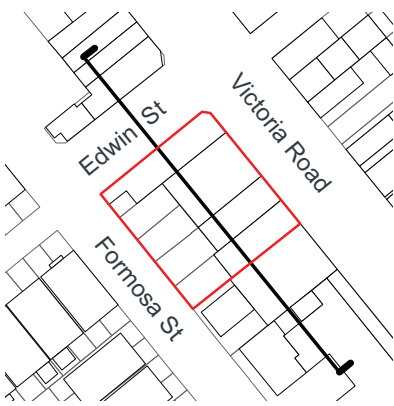
-  Vertical circulation core
-  Balconies
-  Communal open space
-  Pedestrian access
-  Vehicle access



APPENDIX B - DEVELOPMENT SCENARIOS



Cross Section



Key Plan (NTS)

Site 4 Alternative Scenario

137-147 Victoria Road, 64-66 Formosa Street, Drummoyne

Overshadowing Impact

21 June (Mid Winter)

The adjacent plans show the overshadowing impact in mid-winter (21 June) of the alternative development scenario on the surrounding area, including public domain and private properties.

The plans show that overshadowing of nearby properties is limited. Houses on the opposite side of Formosa Street are briefly overshadowed at 9am. The building to the south also receives some overshadowing from the proposal, however this is largely on the blank flank wall on the site boundary.



APPENDIX B - DEVELOPMENT SCENARIOS



Site 4 Alternative Scenario

137-147 Victoria Road, 64-66 Formosa Street, Drummoyne

Modeling Assumptions and Principles

| <p>Key LEP controls</p> | <ul style="list-style-type: none"> • B4 Mixed Use Zone (shop top housing & commercial premises permitted) • Maximum height 15m (Increased to 20m in alternative scenario) • Floor space ratio for sites between 1500-2000m²: 2.75:1 | | | | | | | | | | | | |
|--|--|----------------------|--------------|----------------------|-----------------|------------------|-----------------|-----------------------------------|------------------|------------------|-----------------|------------------|------------------|
| <p>Key DCP controls (Mixed Use Areas and Neighbourhood Centres, Victoria Road Drummoyne, Area C)</p> | <ul style="list-style-type: none"> • Maximum building height is 4 storeys at Victoria Road reducing to two storeys at Formosa Street. (Maximum building height increased to 6 storeys in alternative scenario) • The two storey development should extend for a minimum depth of 1/3 of the site east of Formosa Street. • Maximum parking rates within B4 Mixed Use zone: <table border="1" data-bbox="552 920 1378 1077"> <tr> <td>1 bed apartment</td> <td>0.6 spaces</td> </tr> <tr> <td>2 bed apartment</td> <td>0.9 spaces</td> </tr> <tr> <td>3 bed apartment</td> <td>1.4 spaces</td> </tr> <tr> <td colspan="2">1 visitor space per 5 apartments.</td> </tr> </table> | 1 bed apartment | 0.6 spaces | 2 bed apartment | 0.9 spaces | 3 bed apartment | 1.4 spaces | 1 visitor space per 5 apartments. | | | | | |
| 1 bed apartment | 0.6 spaces | | | | | | | | | | | | |
| 2 bed apartment | 0.9 spaces | | | | | | | | | | | | |
| 3 bed apartment | 1.4 spaces | | | | | | | | | | | | |
| 1 visitor space per 5 apartments. | | | | | | | | | | | | | |
| <p>Key Apartment Design Guide Criteria (SEPP65)</p> | <ul style="list-style-type: none"> • Use a range of maximum apartment depths of 12-18m from glass line to glass line. • Building separation distances between habitable rooms/balconies are 12m up to four storeys and 18m between five and eight storeys. • Communal open space has a minimum area equal to 25% of the site. • Achieving deep soil criteria may not be possible on some sites including in high density areas, centres, and where there is 100% site coverage of non-residential uses at ground floor level. Acceptable stormwater management should be achieved, and alternative forms of planting provided such as on structure. • Habitable rooms have a minimum ceiling height of 2.7m. • The maximum number of apartments off a circulation core is 8. • Minimum area and balcony size per dwelling type: <table border="1" data-bbox="552 1644 1378 1794"> <thead> <tr> <th></th> <th>Minimum area</th> <th>Minimum balcony size</th> </tr> </thead> <tbody> <tr> <td>1 bed apartment</td> <td>50m²</td> <td>8m²</td> </tr> <tr> <td>2 bed apartment</td> <td>70m²</td> <td>10m²</td> </tr> <tr> <td>3 bed apartment</td> <td>90m²</td> <td>12m²</td> </tr> </tbody> </table> | | Minimum area | Minimum balcony size | 1 bed apartment | 50m ² | 8m ² | 2 bed apartment | 70m ² | 10m ² | 3 bed apartment | 90m ² | 12m ² |
| | Minimum area | Minimum balcony size | | | | | | | | | | | |
| 1 bed apartment | 50m ² | 8m ² | | | | | | | | | | | |
| 2 bed apartment | 70m ² | 10m ² | | | | | | | | | | | |
| 3 bed apartment | 90m ² | 12m ² | | | | | | | | | | | |

Modeling Assumptions and Principles cont.

| | |
|--------------------------------|---|
| <p>Design Guidelines</p> | <ul style="list-style-type: none"> • Assumed ground floor retail use, and upper storey residential use. • Residential floor to floor height of 3.1m. • Minimum commercial floor to floor height of 3.7m. • Apartment mix of 1 and 2 and 3 bed apartments. • 50 residential parking spaces provided as per the maximum parking rates for residential flat buildings within the B4 Mixed Use zone. • 20 commercial/retail parking spaces provided as per the minimum parking rates for office and retail premises within the B4 Mixed Use zone. • Existing 20 council car parking space re-provided on site. • An additional 30 public car parking spaces provided on site as part of a development contribution. |
| <p>Measurement Methodology</p> | <ul style="list-style-type: none"> • GFA calculated at 75% of GBA, measured from building model. Internal apartment area calculated at 90% of GFA. |

APPENDIX B - DEVELOPMENT SCENARIOS

Site 5 Compliant Scenario

120 Victoria Road, Drummoyne

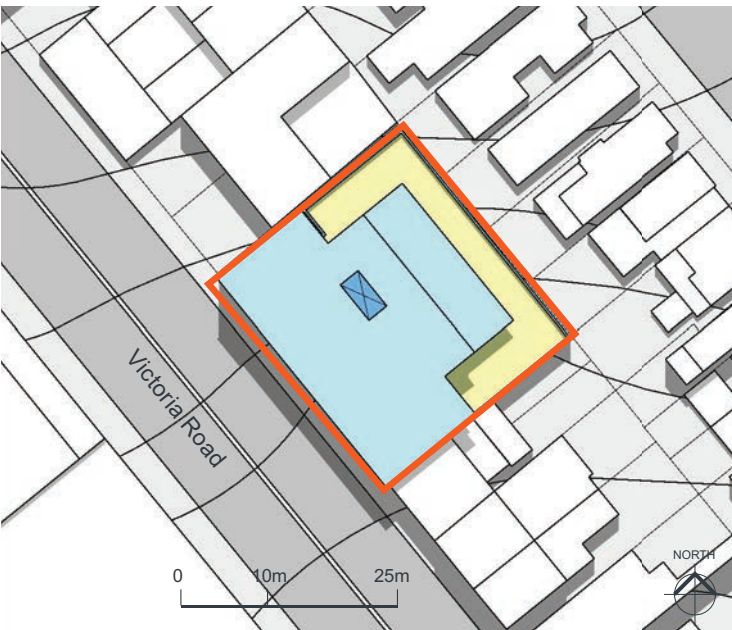


Figure 65 Plan view

Site information

| | |
|---------------|--------------------|
| Site area | 1105m ² |
| Site frontage | 35m |
| Access | Midblock site |
| Amalgamation | No |

Key planning controls

| | | |
|---------------------|---|---|
| Applicable controls | <input checked="" type="checkbox"/> LEP | <input checked="" type="checkbox"/> DCP |
| | <input checked="" type="checkbox"/> SEPP 65 | |
| Zone | B4 Mixed Use | |
| Density | 2.1:1 FSR | |
| Height limit | 12m | |

Area calculation

| | GBA total | GBA car parking (+loading) | GBA remaining | Residential GFA (75% of GBA) | Commercial GFA (80% of GBA) | Apartment area (90% of GFA) | No. of dwellings | No. of car spaces |
|------------------|-------------|----------------------------|---------------|------------------------------|-----------------------------|-----------------------------|------------------|-------------------|
| Basement Level 2 | 585 | 585 | 0 | | | | | 8 |
| Basement Level 1 | 1105 | 1105 | 0 | | | | | 25 |
| Level 1 (GF) | 1105 | 115 | 990 | 45 | 744 | 0 | 0 | |
| Level 2 | 740 | 0 | 740 | 555 | 0 | 500 | 7 | |
| Level 3 | 595 | 0 | 595 | 446 | 0 | 402 | 5 | |
| Total | 4130 | 1805 | 2325 | 1046 | 744 | 901 | 12 | 33 |

| | |
|--------------------------------|------|
| FSR (approx) = GFA / site area | 1.6 |
| Site area m ² | 1105 |

Dwelling mix

| | Average dwelling size (m ²) | No. of dwellings. | Dwelling mix | Average GFA per dwelling in m ² |
|-------------------|---|-------------------|--------------|--|
| 1 - bed apartment | 55 | 3 | 25% | 75 |
| 2 - bed apartment | 77 | 7 | 60% | |
| 3 - bed apartment | 99 | 2 | 15% | |

manual input fields (measured and/or counted)

APPENDIX B - DEVELOPMENT SCENARIOS

Development option overview

| | | |
|---------------------------|-------------------|---|
| No. of storeys | 3 | |
| No. of basement levels | 1.5 | |
| Building height (approx.) | 11m | |
| No. of dwellings | 12 | |
| Commercial GFA (approx.) | 745m ² | |
| No. of car spaces (max.) | 33 (basement) | |
| Communal Open Space | 280m ² | |
| Circulation cores | Residential | 1 |
| | Commercial | 1 |
| Lifts | Residential | 1 |
| | Commercial | 1 |
| Maximum FSR achieved | No | |

Commentary

A mixed use development, under current development controls, can provide 13 one, two and three-bedroom apartments, with an approximate average internal apartment area of 75m² on the upper storeys and approximately 745m² of commercial space at the ground floor. This scenario provides 13 residential parking spaces and 19 commercial parking spaces. If the number of parking spaces could be reduced or relocated to a shared facility, this would reduce the size and number of basement levels. For example if half the residential and non residential parking spaces were provided the basement could be half the size.

The maximum permissible GFA is not achieved on this site. GFA is restricted by the depth of residential floor plates on the upper levels, which will not be as deep as those suitable for commercial use. Additionally DCP controls require the upper floors to step back from the conservation area adjacent to the rear boundary. Upper storey massing is required to sit within a 30 degree plane extended from the top of the ground floor at the rear boundary.

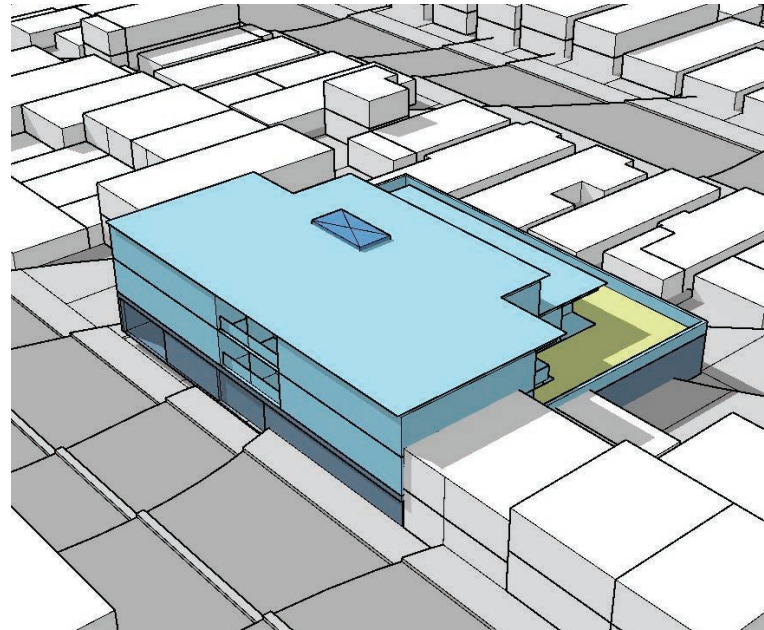


Figure 66 3D model birdseye view

To maximise the GFA, whilst providing residential uses on the upper storeys, a deep commercial floor plate covering almost 100% of the site, is required on the ground floor.

A challenge for sites on the eastern side of Victoria Road is achieving adequate solar access to apartments. The south-west orientation of the street facade and rear setback requirements result in the majority of units facing Victoria Road, with limited solar access in mid-winter.

Due to the height difference between Victoria Road and Renwick Street, single storey development at the rear of the site can appear closer to two storeys high when viewed from the rear gardens of houses on Renwick Street.

Parking would also be an issue, with the RMS unlikely to accept an increase in car access to this site.

Site 5 Compliant Scenario

120 Victoria Road, Drummoyne

Floorplans



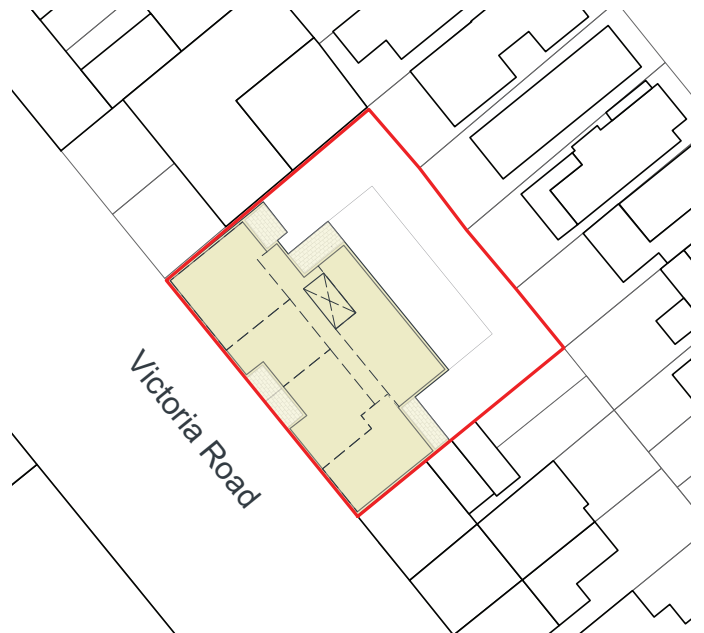
Typical basement plan








Ground floor plan



Level 2 plan

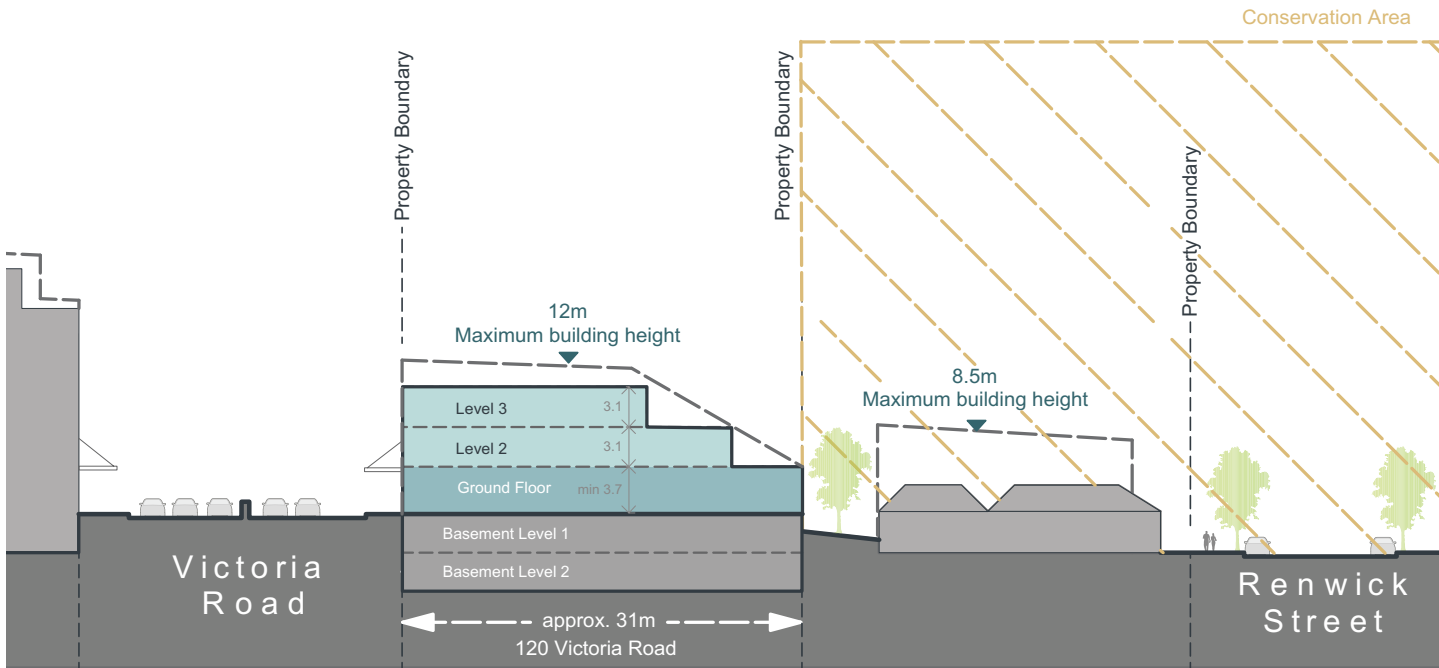


Level 3 (top floor) plan

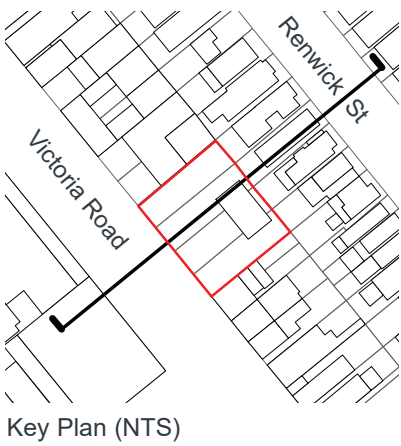
-  Vertical circulation core
-  Balconies
-  Communal open space
-  Pedestrian access
-  Vehicle access



APPENDIX B - DEVELOPMENT SCENARIOS



Cross Section



Site 5 Compliant Scenario

120 Victoria Road, Drummoyne

Overshadowing Impact

21 June (Mid Winter)

The adjacent plans show the overshadowing impact in mid-winter (21 June) of the compliant development scenario on the surrounding area, including public domain and private properties.

The plans show that over-shadowing from the development largely affects Victoria Road, with some overshadowing of properties immediately to the south.



APPENDIX B - DEVELOPMENT SCENARIOS



Site 5 Compliant Scenario

120 Victoria Road, Drummoyne

Modeling Assumptions and Principles

| <p>Key LEP controls</p> | <ul style="list-style-type: none"> • B4 Mixed Use Zone (shop top housing & commercial premises permitted) • Maximum height: 12m • Floor space ratio: 2:1 | | | | | | | | | | | | |
|--|--|----------------------|--------------|----------------------|-----------------|------------------|-----------------|-----------------------------------|------------------|------------------|-----------------|------------------|------------------|
| <p>Key DCP controls (Mixed Use Areas and Neighbourhood Centres, Victoria Road Drummoyne, Area C)</p> | <ul style="list-style-type: none"> • Maximum building height of three storeys to Victoria Road tapering to one storey at the rear boundary. • Maximum parking rates within B4 Mixed Use zone: <table border="1" data-bbox="552 757 1377 913"> <tr> <td>1 bed apartment</td> <td>0.6 spaces</td> </tr> <tr> <td>2 bed apartment</td> <td>0.9 spaces</td> </tr> <tr> <td>3 bed apartment</td> <td>1.4 spaces</td> </tr> <tr> <td colspan="2">1 visitor space per 5 apartments.</td> </tr> </table> | 1 bed apartment | 0.6 spaces | 2 bed apartment | 0.9 spaces | 3 bed apartment | 1.4 spaces | 1 visitor space per 5 apartments. | | | | | |
| 1 bed apartment | 0.6 spaces | | | | | | | | | | | | |
| 2 bed apartment | 0.9 spaces | | | | | | | | | | | | |
| 3 bed apartment | 1.4 spaces | | | | | | | | | | | | |
| 1 visitor space per 5 apartments. | | | | | | | | | | | | | |
| <p>Key Apartment Design Guide Criteria (SEPP65)</p> | <ul style="list-style-type: none"> • Use a range of maximum apartment depths of 12-18m from glass line to glass line. • Building separation distances between habitable rooms/balconies are 12m up to four storeys and 18m between five and eight storeys. • Communal open space has a minimum area equal to 25% of the site. • Achieving deep soil criteria may not be possible on some sites including in high density areas, centres, and where there is 100% site coverage of non-residential uses at ground floor level. Acceptable stormwater management should be achieved, and alternative forms of planting provided such as on structure. • Habitable rooms have a minimum ceiling height of 2.7m. • The maximum number of apartments off a circulation core is 8. • Minimum area and balcony size per dwelling type: <table border="1" data-bbox="552 1496 1377 1653"> <thead> <tr> <th></th> <th>Minimum area</th> <th>Minimum balcony size</th> </tr> </thead> <tbody> <tr> <td>1 bed apartment</td> <td>50m²</td> <td>8m²</td> </tr> <tr> <td>2 bed apartment</td> <td>70m²</td> <td>10m²</td> </tr> <tr> <td>3 bed apartment</td> <td>90m²</td> <td>12m²</td> </tr> </tbody> </table> | | Minimum area | Minimum balcony size | 1 bed apartment | 50m ² | 8m ² | 2 bed apartment | 70m ² | 10m ² | 3 bed apartment | 90m ² | 12m ² |
| | Minimum area | Minimum balcony size | | | | | | | | | | | |
| 1 bed apartment | 50m ² | 8m ² | | | | | | | | | | | |
| 2 bed apartment | 70m ² | 10m ² | | | | | | | | | | | |
| 3 bed apartment | 90m ² | 12m ² | | | | | | | | | | | |

APPENDIX B - DEVELOPMENT SCENARIOS

Modeling Assumptions and Principles (contn.)

| | |
|-------------------------|--|
| Design Guidelines | <ul style="list-style-type: none">• Assumed ground floor retail use, and upper storey residential use.• Residential floor to floor height of 3.1m.• Minimum commercial floor to floor height of 3.7m.• Apartment mix of 1 and 2 and 3 bed apartments.• 13 residential parking spaces provided as per the maximum parking rates for residential flat buildings within the B4 Mixed Use zone.• 19 commercial/retail parking spaces provided as per the minimum parking rates for office and retail premises within the B4 Mixed Use zone. |
| Measurement Methodology | <ul style="list-style-type: none">• GFA calculated at 75% of GBA, measured from building model. Internal apartment area calculated at 90% of GFA. |



APPENDIX C FEASIBILITY ANALYSIS

APPENDIX C - FEASIBILITY ANALYSIS

C-1 Summary

Feasibility analysis was undertaken by AEC Group (property economics) for the selected development option for the three identified catalyst sites. A summary of the findings are outlined below.

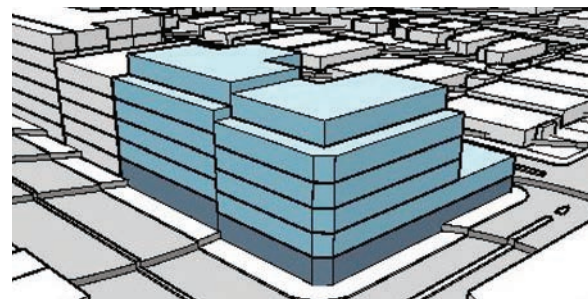
Site 2 - This site is one of the most valuable locations along Victoria Road with some high profile and valuable retailers including the Bowan Island Bakery and the deli café; their attractiveness is enhanced by direct rear access from the Council car park. For this site to meet current commercial feasibilities an FSR of at least 3.5:1 would be required. However, the 6 storey concept design achieved an FSR of 2.3:1. As a general rule a predominantly residential development could require building heights up to 9-10 storeys to achieve an FSR of 3.5:1. With densities lower than 3.5:1, AEC believes incremental development (additional storey/s, part demolition and development) will be more likely to occur than comprehensive redevelopment. Incremental development does allow for an eclectic retention of the character of the area while renewing parts of buildings that are dilapidated.

Site 4 - This site presents a slightly more positive outcome than Site 2. However, the 6 storey concept design achieved an FSR of 2.71:1 which is still insufficient for comprehensive development to be feasible. An FSR of 3:1 to 3.5:1 is likely to be needed. As a general rule a predominantly residential development could require building heights up to 9-10 storeys to achieve an FSR of between 3 to 3.5:1. As this is a corner site, an FSR of 3:1 might be achievable within 8 storeys.

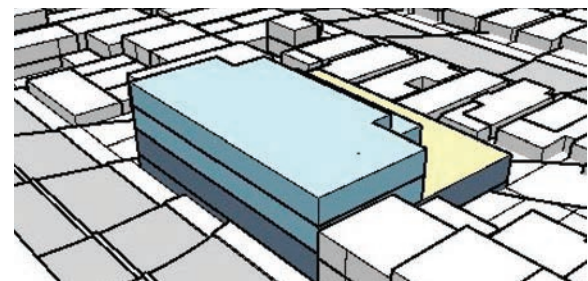
The existing uses and buildings within this site are older, and hence the land purchase component is likely to be not as expensive as Site 2 to consolidate (although 137 Victoria Road is fairly new). If only part of Site 3 (the buildings closer to the corner which are older) was consolidated, the likelihood of comprehensive redevelopment up to an FSR of 2.75:1 would be more likely.



Site 2 - Alternative scenario



Site 4 - Alternative scenario



Site 5 - Compliant scenario

Site 5 - This site is marginal to feasible at an FSR of 2:1, largely because of the limited improvements and buildings on the site. The concept scheme only achieves an FSR of 1.6:1. If the site can accommodate an FSR of 2:1, it could be a potential candidate for redevelopment. This concept scheme assumed 45% of the total floor area was commercial uses at the ground floor. If the site were to accommodate a predominantly residential development it could require building heights up to 5-6 storeys to achieve an FSR of 2:1.

Broad observations

Sites on the western side of Victoria Road (north of Church Street) are highly sought after due to their proximity to Harris Farm, IGA and council car parks. Unless individual properties can be purchased at less than \$6,000/sqm of site area, higher FSRs than are currently available would be needed for redevelopment to be feasible. Sites that are vacant or where buildings are small/dilapidated will likely meet this purchase threshold.

Therefore, for sites on the western side of Victoria Road (north of Church Street), unless there was potential to raise FSRs well beyond the current maximum, the most valuable opportunity to Council would be to facilitate incremental development that might make use of latent development capacity. Sites on the east of Victoria Road are less desirable and sought after compared to their west side counterparts. However, attractiveness also seems to drop off to the south of Church Street.

A few dilapidated strip retail properties on the eastern side of Victoria Road (south of Church Street) have been consolidated in recent years and DAs progressed to an FSR of 2:1. Because these properties are less desirable and sought after, the developers have been able to purchase them at prices that make comprehensive redevelopment feasible.

Incremental development

Incremental development can occur over time through alterations and additions. This can result in a retention of some of the existing built form and an outcome that is an eclectic and interesting mix of style and character.

In valuable areas with fine grain lot patterns (e.g. Surry Hills, Darlinghurst, Camden, Paddington), many landowners consider additional storeys/ part demolition and alterations on single or small amalgamated sites, are a more financially feasible project than if they were to pursue complete demolition and development.

Studio GL Observations

The ability of a site to achieve the maximum FSR is predominantly linked to height and land use, although the size, location and shape of the site also plays a role. As a general rule in mixed use centres where the predominant land use is apartments, which meet the objectives of SEPP 65 and the Apartment Design Guide, a height three times the FSR is needed. This means that for this type of development FSR's of 3 and 3.5:1 could require building heights up to 9-10 storeys.

Given the limited depth of the Drummoyne local centre and the close proximity of low rise conservation areas on either side, this approach is not recommended from an urban design perspective. The alternative of small scale incremental development, encouraging commercial uses and reducing single use parking requirements and providing consolidated shared parking facilities would be a more sensitive approach to encouraging redevelopment in this location.



An example of an incremental development with additional storeys over a heritage building at 543 Crown St, Surry Hills



An example of a new 2+1 storey incremental infill

27 February 2019

Felicity Lewis
Studio GL

Dear Felicity,

RE: VICTORIA ROAD URBAN DESIGN REVIEW - FEASIBILITY ANALYSIS

AEC Group (AEC) is engaged as part of a multi-disciplinary team led by Studio GL to carry out an urban design review of Victoria Road in Drummoyne for Canada Bay Council (Council). AEC's role is to contribute to an understanding of market and development trends and feasibility thresholds on select sites in the Study Area.

Tasks undertaken include:

- Property market appraisal.
- Feasibility testing of concept schemes on select sites.
- Attendance and contribution at workshops.

This briefing note summarises our analysis carried out over the course of October 2018 to February 2019.

MARKET AND DEVELOPMENT TRENDS

Notwithstanding, the challenges of accessibility Market activity for retail and residential floorspace along Victoria Road in Drummoyne is observed to be healthy, particularly north of Church Street.

Since development of the street block into a mixed use development incorporating Harris Farm on the ground floor, sites on the western side of Victoria Road (north of Church Street) are comparatively more sought after. Unsurprisingly, properties that are proximate to Harris Farm, IGA and council car parks are those that experience good occupancy levels and achieve the highest rents and prices.

While properties west of Victoria Road have parking and clearway restrictions, the availability of parking in Council's car parks along Formosa Street is a major offset to what could otherwise be an insurmountable access issue. On the eastern side of Victoria Road, the ability to park on the street mitigates its lack of access to the car parks along Formosa Street. On both sides of Victoria Road, market demand appears to drop off to the south of Church Street.

The comparative desirability of properties on the western side of Victoria Road is a double-edged sword. Valuable existing properties cost more to consolidate into development blocks and consequently require higher FSR thresholds for feasible redevelopment.

In contrast, properties south of Church Street benefit from lower pedestrian footfall and typically cost less to consolidate as development sites. Several sites here have been assembled for redevelopment (FSR 2:1), generally of dilapidated commercial buildings.

Commercial Suites

Like many suburban local and neighbourhood centres, local service commercial floorspace that is contemporary and well located is sought after. Commercial suites that are vacant for extended periods of time are generally not due to a lack of demand, rather a result of their being unsuitable to meet market requirements.

Victoria Road is no different. Older style commercial tenancies experience low levels of market interest and generally experience prolonged periods of vacancy. In contrast, there is a healthy level of market demand for commercial suites that are contemporary and modern. Testament to the market interest in contemporary suites is the market response and take-up of space in 135 Victoria Road.

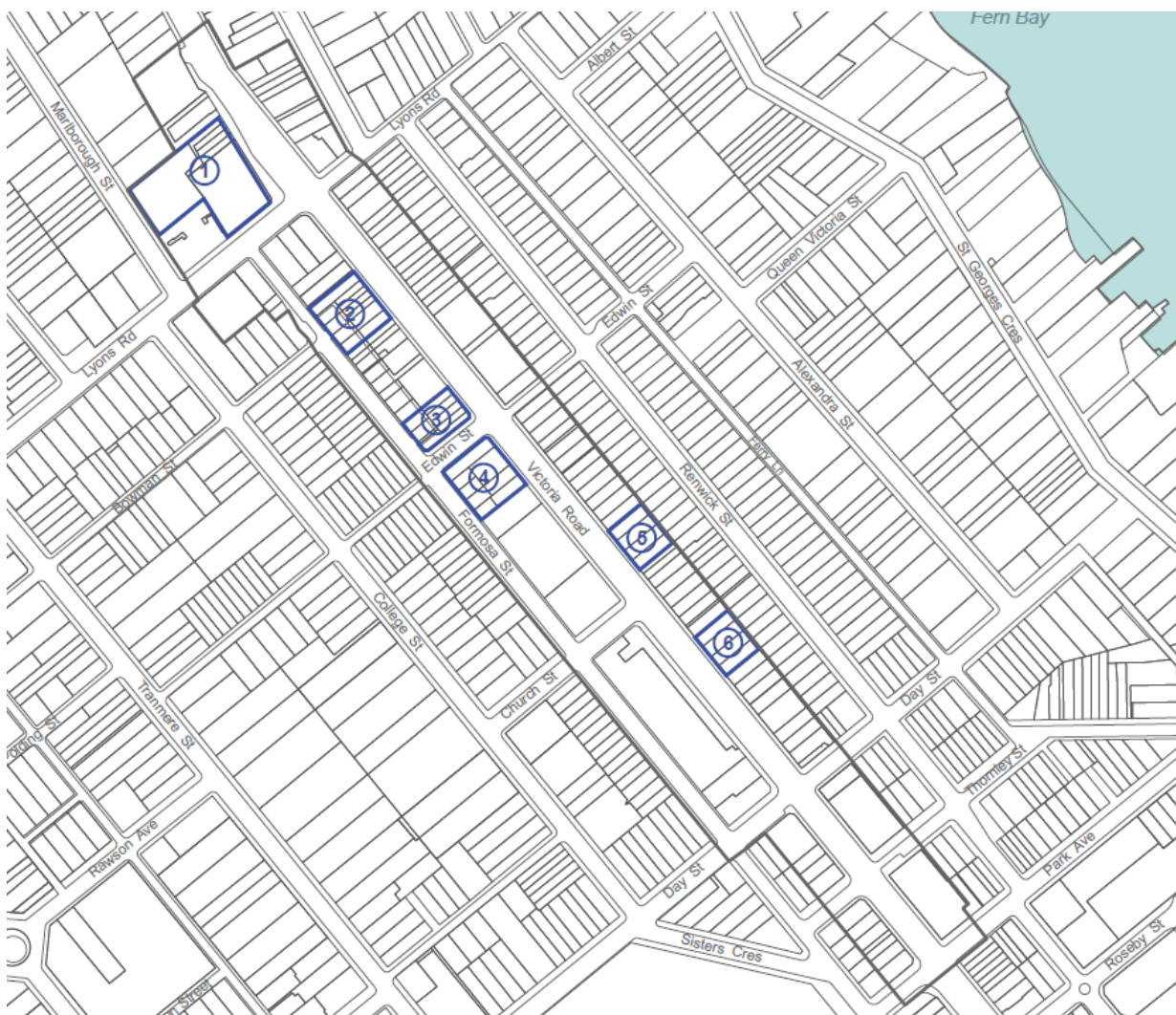
Commercial suites in 135 Victoria Road are observed to be tightly held, with the latest sale in July 2018 achieving a price of \$10,000/sqm of building area (incidentally a record for the area). Feedback from marketing agents indicate these are highly sought after by professionals who live locally and who want to work in an accessible and high amenity environment. A review of the tenancy profile of these suites indicates they are mainly leased to professional services (solicitors, accountants, health therapists, etc.).

Car parking provision is an important consideration for these commercial suites. A suite of 80-100sqm generally markets with an expectation of one car space.

FEASIBILITY TESTING

This section provides an overview of feasibility testing carried out on three sites selected by Studio GL. Depicted in Figure 1, three sites (Sites 2, 4, 5) were selected out of a possible six sites to test development scenarios.

Figure 1: Victoria Road Study Area and Potential Catalyst Sites



Source: Studio GL

Two development scenarios were tested by Studio GL for each of the three sites:

- A scenario that complies with current planning controls.
- An alternate scenario which considered options beyond the planning controls where additional development capacity could be traded for community benefit.

Methodology

Prevailing property values ('as is' values) are a key factor to development feasibility. Sites with significant and valuable buildings will expectedly have higher property values and therefore cost more to amalgamate into a development block. A desktop review of existing uses and buildings in the Study Area was undertaken, with estimates of 'as is' values made from observations in ongoing market activity.

The Residual Land Value (RLV) approach has been adopted as the method of assessment. The RLV approach involves assessing the value of the end product of the development, allowing for development costs, and making a further deduction for the profit and risk that a developer would require to take on the project.

The Residual Land Value (RLV) can be defined to be the maximum price a developer would be prepared to pay for a site in exchange for the opportunity to develop the site, whilst achieving target hurdle rates for profit and project return. This approach involves assessing the value of the completed product, making a deduction for development costs and further deduction for profit and risk whilst ensuring the development achieves the target project margin and return.

A key metric for development feasibility is land value, which is a 'residual' after all costs and revenues are taken into account. The figure must be of a sufficient amount to encourage the owner to sell and/or displace the current use. In order for development of the Site to be viable, the Residual Land Value must exceed the 'as is' value of the land, i.e. the value of the land in its existing use including all improvements.

We highlight that feasibility modelling carried out is indicative only, relying on generic cost and revenue assumptions in the absence of detailed scheme information.

Site 2 (177-189 Victoria Road, 70 Formosa Street)

Site 2 is one of Victoria Road's most valuable 'spots', with some high profile and valuable retailers located therein (including Bowan Island Bakery, deli café). Their attractiveness is enhanced by direct rear access to the car park on Formosa Street. The car park at 70 Formosa Street is understood to accommodate 18 at-grade parking spaces.

Figure 2: Site 2 Aerial Image



Source: Nearmap

For the purposes of generic feasibility testing the following assumptions are made with respect to the cost of land:

- Sites in private ownership (177-189 Victoria Road) will be consolidated at market value plus a premium.
- Council's carpark (70 Formosa Street) is included in return for 18 parking spaces in a completed development or, for its value as a development site (at FSR 2:1), whichever is the higher.

Studio GL’s initial modelling of a compliant scheme demonstrated the maximum FSR could not be achieved. An alternate scenario was tested, allowing for an increased height up to 20m (6 storeys) and the creation of a new pedestrian link to Victoria Road.

Table 1: Site 2 Alternate Development Scenario

| Alternate Scenario | |
|--------------------|--|
| Site Area | 1,888sqm |
| FSR | 2.3:1 |
| Gross Floor Area | 4,519sqm |
| Development Yield | 41 units, 1,118sqm non-residential GFA |
| Feasible? | No |

Source: Studio GL, AEC

Generic feasibility testing suggests that because existing uses are valuable, a minimum FSR of 3.5:1 is needed for comprehensive redevelopment to be feasible. At lower densities, incremental development (e.g. additional storey/s, part demolition and development) will be more likely to occur than comprehensive redevelopment.

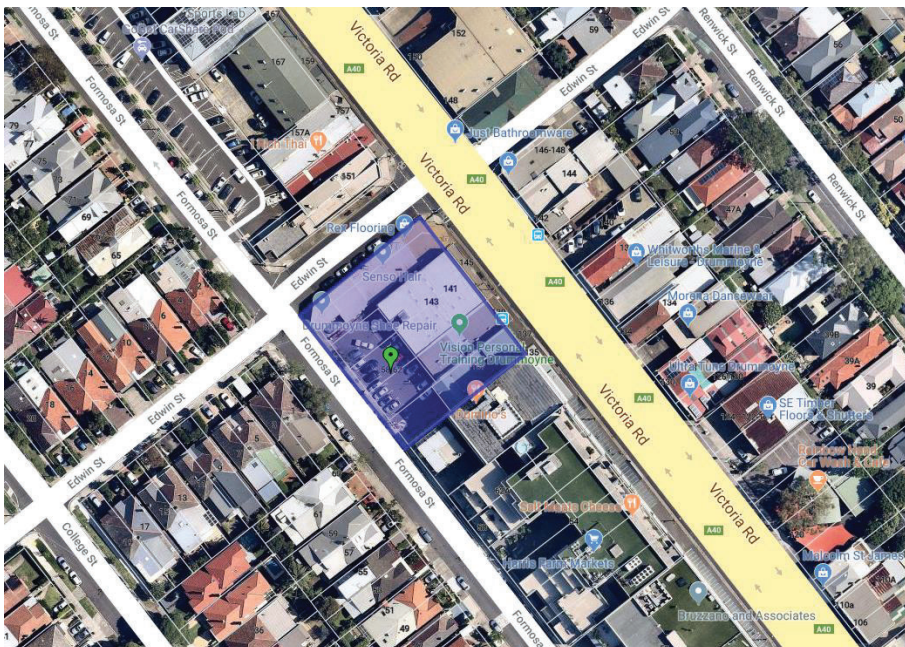
The likely cost to assemble Site 2 is cost-prohibitive for new development under FSR 3:1. After incorporating a 25% incentive premium to entice landowners to part with their property, the total cost of land of \$15.6m indicates that properties within Site 2 are currently in their highest and best use, i.e. as retail/ commercial buildings.

If FSRs were largely maintained (or even increased to FSR 2.75:1), we would recommend investigating opportunities where incremental development could be pursued.

Site 4 (137-147 Victoria Road, 64-66 Formosa Street)

Overall, Site 4 is expected to be marginally less valuable in comparison to Site 2. Site 4’s attractiveness is enhanced by direct rear access to Council’s car park on Formosa Street. The car park at 64-66 Formosa Street is understood to presently accommodate 20 at-grade parking spaces.

Figure 3: Site 4 Aerial Image



Source: Nearmap

For the purposes of generic feasibility testing, the following assumptions are made with respect the cost of land:

- Sites in private ownership (137-147 Victoria Road) will be consolidated at market value plus a premium.
- Council’s carpark (64-66 Formosa Street) is included in return for 20 parking spaces in a completed development or, for its value as a development site (at FSR 2:1), whichever is the higher.

Studio GL’s initial modelling of a compliant scheme demonstrated that the maximum FSR on the site could not be achieved. An alternate scenario was tested that allowed for increased height up to 20m (6 storeys).

Table 2: Site 4 Alternate Development Scenario

| Alternate Scenario | |
|--------------------|--|
| Site Area | 1,885sqm |
| FSR | 2.7:1 |
| Gross Floor Area | 5,099sqm |
| Development Yield | 46 units, 1,307sqm non-residential GFA |
| Feasible? | No |

Source: Studio GL, AEC

Generic feasibility testing finds that the FSR threshold required for feasible development of Site 4 is marginally lower than that for Site 3, requiring between FSR 3:1 and FSR 3.5:1.

Aside from 137 Victoria Road which appears to have been recently upgraded, the other properties within Site 4 appear to be older. If only part of Site 4 was required for development (excluding 137 Victoria Road which is likely to be the most costly property to acquire), development to FSR 2.75:1 is more likely to be feasible.

Site 5 (120 Victoria Road)

Site 5 is a relatively large site which presently accommodates a car wash business. The site is not as intensely developed as some of its neighbours which are improved with double storey commercial buildings. Existing improvements comprise a single level building with reception/ waiting area and hardstand area for parking cars.

Figure 4: Site 5 Aerial Image



Source: Nearthmap

Studio GL’s initial modelling of a compliant scheme demonstrated that the maximum FSR could not be achieved. An alternate scenario could allow for an increased height of 14m (4 storeys) and increased ground floor setback from Renwick Street.

However, due to vehicle access constraints from Victoria Road and sensitive interface with the adjacent conservation area, the compliant scenario (rather than the alternate scenario) was selected for further investigation.

Table 3: Site 5 Compliant Development Scenarios

| Compliant Scenario | |
|--------------------|--------------------------------------|
| Site Area | 1,105sqm |
| FSR | 1.6:1 |
| Gross Floor Area | 1,790sqm |
| Development Yield | 12 units, 818sqm non-residential GFA |
| Feasible? | No |

Source: Studio GL, AEC

Generic feasibility testing finds that the compliant scenario tested by Studio GL (FSR 1.6:1) is not feasible.

Notwithstanding, due to the nominal improvements on the site, development could be a marginal to feasible proposition if an FSR of 2:1 could be achieved.

CONSIDERATIONS FOR REVITALISATION OF VICTORIA ROAD

Overall, development opportunities in the Study Area are sought after. While offering high exposure/ visibility to businesses fronting Victoria Road, Victoria Road is not a pleasant pedestrian experience. Car parking is additionally difficult on Victoria Road, with Council’s carparks on Formosa Street playing an important role to mitigate this.

Our findings suggest that while Victoria Road will continue to suit businesses wanting exposure/ visibility being on a major arterial road, there could be an opportunity to position Formosa Street to accommodate a more pleasant retail experience. There are already businesses on Formosa Street that offer this (e.g. Copo Café). The linking of the ‘high-vis’ uses on Victoria Road with retail strip on Formosa Street would be important to leverage patronage.

Revitalisation and renewal in existing urban areas is challenging, with the largest challenge arguably being the cost to consolidate sites economically given valuable existing uses and fragmented ownership patterns.

In some urban areas development to greater densities can be considered to unlock development potential, however in other areas a significant increase to densities is not viable given their proximity to, and interface with heritage conservation areas and low density residential areas.

In the case of Victoria Road, Drummoyne, development to existing FSR controls is feasible where existing buildings are old/ dilapidated and therefore less valuable. This is observed on the eastern side of Victoria Road south of Church Street where a number of dilapidated strip retail properties have been consolidated in recent years and progressed as DAs to FSR 2:1. Because these properties are less desirable, developers have been able to purchase them at economic prices that make comprehensive development to the existing FSR controls feasible.

Where there are valuable buildings and high values, comprehensive redevelopment would require FSRs that are unlikely to be compatible in the urban context. Incremental development to realise latent floorspace capacity is the more likely outcome in these situations.

In urban areas such as Surry Hills and Darlinghurst, incremental development activity is observed to be a common approach by landowners to add value to their properties. Where the existing buildings are functional and valuable, alterations and additions to realise undeveloped floorspace (either by partial demolition and development, or by addition of storey/s) can be more economically viable than to demolish the existing buildings for new development.

Incremental development allows for an eclectic retention of the character of the area while renewing parts of buildings that are dilapidated. Though not all sites and buildings lend themselves to incremental development.

Yours sincerely

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